

**CHARTER TOWNSHIP OF VAN BUREN
PLANNING COMMISSION AGENDA
Wednesday, July 27th, 2022 – 5:30 PM
Van Buren Township Hall
46425 Tyler Road**

Per guidance provided by the Wayne County Local Public Health Department for meetings of governmental bodies held under public act 228 of 2020, effective January 1, 2022, the following Zoom link is provided to enable remote participation in a meeting of the public body by the general public:

When: Jul 21, 2022 05:30 PM Eastern Time (US and Canada)

Topic: July 27, 2022 Van Buren Township Planning Commission Meeting

Please click the link below to join the webinar:

<https://us06web.zoom.us/j/89541735180>

Or One tap mobile :

US: +13017158592,,89541735180# or +13126266799,,89541735180#

Or Telephone:

Dial(for higher quality, dial a number based on your current location):

US: +1 301 715 8592 or +1 312 626 6799 or +1 646 931 3860 or +1 929 436 2866 or +1 253 215 8782 or +1 346 248 7799 or +1 386 347 5053 or +1 564 217 2000 or +1 669 444 9171 or +1 669 900 6833

Webinar ID: 895 4173 5180

See the Van Buren Charter Township website for additional information regarding how to participate in a Zoom public meeting.

CALL TO ORDER:

ROLL CALL:

APPROVAL OF AGENDA:

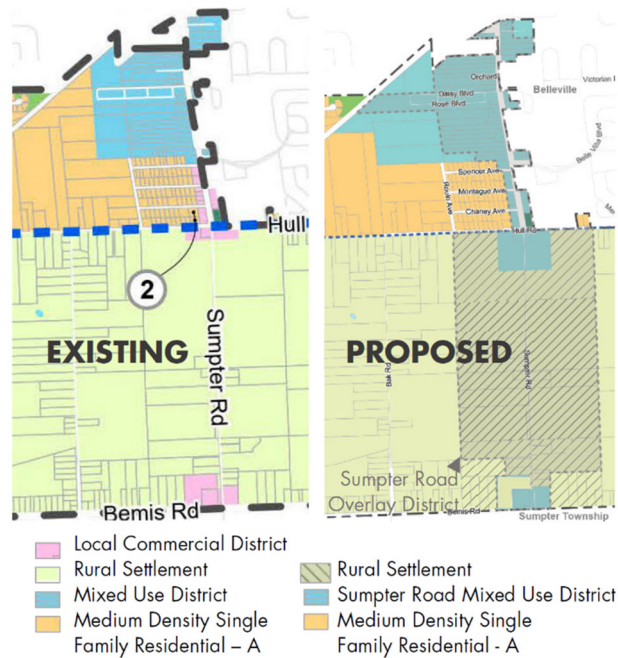
MINUTES: Approval of minutes from the regular meeting of July 13th, 2022.

CORRESPONDENCE:

PUBLIC HEARING:

ITEM # 1 SUMPTER ROAD CORRIDOR PLAN PUBLIC HEARING

DESCRIPTION: The Charter Township of Van Buren has completed a draft amendment to its 2020 Master Plan to address a sub area located along and near Sumpter Road between Bemis Road and Industrial Park Drive. The Plan includes proposed future land use recommendations, including changes to the future land use designations of certain properties in this area, and the creation of a proposed Sumpter Road Mixed Use District.



- ACTION ITEMS:**
- A. Planning Commission Opens Public Hearing
 - B. Public Comment
 - C. Planning Commission closes Public Hearing

ITEM # 2 SUMPTER ROAD CORRIDOR ZONING ORDINANCE TEXT AMENDMENTS

- DESCRIPTION:**
- Proposed amendments and additions to the Van Buren Township Zoning Ordinance to initiate a proposed Sumpter Road Mixed Use (SRMU) Zoning District and Sumpter Road Overlay District (SROD):
1. Article 2, Section 2.102. Definitions, Specific Terms.
 2. Article 3 – Zoning Districts and Permitted Uses: Section 3.104 (Permitted Uses by District), Section 3.105 (Footnotes to the Table of Permitted Uses and Special Land Uses by District), Section 3.119 (Belleville Road Mixed Use District) and Section 3.121 (Sumpter Road Mixed Use District).
 3. Article 4, Section 4.102. Schedule of Regulations.
 4. Article 6, Chapter 4. Supplemental zoning district standards, Sumpter Road Overlay District.

- ACTION ITEMS:**
- A. Planning Commission Opens Public Hearing
 - B. Public Comment
 - C. Planning Commission closes Public Hearing

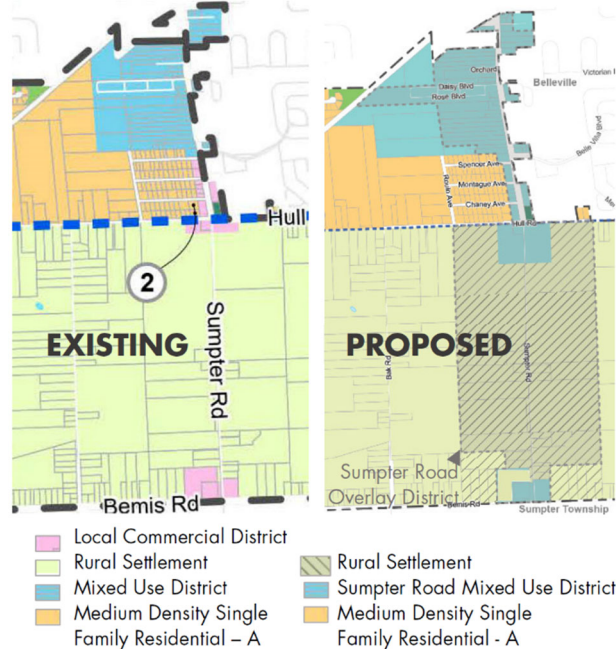
OLD BUSINESS:

NEW BUSINESS:

ITEM # 1 SUMPTER ROAD CORRIDOR PLAN ADOPTION

- DESCRIPTION:**
- The Charter Township of Van Buren has completed a draft amendment to its 2020 Master Plan to address a sub area located along and near Sumpter Road between Bemis Road and Industrial Park Drive. The Plan includes proposed future land use

recommendations, including changes to the future land use designations of certain properties in this area, and the creation of a proposed Sumpter Road Mixed Use District.



ACTION ITEMS:

- A. Presentation from Staff
- B. Planning Commission discussion
- C. Public Comment
- D. Planning Commission considers resolution to Township Board for adoption of Sumpter Road Corridor Plan.

ITEM # 2

SUMPTER ROAD CORRIDOR ZONING ORDINANCE TEXT AMENDMENTS

DESCRIPTION:

Proposed amendments and additions to the Van Buren Township Zoning Ordinance to initiate a proposed Sumpter Road Mixed Use (SRMU) Zoning District and Sumpter Road Overlay District (SROD):

1. Article 2, Section 2.102. Definitions, Specific Terms.
2. Article 3 – Zoning Districts and Permitted Uses: Section 3.104 (Permitted Uses by District), Section 3.105 (Footnotes to the Table of Permitted Uses and Special Land Uses by District), Section 3.119 (Belleville Road Mixed Use District) and Section 3.121 (Sumpter Road Mixed Use District).
3. Article 4, Section 4.102. Schedule of Regulations.
4. Article 6, Chapter 4. Supplemental zoning district standards, Sumpter Road Overlay District.

ACTION ITEMS:

- A. Presentation from Staff
- B. Planning Commission discussion
- C. Public Comment
- D. Planning Commission recommendation to Township Board for consideration of adoption of zoning ordinance amendments.

ITEM # 3

LANDSCAPING PLAN MODIFICATION – 10010 BELLEVILLE ROAD (MENARD’S)

TITLE: The Planning Commission is requested to consider proposed modifications to the approved landscaping plan for the Menard's store located at 10010 Belleville Road (parcel ID number 83 061 99 0005 726) to allow for a gated express lane entry into the store's lumber yard, to allow a reduction of a roughly 160' long section of the site's northern greenbelt to a width of less than 20'.

ACTION ITEMS: A. Presentation from Staff
B. Planning Commission discussion
C. Public Comment
D. Planning Commission considers approval of landscape plan modification.

ITEM # 4 ZONING ORDINANCE DISCUSSION: SIDE ENTRY GARAGE AND BUILDING SEPARATION REQUIREMENTS FOR RESIDENTIAL DEVELOPMENTS

TITLE: The Planning Commission is requested to discuss zoning requirements for minimum percentage of side entry garages and building separation in residential developments.

ACTION ITEMS: A. Presentation from Staff
B. Planning Commission discussion
C. Public Comment
D. Planning Commission considers continuation of research and discussion

ITEM # 5 GASOLINE FILLING AND GASOLINE SERVICE STATION ZONING ORDINANCE TEXT AMENDMENT

TITLE: Proposed amendments to Article 3 (Zoning Districts and Permitted Uses) regarding the categorization of gasoline filling service stations.

ACTION ITEMS: A. Presentation from Staff
B. Planning Commission discussion
C. Public Comment
D. Planning Commission recommendation to Township Board for consideration of adoption of zoning ordinance amendments.

GENERAL DISCUSSION AND UPDATES

ADJOURNMENT:

**CHARTER TOWNSHIP OF VAN BUREN
PLANNING COMMISSION
July 13, 2022
MINUTES**

Chairperson Kelley called the meeting to order at 5:30 p.m.

ROLL CALL:

Present: Kelley, Jahr, Barr, Budd

Excused: Cullin and Atchinson.

Staff: Director Power, Director Renaud, Executive Assistant Dohring, Director Ireland, and Secretary Zantop.

Planning Representatives: McKenna Associates, Vidya Krishnan, and Fishbeck Associate, Paul Kammer.

Applicant(s) in Attendance: Craig Armstrong of Elevatus Architecture on behalf of Meijer, Stephen Gedert on behalf of Van Buren Township.

Audience: Six (6)

APPROVAL OF AGENDA:

Motion Barr, Budd second to approve the agenda of July 13, 2022, as presented. **Motion Carried.**

APPROVAL OF MINUTES:

Motion Jahr, Barr second to approve the regular meeting minutes of June 22, 2022, as presented. **Motion Carried.**

PUBLIC HEARING:

ITEM # 1 GASOLINE FILLING AND GASOLINE SERVICE STATION ZONING ORDINANCE TEXT AMENDMENT

**Proposed amendments to Article 3 (Zoning Districts and Permitted Uses)
regarding the categorization of gasoline filling service stations.**

Motion Jahr, Barr second to open the public hearing. Motion Carried.

Director Power gave a brief introduction on the proposed zoning ordinance text amendments. The amendment would make all gas filling stations a special land uses in their perspective zoning districts where they are permitted. Currently, gas stations are permitted by right in a number of zoning districts. Special land use process requires a public hearing and Planning Commission and Board approval.

No comments from the audience or remote viewers.

Motion Budd, Jahr second to close the public hearing. Motion Carried.

ITEM # 2 CASE 22-006: MEIJER, INC – PHARMACY DRIVE-THRU AND GASOLINE STATION RENOVATIONS – SPECIAL LAND USE

Applicant Craig Armstrong of Elevatus Architecture on behalf of owner Meijer, Inc. requests to complete renovations to the existing 232,431 square foot Meijer retail store and gas station site to construct a drive-thru addition to the store’s pharmacy, a 474-square foot addition to an existing 2,691 square foot gasoline station convenience store, and related building and site improvements. The property is zoned C-2 - Extensive Highway Business District.

9701 Belleville Road (parcel ID number V-125-83-058-99-0006-705), located on the east side of Belleville Road south of Tyler Road.

Motion Jahr, Barr second to open the public hearing. Motion Carried.

Director Power clarified the special land use consideration is specific to the addition of a pharmacy drive-thru at the Meijer site at 9701 Belleville Road.

No comments from the audience or remote viewers.

Motion Budd, Jahr second to close the public hearing. Motion Carried.

NEW BUSINESS:

ITEM # 1 CASE 22-006: MEIJER, INC – PHARMACY DRIVE-THRU AND GASOLINE STATION RENOVATIONS – SPECIAL LAND USE

Applicant Craig Armstrong of Elevatus Architecture on behalf of owner Meijer, Inc. requests to complete renovations to the existing 232,431 square foot Meijer retail store and gas station site to construct a drive-thru addition to the store’s pharmacy, a 474-square foot addition to an existing 2,691 square foot gasoline station convenience store, and related building and site improvements. The property is zoned C-2 - Extensive Highway Business District.

LOCATION: 9701 Belleville Road (parcel ID number V-125-83-058-99-0006-705), located on the east side of Belleville Road south of Tyler Road.

Director Power displayed plans for reference and deferred to Principal Planner Vidya Krishnan of McKenna Associates for presentation of her review.

Vidya Krishnan of McKenna Associates summarized her review letter dated 6-30-22. The proposal is to remodel the existing Meijer store. The building will offer a drive-thru pharmacy, a reconfigured garden center along with upgraded façade treats for the entire building and the remodeling of the existing gas station. The proposed special land use meets the general standards for consideration of special land use approval. Therefore, McKenna Associates recommends the Planning Commission recommend to the Township Board of Trustees to grant special land use approval for the proposed pharmacy drive-thru use to be added to the Meijer site.

Craig Armstrong of Elevatus on behalf of owner Meijer, Inc. stated all drawings are being revised to remove outdoor display sales areas and add portions of landscaping in accordance with the C-2 zoning district. Mr. Armstrong is available to address any questions.

Commissioner Jahr commented that the report from Elevatus Architecture was very thorough.

No questions or comments from the audience or remote views.

Motion Jahr, Barr second to recommend to the Township Board to grant the applicant Craig Armstrong of Elevatus Architecture on behalf of owner Meijer, Inc. the special land use approval to permit the construction of a drive-thru addition to the store's pharmacy at the site located at 9701 Belleville Rd based of the analysis and subject to any conditions in the McKenna letter dated 6-30-2022 and the revised letter from the Planning Director.

Roll Call:

Yeas: Kelley, Jahr, Barr, Budd

Nays: None.

Excused: Cullin and Atchinson.

Motion Carried.

ITEM # 2 CASE 22-006: MEIJER, INC – PHARMACY DRIVE-THRU AND GASOLINE STATION RENOVATIONS – PRELIMINARY SITE PLAN

Applicant Craig Armstrong of Elevatus Architecture on behalf of owner Meijer, Inc. requests to complete renovations to the existing 232,431 square foot Meijer retail store and gas station site to construct a drive-thru addition to the store's pharmacy, a 474-square foot addition to an existing 2,691 square foot gasoline station convenience store, and related building and site improvements. The property is zoned C-2 - Extensive Highway Business District.

LOCATION: 9701 Belleville Road (parcel ID number V-125-83-058-99-0006-705), located on the east side of Belleville Road south of Tyler Road.

Director Power clarified the Planning Commission may approve preliminary and final site plan approval at the same time based on the language in the zoning ordinance. Director Power displayed the site plan.

Vidya Krishnan of McKenna Associates presented her site plan review letter dated 6-30-2022. The site was constructed predating the Belleville Road Overlay District (BROD). The site was brought as much to compliance as possible to meet the requirements of the BROD. The applicant eliminated parking spaces by putting in new landscape island. It is recommended that in the future if any additional outlots are developed on the north side of the site, the excess parking be used towards such use instead of construction of more impervious surface. If engineering is compliant at this time, McKenna Associates recommends preliminary and final site plan approval for the Meijer project subject to the four (4) conditions listed in the site plan review letter dated 6-30-2022. A set of revised plans shall be submitted to Director Power addressing the following conditions:

1. Removal of outdoor sales area shown on sheet E-101(2).
2. Clarification of calculations with regard to required landscaping square footage of various areas on the site.
3. Revised photometric plan to indicate compliance with illumination intensity below the gas station canopy.
4. Wayne County and Township Engineer approval of storm water detention

Paul Kammer of Fishbeck Associates presented his final site plan review letter dated 7-7-2022. Due to the lack of underground work the applicant has chosen not to do soil boring. The applicant has stated that a 3rd Party inspector will be onsite, determining soil bearing capacity. It is ultimately the applicant's responsibility to determine soil bearing capacity and ground water for any excavation work they are performing. The Township inspector(s) has the right to require additional testing be done if results are not acceptable. It is Fishbeck's understanding that the Meijer Site drainage system and pond have been a cause for some concern with the Township. The applicant has provided a "Stormwater Restoration Work" narrative, explaining the restoration and revitalization work that was done to the existing pond in September of 2020. It shall be the applicant's responsibility to maintain the functionality of the system throughout and after construction is complete. Fishbeck recommends Final Site Plan approval based on the Engineering Review for the Meijer Store addition and renovation project subject to the applicant addressing the final comments listed in the letter dated 7-7-2022.

Director Power gave an overview of the Fire Marshall's review of the Meijer plans. All concerns have been addressed and note the information for "Knox Box" on the review letter.

Commissioners and audience members had the following questions and comments:

- Commissioner Jahr asked Mrs. Krishan about the sign package and sign plans. Typically, applicants submit the sign package once the building permits have been approved. Note the sign shown in the architecture plan are not approved in those dimensions and will need to be permitted separately.
- Commissioner Jahr asked Mr. Kammer about the condition he removed regarding steps. In the original review of the detailed grading plan, there were grades that were off by about one (1) foot. The applicant was asked to note on the grading plan that those are in fact steps. The comment has been resolved.
- Commissioner Jahr commented that the plans look nice.
- An audience member asked how the drive-thru for the pharmacy will be routed and if there will still be a garden center. The route for the drive-thru is a horseshoe shape shown and explained on the displayed plan. Meijer associates explained the outdoor garden area will be reduced in size to not interfere with the drive-thru.
- Commissioner Jahr stated Mrs. Krishnan and Mr. Kammer are in favor of preliminary and final site plan approval and asked Director Power what his stance is. Director Power is in favor of both preliminary and final site plan approval.

Motion Barr, Jahr second to grant approval for preliminary and final site plan for the Meijer Pharmacy drive-thru and gas station renovations at 9701 Belleville Road based on and subject to any recommendations in the June 30, 2022, McKenna letter, adding point five (5) that signs still require approval, the 7-7-2022 Fishbeck engineering letter, acknowledging that the grading plans would indicate the steps, and the 5-17-2022 letter by the Van Buren Fire Department.

Roll Call:

Yeas: Kelley, Jahr, Barr, Budd

Nays: None.

Excused: Cullin and Atchinson.

Motion Carried.

**ITEM # 3 CASE 22-008 – VAN BUREN CHARTER TOWNSHIP COMMUNITY CENTER
RENOVATION AND ADDITION – FINAL SITE PLAN**

The Applicant, Stephen J. Gedert, On behalf of Owner Van Buren Charter Township, has applied for roughly 13,446 square feet of renovations and a 16,411 Square Foot Gymnasium addition adjacent to the existing municipal building, renovations to the existing building, and the construction of associated pavements and utilities.

LOCATION: 46425 Tyler Road (PARCEL ID NUMBER 83 061 99 0006 000). the site is located east of Quirk Road and south of Tyler Road.

Director Power present his final site plan review letter dated 7-8-2022 and displayed the site plan. Most preliminary site plan comments have been addressed. Director Power recommends conditional approval of the Plan dated 4-5-2022, subject to the following conditions:

1. If the alternative second driveway is used, Wayne County Department of Public Services (WCDPS) approval of the driveway must be provided at the time of construction.
2. Any changes to the configuration of the Water and Sewer building material yard and parking area are subject to approval by the Van Buren Township Water Department.
3. The three (3) trees proposed for removal along the south end of the Water and Sewer building material yard and parking area must be replaced. • Any proposed replacement fencing along the south end of the Water and Sewer Building site is subject to review and approval by the Planning Director.
4. The final pond design shall be coordinated between the applicant and the Township Department of Public Services to ensure the basin meets all aesthetic, operational, and stormwater design needs.

Paul Kammer of Fishbeck Associates presented his final site plan review letter dated 7-8-2022. Fishbeck Associates recommends the Planning Commission grant Final Site Plan Approval, based on the engineering aspects of the project, subject to the comments and conditions in the letter being met. The stormwater management and detention pond design is the main aspect of design that will need to be worked out with the Township and Engineer prior to getting final construction approval. It is not anticipated that any of the comments or conditions listed above will impact the final layout or site design when finalized, sans the fence, and impact to the existing DPW lot. The applicant will be required to submit a final Issued for Construction plan set for approval by the Township and Engineering prior to moving forward to the PreConstruction phase of the project.

Dan Power reviewed the letter from the Fire Department from 4-26-2022 plans are approved with conditions/comments

1. There is no need at this time to place a turn-around at the end of the lot. The parking lot does not meet the provisions of an access road. NFPA 1 2018 18.2.3.1.3* (6)
2. The FDC has been relocated to the South East corner of the building.
3. It is further understood that Building, Fire Suppression, and Fire Alarm will be submitted at a later date.
4. Two-Way Radio Communication Enhancement Systems are required for all new construction, unless after finished construction the occupant can prove through a signal test with the AHJ that it is not needed.

5. In all new and existing buildings, minimum radio signal strength for the fire department communications shall be maintained at a level determined by the AHJ. NFPA 72 24.5.2.2.3

Director Power reviewed the letter from Public Safety dated 4-12-22. In the letter Chief Wright, recommended a fence for security purposes be erected around the police department's employee parking lot along with a mechanical gate across the Quirk Road parking lot entrance/exit. This will provide a visual deterrent as well as a proper barrier to stop unnecessary general public access to where all township employees park their personal vehicles. It will also provide better security for marked patrol vehicles, DPW trucks, and equipment that is stored and utilized within this area.

Commissioners had the following questions and comments:

- Treasurer Budd questioned if the enlarged stormwater pond will interfere with the existing soccer fields. Paul Kammer of Fishbeck showed the stormwater pond will be extended east not to interfere with the soccer fields. Treasurer Budd asked if there will be a fence around the pond. There is concern that the proximity to the outdoor arena area will attract kids. It is designed to be a dry detention pond. It needs to hold excess flow for 48-72 hours then it will discharge back into the system. During and after rain events there could be up to 2 ft of water. The bottom being flat and the side slopes will be walkable. Dry ponds do not require a fence. Treasurer Budd stated her concern with the pond's proximity to the splash pad and the proposed outdoor area.
- Director Power clarified if the pond is functioning properly it will be dry most of the time if there has been no rain. There could be low-level water or muddy conditions from the basic flow of water from the splash pad. In general, within 48 hours of a storm, depending on the size, there should be dry conditions in the pond.
- Commissioner Jahr asked if the current pond is dry because it is broken? Is the new dry pond designed to be mostly dry? The existing pond is designed to be a dry pond. It does not always function how it is supposed to. Commissioner Jahr asked what is the surface layer composed of, is it gravel is it grass is it mud? It should be grass. The issue with dry ponds during wet and rainy seasons is it could cause a muddy surface. Commissioner Jahr asked if the pond is functioning correctly when there is a rain event within 48 hours there should be no more standing water and it should be grass, it should look like a field most of the time? This is correct.

Motion Jahr, Barr second to grant the applicant, Stephen J. Gedert, On behalf of Owner Van Buren Charter Township the final site plan approval to construct the renovations on the Gymnasium addition adjacent to the existing municipal building, renovations to the existing building, and the construction of associated pavements and utilities at the site located at 46425 Tyler Rd. Based on the analysis and subject to the conditions in the Fishbeck letter dated 7-8-2022 Van Buren Fire dated 4-26-2022 Revised July letter from staff. Noting the

feedback and concerns disused and subject to an update on the design and safety concerns of the dry pond, which are to be present to the commission in a future meeting.

Roll Call:

Yeas: Kelley, Jahr, Barr, Budd

Nays: None.

Excused: Cullin and Atchinson.

Motion Carried.

ITEM # 4 GASOLINE FILLING AND GASOLINE SERVICE STATION ZONING ORDINANCE TEXT AMENDMENT

Proposed amendments to Article 3 (Zoning Districts and Permitted Uses) regarding the categorization of gasoline filling service stations.

Item # 4 was withdrawn by staff.

GENERAL DISCUSSION:

Director Power informed the commission there will be a public meeting for the sumpter corridor plan and a public hearing for the related zoning ordinance amendments on July 27th, 2022.

ADJOURNMENT:

Motion Jahr, Budd second to adjourn the meeting at 7:12 p.m. Motion Carried.

Respectfully submitted,

Kendyl Zantop
Recording Secretary



MEMO

TO:	Van Buren Township Planning Commission
FROM:	Dan Power– Director of Planning and Economic Development
RE:	Adoption recommendations for Sumpter Road Corridor Plan and Sumpter Road Corridor Zoning Ordinance Amendments
DATE:	July 22, 2022

Planning Commissioners:

The Sumpter Road Corridor Plan (“the Plan”) will be an update to the Township’s Master Plan which focuses on the sub-area of the Township around the Sumpter Road Corridor. The Plan lays a broad foundation for the vision for the Corridor and its future land use. A full draft of the Plan, dated February 4, 2022 has been in circulation among the Township’s neighboring communities and service providers for greater than 42 days as required by Michigan PA 33 of 2008, or the Michigan Planning Enabling Act (MPEA). A public hearing was held regarding the Plan at the Planning Commission’s regular meeting of May 25, 2022 and another hearing is being held on July 27, 2022. The Plan is the result of extensive public input and consideration by the Planning Commission. Final changes to the Plan are in progress. These changes are minor, including minor graphics and formatting updates. The Plan is ready for adoption.

Prior to recommending Plan adoption, the Planning Commission was advised to develop the final draft Sumpter Road Mixed Use District (SRMU) and Sumpter Road Overlay District (SROD) zoning ordinance amendments, as these ordinance amendments will be the implementation tool for much of the Plan. Following a discussion of these amendments on June 22nd and some clarifications to the text made at that time, these amendments are also ready for a recommendation on potential adoption. The proposed Zoning Ordinance amendments and additions include the following sections:

1. Article 2, Section 2.102. Definitions, Specific Terms.
2. Article 3 – Zoning Districts and Permitted Uses: Section 3.104 (Permitted Uses by District), Section 3.105 (Footnotes to the Table of Permitted Uses and Special Land Uses by District), Section 3.119 (Belleville Road Mixed Use District) and Section 3.121 (Sumpter Road Mixed Use District).
3. Article 4, Section 4.102. Schedule of Regulations.
4. Article 6, Chapter 4. Supplemental zoning district standards, Sumpter Road Overlay District.

If the Planning Commission is inclined to recommend adoption of both the Plan and the SRMU and SROD Zoning Ordinance text amendments, I recommend they should make motions to approve these two items separately. The Board of Trustees will then be asked to consider adoption of both separately on their first available meeting agenda.

Thank you for your ongoing participation in this effort.

Sincerely,

A handwritten signature in black ink, appearing to read "Dan Power". The signature is fluid and cursive, with the first name "Dan" and last name "Power" clearly distinguishable.

Dan Power, AICP
Planning and Economic Development Director
Charter Township of Van Buren Public Services Department

CC: Vidya Krishnan, McKenna Associates – Van Buren Township Planning Consultant
Adam Cook, McKenna Associates

Charter Township of Van Buren, Wayne County, Michigan
2022 Sumpter Road Corridor Plan
RESOLUTION of ADOPTION

WHEREAS the Michigan Planning Enabling Act (Public Act 33 of 2008), as amended, provides for a Township planning commission to prepare and adopt a Master Plan for physical development of the community, and allows for the adoption of a subplan for a geographic area less than the entire planning jurisdiction of the Township; and

WHEREAS through the adopted 2020 Township Master Plan, the Van Buren Charter Township Planning Commission identified that property near and adjacent to Sumpter Road is in need of particular planning focus, and the Planning Commission prepared an intention to provide a sub area plan for this area within roughly one quarter of a mile to its east and west from Bemis Road to Industrial Park Drive; and

WHEREAS the Van Buren Charter Township Planning Commission has prepared such a Sub Area Plan in compliance with the Michigan Planning Enabling Act, including relevant charts, maps and text; and

WHEREAS the Van Buren Charter Township Planning Commission has provided multiple opportunities for public participation in the planning process; and

WHEREAS the Van Buren Charter Township Board of Trustees approved the draft Plan for distribution on March 1, 2022, and subsequently the Sumpter Road Corridor Plan was so distributed for review by surrounding communities and other public agencies as required by the Michigan Planning Enabling Act; and

WHEREAS the Van Buren Charter Township Planning Commission held a formal public hearing on the draft Sumpter Road Corridor Plan on May 25, 2022 and July 27, 2022 in order to provide additional opportunity for public comment; and

WHEREAS all comments received during the planning process have been carefully considered and the Planning Commission is satisfied that the Sumpter Road Corridor Plan is ready for adoption.

NOW THEREFORE BE IT RESOLVED that the Van Buren Charter Township Planning Commission hereby adopts the 2022 Van Buren Charter Township Sumpter Road Corridor Plan, as presented at the public hearing held on July 27, 2022, subject to incorporation of minor revisions, which are non-substantive and do not impact future land use direction.

BE IT FURTHER RESOLVED that the Van Buren Charter Township Planning Commission directs the Commission Chairperson and Commission Secretary to sign this Resolution signifying the adoption of the Van Buren Charter Township Master Plan, to file an attested copy with the Township Clerk, and to request that the Master Plan, as revised (if applicable), be placed on the next available agenda of the Township Board of Trustees for consideration and adoption consistent with the Michigan Planning Enabling Act.

Motion by _____ and seconded by _____

AYES: _____

NAYS: _____

ABSENT: _____

Resolution Declared Adopted.

Bryon Kelley, Chairperson
Van Buren Charter Township Planning Commission

Tina Harman, Recording Secretary
Van Buren Charter Township Planning Commission



VAN BUREN

CHARTER TOWNSHIP

SUMPTER ROAD CORRIDOR PLAN

VAN BUREN CHARTER TOWNSHIP, WAYNE COUNTY, MICHIGAN

DRAFT – June 21, 2022
Revised - July 19, 2022



VAN BUREN
CHARTER TOWNSHIP

SUMPTER ROAD CORRIDOR PLAN

VAN BUREN CHARTER TOWNSHIP, WAYNE COUNTY, MICHIGAN

DRAFT – June 21, 2022

Prepared with the assistance of:



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DRAFT - June 21, 2022

2021 Sumpter Road Corridor Plan | i

ACKNOWLEDGMENTS

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INTRODUCTION

Sumpter Road is a north-south corridor that bisects the southern portion of Van Buren Charter Township. It serves as a southern gateway to the City of Belleville, which remains the historic downtown center of the Township. In addition, the corridor also serves as a northern gateway to the rural, southern portion of the Township.

The 2020 Van Buren Charter Township Master Plan designated portions of the Sumpter Road corridor as Mixed Use in the Future Land Use Plan, noting that areas of the Township so designated were intended to be planned at an area-specific level in the future. Furthermore, the 2020 Master Plan is intended to serve as the first book in a series, being Township-wide in scope, with future books to be added as new area-specific plans are developed, and as previous area-specific plans are brought up-to-date.

In early 2021, Township staff and stakeholders identified the Sumpter Road Corridor as the first area-specific plan to be completed in a format consistent with the 2020 Township-wide Master Plan. This Sumpter Road Corridor Plan is therefore guided by the same Township-wide policies, Goals, and Objectives expressed in the 2020 Master Plan—applied at a place-specific level.

THE CORRIDOR THROUGH THE YEARS

The growth and development of the modern City of Belleville has influenced change along the Sumpter Road corridor over the course of 150 years. Sumpter Road was originally a section line in the Northwest Survey, taken in the area in 1815. The settlement of Belleville was originally platted in 1847, twelve years after Van Buren Township's incorporation. Belleville developed into a commercial and cultural center following completion of the Wabash Railroad in 1881, which added value to the open farmland to the south along Sumpter Road. The diagonal course of the railroad in this area bisected some of the landholdings south of Belleville, creating fragmented land parcels lacking access to a public roadway. Through the early 20th Century, large parcels of land along the corridor were held by relatively few owners. Agriculture appeared to focus on cash crops and fruit orchards, produce that could easily be shipped to the growing City of Detroit and its suburbs via the adjacent railroad.

In the 1920s, adoption of the automobile and the creation of Belleville Lake spurred new demand for residential uses in the area surrounding Belleville. P.J. Roulo, a local justice of the peace, had taken claim to much of the land along Sumpter Road from Belleville south to what was then known as Eureka Road—now known as Hull Road—and began subdividing the land at the northwest corner of Sumpter and Hull Roads in 1923. As seen in the plat map on page 28, Roulo's Subdivision was originally

TIMELINE

The images below show 100 years of change along Sumpter Road. The large, agricultural land holdings of the 1880s were bisected by the Wabash Railroad, creating the framework for landlocked parcels in this area. P.J. Roulo platted the first residential neighborhood on the corridor in the 1920s. In 1949, Sumpter Road had become predominantly residential, though several



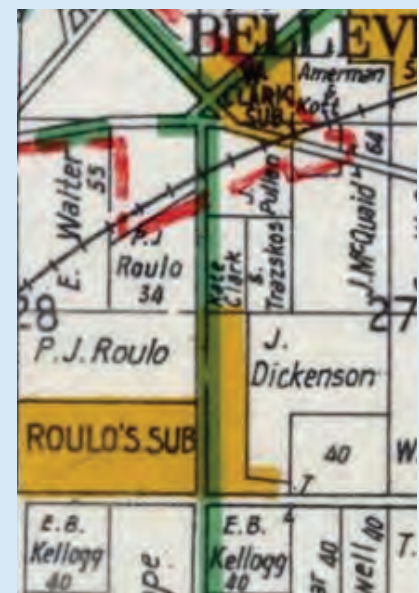
1876

Source: H. Belden & Co., Wayne County Atlas



1883

Source: W.C. Sauer, Wayne County Atlas



1925

Source: W.S. McAlpine, Wayne County Farm Atlas

intended to be twice as large as the area ultimately built; the three blocks to the west of Roulo Avenue were consolidated in 1933 and never developed. While Roulo's Subdivision grew slowly through the 1940s, it became popular in the "baby boom" years following World War II, when many of the original lots were split to add additional houses. The neighborhood retains a charming, rural character due to the varying architectural styles of the houses on gravel streets.

As demand for housing grew in the postwar years, holdings immediately north of Roulo's Subdivision were divided into long, skinny sections for residential purposes. Belleville continued to grow, annexing Township land to the east of Sumpter Road through the 1960s. Individual landowners were given the choice of "City or Township" during this process, creating a sawtooth pattern in the boundary between Belleville and Van Buren Township that straddles Sumpter Road today; this anomaly led to two separate, and sometimes incongruous, planning and zoning policies being implemented along the corridor.

Further households came to the corridor by the 1970s, with the completion of the Mobile Manor and Belle Villa Meadows communities. This new "critical mass" of consumers induced development of several strip-type shopping centers along the corridor, which flourished when brand new, but have not aged well.

Finally, light industrial uses began to cluster near the railroad beginning in the 1960s. The earliest buildings were modest in size, and contain adjacent service bays that accommodate smaller commercial vehicles. This type of development culminated with the opening of the Davenport Industrial Park on the east side of the corridor in the 1990s.

fruit orchards remained active. The Mobile Manor community and Owen School, followed by the large Belle Villa Meadows community to the east, were developed in the 1960s and 70s. The land use pattern along the corridor was fixed by 1983: semi-rural residential lots, several strip-type local shopping plazas, and light industrial uses.



1949

Source: DTE Aerial Photo Collection



1967

Source: DTE Aerial Photo Collection



1983

Source: Historic Aerials

THE CORRIDOR TODAY

The Sumpter Road Corridor begins at the southern gateway to the City of Belleville, where South Street intersects the Norfolk Southern (formerly Wabash) Railroad. Sumpter Road then extends south for 1.8 miles, crossing Hull Road, and intersects Bemis Road at the border of Van Buren and Sumpter Townships. The Corridor can be viewed in two segments: the northern segment, from the railroad south to Hull Road; and the southern segment, from Hull Road to Bemis Road.

The northern segment of the Corridor is distinctively semi-rural in character. Though it is adjacent to more intense residential, commercial, and institutional land uses to the east, the western side of Sumpter Road here is defined by long, narrow residential parcels with attractive houses set back 50 to 100 feet from the road. These parcels contain large areas of underutilized space at the rear of their principal structures, creating a backdrop of natural space. Both the Mobile Manor and Belle Villa communities are set back roughly 300 feet from Sumpter Road, precluding the passer-by from sensing the large number of families contained in each. Closer to Hull Road, the Roulo Subdivision evokes the feeling of a rural hamlet, with gravel streets extending to the west, serving well-kept, modestly-sized homes.

This semi-rural feeling dominates a segment of the corridor that is shared by more intense land uses. Several “strip commercial” shopping plazas were developed here during the 1970s and 1980s, notably Belle Park Plaza on the east side of Sumpter Road and a smaller plaza on the west side. The former development, straddling the City-Township border, is in 2021 anchored by a “dollar” variety store and contains several independent service-oriented businesses. However, nearly half of the individual storefronts are vacant, and the site is dominated by an expanse of underutilized surface parking. The latter shopping plaza formerly housed the



The gas station and convenience store at the southwest corner of Sumpter and Hull Roads is well-kept and busy.



A semi-rural feel is created through large, long residential lots, yielding plenty of extra firewood.



Sumpter Road is home to several successful small businesses that serve locals' needs.

“Cork n’ Bottle” liquor store, and contains four retail spaces which are completely vacant. Additionally, there are several individual vacant commercial buildings along this segment of the corridor, including a former restaurant and a former lumber yard. A cluster of older, light industrial buildings is located at the northern end of the corridor, adjacent to the railroad tracks and Davenport Industrial Park. The smaller scale of these buildings, in contrast with their modern counterparts in the Industrial Park, adds to the sense that the corridor’s character is shifting toward a rural landscape as one moves away from Belleville.

The northern segment of the corridor contains several vibrant commercial and institutional developments, including a veterinary office, a gas station on the southwest corner of Sumpter and Hull Roads, and the Township Fire Station #1 on the northeast corner of the same intersection. These counterexamples show that changing retail patterns place certain commercial properties—those that were built around easy automobile access and large floorplates—at a disadvantage given their inflexible designs, rather than belying a fundamental fault with the local market potential for retail goods and services.



The Belleville Cooperative Senior Apartments on the east side of Sumpter Road add housing diversity to the corridor.



The Township’s Urban Growth Boundary runs along Hull Road. Sumpter Road south of Hull Road is defined by agriculture and open spaces.

Hull Road marks a distinct transition to the southern portion of the corridor, and the rural, southern portion of the Township. Excepting the gas station, land along Sumpter Road to the south is open, and consists primarily of active agriculture and 1-acre minimum residential lots. The 2006 South Side Master Plan established an Urban Services Boundary along Hull Road, though new development immediately south of Hull Road could make use of existing water and sewer infrastructure. The Boundary was established in response to a desire to preserve and retain the rural character in the southern portion of the Township. When this topic was revisited during development of the 2020 Master Plan, community members remained steadfast in their desire to keep a rural look and feel south of Hull Road.

The 2020 Master Plan has a clearly established Goal ensuring "...the preservation of the Township's rural character, particularly in the southern portion of the Township..." The recommendations for Future Land Use in this Corridor Plan are therefore consistent with this Goal and all associated Objectives in the 2020 Master Plan.

The southern portion of the Sumpter Road corridor, from Hull Road south to Bemis Road, is predominantly rural, with open fields extending to the east and west. Sumpter Road is buffered from the fields by a row of natural vegetation.



Van Buren Township Fire Station #1 is located at the northeast corner of Sumpter and Hull Roads.



A barn remains in situ north of the Township Fire Station #2, evoking an earlier era when the Sumpter Road corridor was completely agricultural.

Several houses exist on this part of the corridor, developed on large lots with deep setbacks. The most intense land use is the River of Life Church, on the west side of Sumpter Road, one quarter mile north of Bemis Road.

Bemis Road is the boundary of Van Buren and Sumpter Townships. The southern half of the intersection of Sumpter and Bemis Roads, located in Sumpter Township, contains a small gas station and a separate restaurant. To match this existing commercial development, a small pocket of eleven acres on the northern side of the intersection remained designated as Local Commercial in the 2020 Master Plan's Future Land Use Plan. This designation was carried over from the 2006 South Side Master Plan, though the land remains undeveloped as of 2021.

The design of Sumpter Road, particularly in the northern portion of the corridor, poses safety threats to all users. Lacking curb-and-gutter, there are many informal dirt "pull-outs" where commercial and light industrial buildings exist. The narrow frontage residential lots result in many driveway approaches clustered in a small area. Sidewalks are intermittent along the east side of Sumpter Road, and nonexistent on the west side; consequently, pedestrians and cyclists have an unpleasant and unsafe experience navigating the corridor.

Sumpter Road is generally two lanes wide between the City of Belleville and Bemis Road, though there are several areas where a third lane is present to accommodate the frequent turning movements presented by multiple driveways in a short space. The existing cross-section of Sumpter Road lends to the semi-rural character of the area. Modifications to Sumpter Road that would improve safety should involve removing unnecessary driveway entrances, providing off-street nonmotorized facilities, and ensuring that new development on the east side of the road north of Hull is accessible by a parallel slip street.



Several single-use commercial buildings have sat vacant along the corridor for years, awaiting redevelopment.



Single-use retail strip centers are not designed to meet the needs of today's shoppers and are dominated by parking.



This well-kept house is representative of those in the Roulo Subdivision at the northwest corner of Hull and Sumpter.

PUBLIC ENGAGEMENT

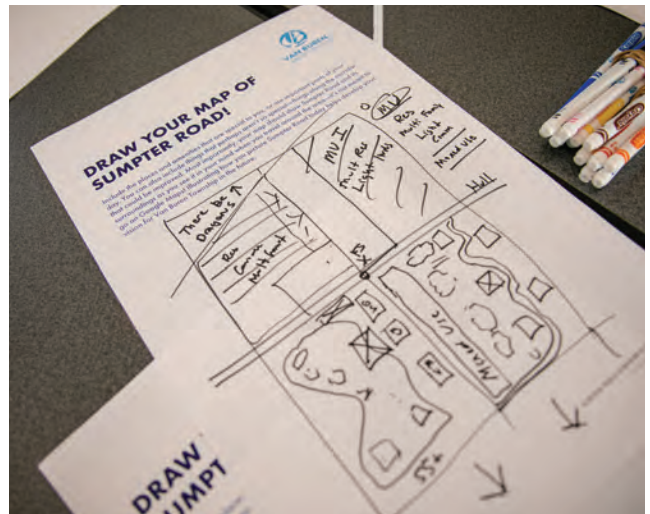
Public engagement was conducted throughout 2021 to understand Township residents' and business owners' perspectives on the Sumpter Road corridor. In response to publicity of the planning effort, Township staff communicated directly with residents via phone and email throughout the year, logging every comment and interaction. Additionally, two workshop events were held:

- A virtual introductory webinar was held on Wednesday, April 28, 2021. Conducted over Zoom, the webinar introduced attendees to the scope and objectives of the Corridor Plan, and generated constructive discussion focused on identifying strengths and weaknesses along the corridor.
- An in-person workshop was held on Wednesday, July 28, 2021. Taking place on the corridor, at Township Fire Station #1, the evening consisted of a variety of hands-on exercises and activities that generated many in-depth conversations.
- At a public meeting of the Planning Commission held on September 8, 2021, a Corridor Plan Subcommittee was formed consisting of the Township Planning Director and three Planning Commission members. Building off public feedback, this working group established consensus over the future Vision for the corridor and the associated land use policies.

Public feedback was grouped into six major themes, summarized below in Table 1. The predominant theme, echoed across all engagement efforts, was that the rural character of the corridor should be preserved. Nearby residents have either made a conscious decision to locate in the southern portion of the Township precisely for this atmosphere, or have lived in the area long enough to see suburbanizing land use patterns elsewhere pose a threat. The next two themes are intertwined: community members find the appearance of the corridor to be rundown, due to the cluster of underperforming and vacant commercial spaces. There is a sense of pride that comes with living in Van Buren Township, and it is felt that the Sumpter Road corridor is not representative of the qualities of the Township overall. The third theme therefore focuses on bringing needed neighborhood services back to the corridor. While large-scale suburban development (big box stores) is not desirable, the need for an appropriately-sized full service grocer or market is palpable.

Table 1: Major Themes in Public Feedback

Category	Representative Themes in Public Feedback
Rural Preservation	<ul style="list-style-type: none"> • I moved my family here for the nature. I won't stay if it converts to traffic and crowding. • Preserve the agriculture no differently than you would Belleville Lake. • Keep it rural and take care of what we already have.
Vacant Businesses	<ul style="list-style-type: none"> • I appreciate growth, but what can be done with empty storefronts? • There are so many vacant businesses - they have been vacant for years. • Use existing vacant buildings for business - don't build more.
Neighborhood Services	<ul style="list-style-type: none"> • We need a small grocery store or market. Right now, we have to drive through Belleville to access a full range of groceries - and that can be very inconvenient at rush hour. • A small restaurant or cafe would be nice on this side of Belleville.
Senior Housing	<ul style="list-style-type: none"> • What about a community for those over 55 years old? Many longtime residents want to stay in the area, but are worried about maintaining their houses and property. • A village-type concept for seniors would be nice.
Complete Streets	<ul style="list-style-type: none"> • I would like to see sidewalks along both sides of Sumpter, connecting to downtown Belleville. • My family and I would like to ride our bikes to downtown Belleville, but it's too unsafe right now. Continuous sidewalks, or better yet, a bike trail would be great.
Recreational Amenities	<ul style="list-style-type: none"> • The Romulus Rec Center and Quirk Park are too far away. People need more recreational opportunities around here! • A nature park and walking trail would be wonderful. • I would like to see a community garden.



Images from the Sumpter Road Corridor Workshop held on July 28, 2021.

THE CORRIDOR TOMORROW

The character of Sumpter Road has evolved over 150 years, in response to changes in technology and the economy. While these changes will inevitably continue into the future, what has been heretofore absent is an overall Vision for the corridor. By articulating a Vision, future planning and zoning decisions will positively influence how land uses and urban design shape the corridor in the future. A Vision for the corridor that is responsive to the needs and ideas expressed by local residents must address the following issues:

- Future land uses along Sumpter Road must shift to a model that allows a flexible mix of uses. This mix of uses can take place within individual buildings, or it can be achieved by allowing combinations of uses on individual parcels, across multiple buildings. New buildings, wherever they are located, must themselves be designed to be flexible and adaptable to changes in use over time.
- Preservation of the semi-rural character along the northern portion of the corridor must take precedence. Future development must be regulated through form-based standards, ensuring that buildings are constructed to be compatible in scale and architecture with a traditional, semi-rural context. Given that half of the frontage of the east side of Sumpter Road north of Hull road is located in Belleville, special coordination between the Township and City must take place to ensure “edge matching” is achieved through the zoning standards of both entities.
- In addition to having appropriate form, new buildings must be adaptable to an ever-evolving mix of uses over many decades. Many buildings that were constructed after 1950 were designed for a specific use at a specific point in time; they are increasingly unsuitable for productive use in the 21st Century. A corridor that remains economically viable must contain buildings that “stand the test of time”.
- Many land parcels on the west side of Sumpter Road are long and narrow, and some rear parcels have no access to public rights-of-way. Owners of these underutilized areas have not been able to make efficient use of the land. Providing new means of rear access would allow owners to derive untapped value from their holdings. Importantly, new development in these areas must take on a form different from that of the suburban office park or industrial park. Instead, a calibrated mixture of rural-style buildings should be permitted through form-based standards that cultivate small entrepreneurial, craft, artisanal, and extremely light industrial endeavors.
- Preservation of the rural character along the southern portion of the corridor is essential. The expansive open spaces that define this portion of Sumpter Road should be retained to the maximum extent possible. However, existing zoning in this area has allowed for conventional residential development to occur at a density of 1 dwelling unit per acre. The Vision must account for a path allowing new residential development south of Hull Road that preserves open space to the maximum extent possible.
- The land adjacent to the intersection of Sumpter and Hull Roads, while situated inside the Urban Growth Boundary, can be developed to a more intense standard considering its adjacency to existing urban services. The land adjacent to the intersection of Sumpter and Bemis Roads could be similarly developed. However, new commercial buildings at these intersections must not take on the characteristics of the typical suburban retail node or office park. Buildings adjacent to these intersections should be clustered together to preserve surrounding open space, in the form of a rural hamlet.
- The design of Sumpter Road itself is oriented almost exclusively toward automobile users. A future design concept for a configuration of Sumpter Road, between Hull Road and Belleville, should incorporate features that make Sumpter Road a safe and pleasant environment for users of all transportation modes.

The Vision Statement on the following page states four simple principles, providing a framework from which the above issues can be addressed.

VISION STATEMENT

The Sumpter Road Corridor is a southern gateway to Belleville and the economic centers of Van Buren Township. It also serves as a transition to the rural, southern portion of the Township.

The community's Vision for the Corridor is embodied in the following principles:

- The existing semi-rural character of the Corridor will be protected and preserved into the future.*
- The Corridor will accommodate a mix of land uses that are appropriate for its existing character and location within the Township.*
- New buildings along the Corridor will be traditional in appearance, appropriately scaled, and adaptable to evolving future circumstances.*
- Sumpter Road and nearby local streets will be safe, serene, and attractive places for all who use them—including pedestrians, cyclists, and motorists.*

Sumpter Corridor Future Vision





Slip street along east side of Sumpter with multi-use path

Shallow Frontage Lots

Rural Settlement: Cluster single family houses, preserve open space

Hull Road

Sumpter Road

Preserve character of Roulo subdivision

Anchor Lots: Hamlet-style node, neighborhood services

FUTURE LAND USE PLAN

The majority of the northern portion of the Sumpter Road corridor was designated Mixed Use on the Future Land Use Map in the 2020 Master Plan. The areas immediately surrounding the intersections of Hull and Bemis Roads retained the Local Commercial designation, and the remainder of the southern portion of the corridor retained the Rural Settlement designation. A modification to the Future Land Use Map for the Sumpter Road corridor is presented in the map on the facing page. This modification introduces the Sumpter Road Mixed Use category, and qualifies the some of the areas currently designated Rural Settlement as a Rural Residential Pattern Area (see description on pages 26-28).

Sumpter Mixed Use

DESCRIPTION

Intended to prevent typical strip commercial development, and to encourage the redevelopment of both shallow frontage lots and rear areas of deeper lots that are underutilized by creating “layers” of uses. Residential and nonresidential uses should be combined, located in mixed-use buildings or in separate buildings on the same site designed as an integrated development. Development should generally be limited to two or fewer stories, should be pedestrian-oriented, and strictly automobile-oriented uses should not be permitted.

Development in areas designated Sumpter Mixed Use should fit the context of a semi-rural area, consisting of buildings that have appropriate size, massing, and architectural form. Where possible, buildings should be clustered to preserve surrounding open space.

Development in areas designated Sumpter Mixed Use will not include traditional manufacturing, logistics, or warehousing uses at a large scale. Rather, a variety of residential, commercial, and small-scale “craft” and/or “artisanal” uses will comprise the range of permitted uses in this area.

APPLICABLE PORTIONS OF TOWNSHIP

The areas immediately to the east and west of Sumpter Road from the City of Belleville south to Hull Road. The areas immediately surrounding the intersections of Sumpter and Hull Roads, and Sumpter and Bemis Roads.

TRANSECT ZONES

T-3, T-4

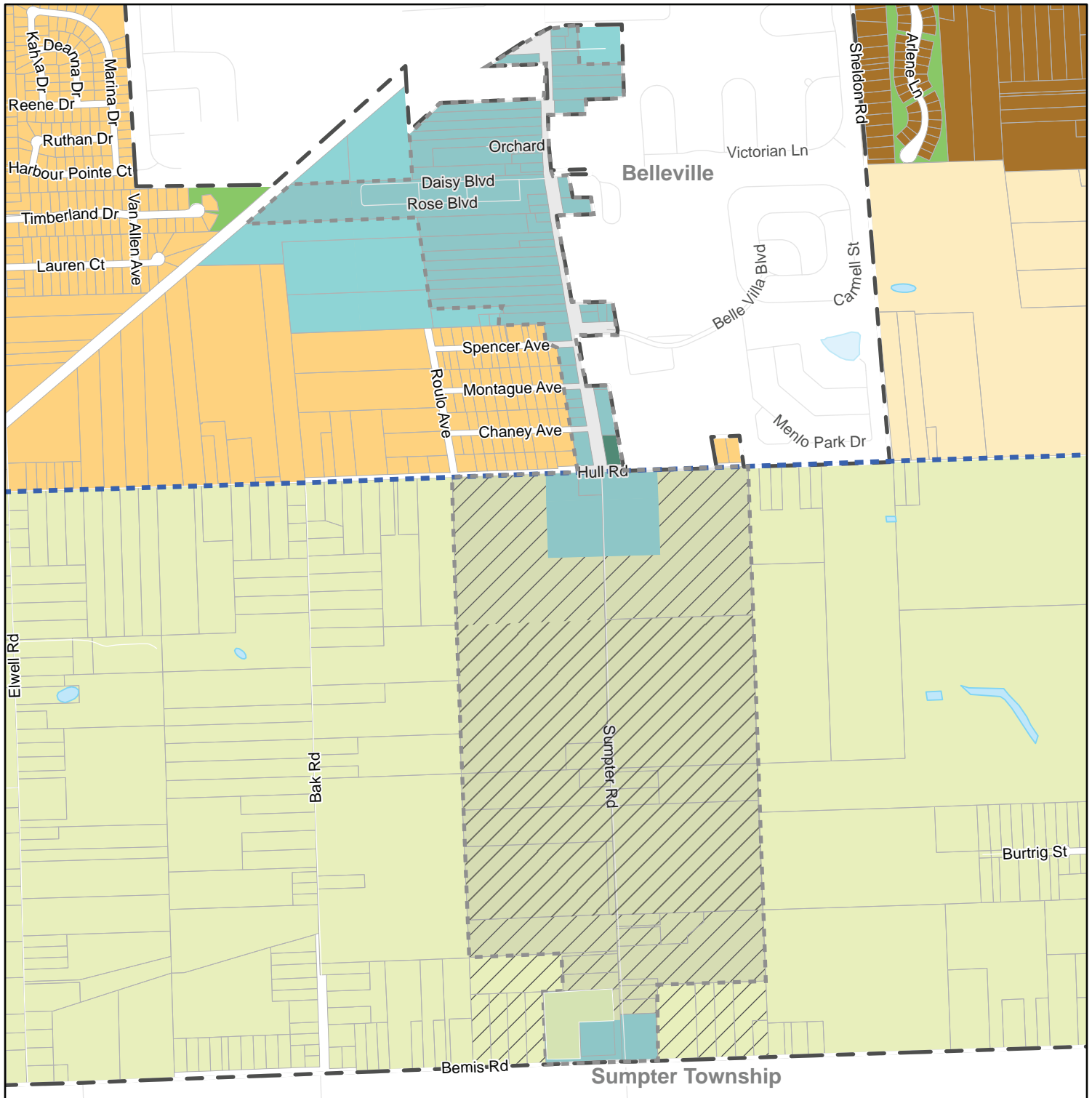
CORRESPONDING ZONING

SMU

SEE ALSO

2020 Van Buren Township Master Plan

South Side Master Plan



Sumpter Road Future Land Use Plan

Van Buren Township, Wayne County, MI

January 11, 2022 - DRAFT

LEGEND

- Rural Settlement
- Low Density Single Family A - 20,000 Sq. ft.
- Medium Density Single Family A - 10,000 Sq. ft.
- Village Residential - 8,400 Sq. Ft.
- Sumpter Mixed Use
- Public / Semi-Public
- Parks / Open Space
- Private Open Space
- Lakes and Rivers
- Urban Services Boundary
- Township Boundary
- Sumpter Road Overlay District Boundary
- Rural Residential Pattern Area



0 400 800
Feet

Basemap Source: Michigan Center for
Geographic Information, v. 17a.
Data Source: Van Buren Township 2021,
McKenna 2021.





Achieving the Vision for the Sumpter Road corridor must involve regulations that allow flexibility in development of new buildings. Importantly, the corridor is a fine-grained environment: a particular mix of buildings may have different results at various points along the corridor, and a form-based code that recognizes this must be flexible and easy to administer. To this end, a set of Building Types and Lot Types is presented here. Individual lots along the corridor are given one of four classifications, and within each classification a subset of appropriate Building Types is permitted, either as a Principal, Accessory, or Special Land Use. Some Lot Types are divided into “layers”, with differing subsets of Building Types permitted at the fronts versus at the rears.

It is anticipated that the following library of Building and Lot Types will constitute the Regulating Plan and Architectural Standards sections of a form-based code for a Sumpter Mixed Use District. Fitted together, they allow new development along Sumpter Road to take place in a manner consistent with the Vision, and in a way that is predictable to landowners, residents, and developers alike.

BUILDING TYPES

Accessory Dwelling Unit (ADU)



1. Ann Arbor, MI
2. Ross Chapin "Justenuf" Studio building type
3. Zionsville, IN

The Accessory Dwelling Unit is a building that provides space for one additional residential unit, or a home office or studio, separate from the principal residence. It may also contain vehicle storage spaces.

Artisan Studio



1. Village Arts Factory, Cherry Hill Village, MI
2. Sandhill Crane Vineyards, Jackson, MI
3. Woodstock, NY School of Art

The Artisan Studio is a flexible building type that accommodates entrepreneurial activities including culinary, winemaking, craft fabrication, and other light-footprint industries. The Village Arts Factory, shown above, is discussed in the Case Study on Page 21.

Artist Cottage



1. Art Farm at Serenbe, GA
2. Baldwin, MI
3. Ross Chapin "Blue Sky Cabin" building type

The Artist Cottage is a small-floorplate building, resembling a residential cottage. It is intended to provide space for production of small goods that do not require electrical, mechanical, or plumbing equipment or fixtures of a grade atypical for residential use.

Barn-Style



1. Brick Walker Barn, Napoleon, MI
2. Dexter Cider Mill, Dexter, MI
3. Hickory Creek Winery, Buchanan, MI

The Barn-style building type is intended in this Plan to accommodate non-agricultural uses, such as retail farm markets, event spaces, and wineries. Light storage and workshop uses may also be appropriate if not the principal use.

Cottage Retail



1. Ada, MI General Store
2. Dixboro, MI General Store
3. Glen Haven, MI General Store

The Cottage Retail building provides a public-facing retail shopfront on a residential-style building. It may function as a live-work building, or as a single- or mixed-use building. Historically, these buildings functioned as General Stores for small hamlets across Michigan. The examples above are all currently used as retail stores catering to modern consumers.

Detached Garage



1. Ross Chapin, "Dine's Point Garage" building type
2. Ross Chapin, "Tomcat Garage" building type
3. Hometown Structures

The Detached Garage is an accessory building intended for vehicular storage associated with a residential unit on the same lot.

Liner Building



1. Alys Beach, FL
2. Kingston, WA
3. Mashpee Commons, Mashpee, MA

The Liner Building is a small retail building which is inexpensive to construct, and is shallow in depth (typically 20-30 feet deep). It is designed to conceal large parking lots while providing pedestrian-friendly commercial spaces along edges.

Maker Space



1. Ann Arbor T-Shirt Company, Ann Arbor, MI
2. American Equipment Co. Building, Detroit, MI

The Maker Space is a building that combines a public-facing retail shopfront with an adjacent light manufacturing space. It may contain direct vehicular access to the interior via a garage bay. In the first two examples above, the adjacent manufacturing spaces are used to produce apparel and brew beer for sale on the premises.

Mixed Use



1. Ada, MI
2. New Boston, MI
3. Agricole Farm Stop, Chelsea, MI

The Mixed Use building is a traditional component of communities large and small in North America. Retail shopfronts typically occupy the ground floor, with office and/or residential lofts typically located on the second floor. The example from Chelsea, Michigan, contains a small grocery store specializing in locally-grown food.

Retail



1. Shops at the Green, Dixboro, MI
2. Jefferson Market, Ann Arbor, MI
3. East Hills Center, Grand Rapids, MI

The Retail Building contains one or several shopfronts that function as retail, professional service, or office spaces. It is designed to appeal to shoppers arriving on foot, with large display windows and appealing, pedestrian-scaled façades.

Studio Shed / Roadside Stand



1. Chris' Roadside Stand, Algonac, MI
2. Ocean East Construction
3. Winona Lake, IN

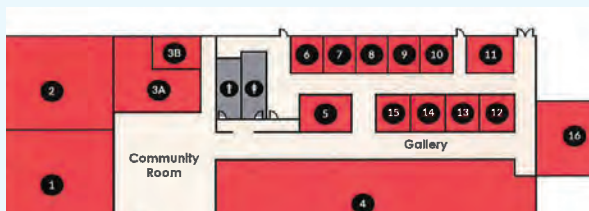
The Studio Shed/Roadside Stand is intended for the sale and production of goods on a seasonal or intermittent basis. These structures are permanent or semi-permanent, and are easily adaptable for other uses. They are "drive up" in nature, rather than "drive through" - the former does not require large expanses of pavement for queuing, nor are orders taken via intercom systems.

Workshop



1. Ross Chapin, "Greenwood Studio" building type
2. Stockton, NJ
3. Wholesale Direct Carports

The Workshop is an accessory building similar in nature to the Detached Garage, but does not exceed 5,000 square feet in area. It is able to accommodate larger vehicles and a wider variety of very light manufacturing uses.



Case Study: Village Arts Factory

Located in the Cherry Hill Village district of Canton Township, Michigan, the Village Arts Factory is an adaptive reuse of an industrial building dating to the 1950s. The building contains over 15 dedicated studio and workshop spaces occupied by artists, artisans, and entrepreneurs. In 2021, tenants included dance, fitness, and photography studios; cosmetics and soapmakers; and pottery, jewelry, and painters' studios.



Case Study: Bearclaw Coffee Co.

Bearclaw Coffee Company has three locations in southeast Michigan, each in a unique "drive up" format. The location pictured at left is located at a semi-rural crossroads near Dexter, Michigan. The small Roadside Stand format used by this company is compatible with the surrounding open spaces and larger lot residential uses in Dexter Township. The stand is sited on a modest gravel driveway, and has a small outdoor seating area.

ANCHOR LOT



Rendering of hypothetical Anchor Lot development looking northeast from the intersection of Sumpter and Bemis Roads.

Anchor Lots are those areas immediately adjacent to the intersections of Sumpter and Hull Roads on the south side, and Sumpter and Bemis Roads on the north side. These areas have potential for mixed-use development that creates nodes of activity and satisfies demand for goods and services.

Importantly, new development on these lots is intended to be anchored by buildings that are traditional in design, accommodating mixed-use, retail, and other neighborhood-serving uses. They may be complemented by accessory production or light manufacturing components at the rear.

Anchor Lots are not intended to be developed in a conventional suburban manner, nor should they be dominated by parking. Adequate landscaping and screening will be established on these lots, and additional screening standards are recommended where these lots are adjacent to or across from residential land uses or residentially zoned land.

The example rendering above depicts one potential vision for Anchor Lot development, looking to the northeast from the intersection of Sumpter and Bemis Roads.

ALLOWABLE BUILDING TYPES

Building Type	Front	Rear
Accessory Dwelling Unit (ADU)		
Artisan Studio		P
Artist Cottage		P
Barn-Style		A
Cottage Retail	P	
Detached Garage		
Liner Building	P	
Maker Space		
Mixed Use	P	
Retail	P	
Single Family Home		
Studio Shed / Roadside Stand	A	A
Workshop		A

P Principal Permitted Use
 A Accessory Permitted Use
 S Special Land Use

FRONTAGE LOT: DEEP



Rendering of hypothetical Deep Frontage Lot development on the west side of Sumpter Road, north of Hull Road.

Deep Frontage Lots are those lots that abut Sumpter Road and are generally greater than 300 feet deep. In their present form, these lots are predominantly developed as single-family houses, and the rear areas are underutilized.

Development on Deep Frontage Lots should preserve the existing uses nearer to Sumpter Road while allowing new uses to cluster toward the rear of the lots. Such development would only occur if new local streets were constructed further to the west of Sumpter Road to provide access.

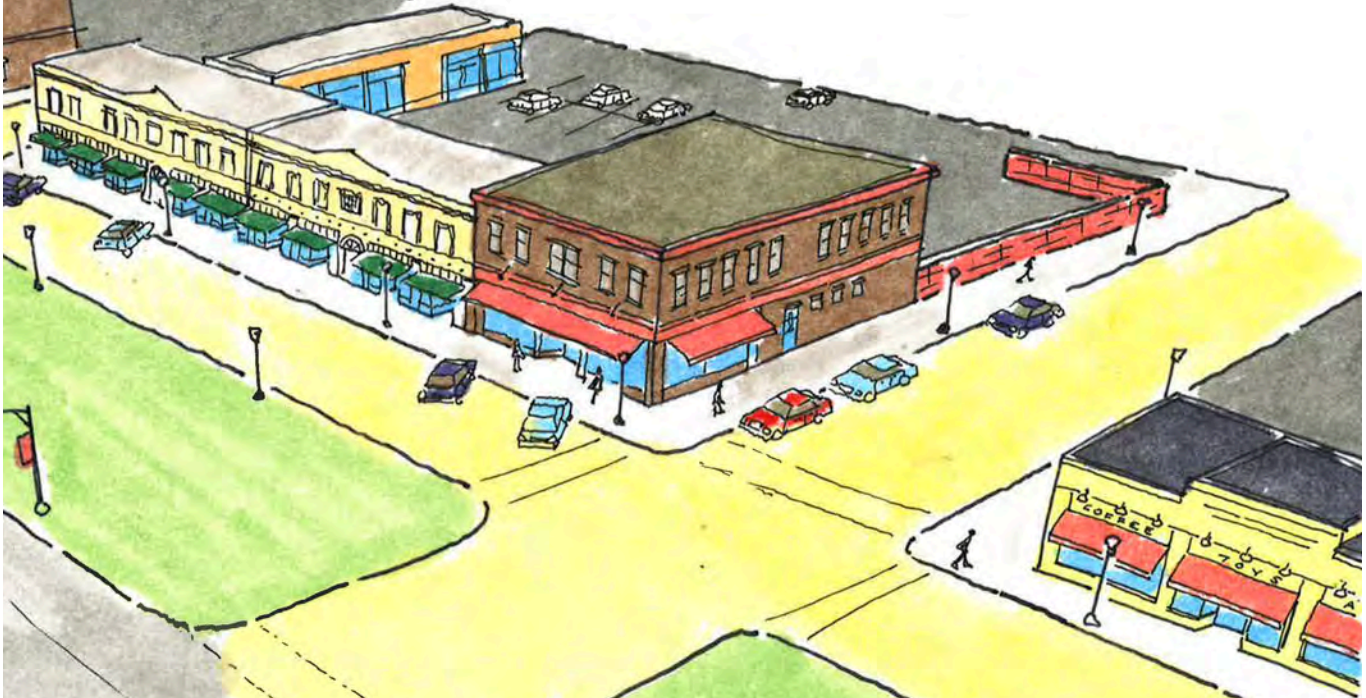
The “Front” and “Rear” designations in the table at right are based on the relationship to the road providing frontage to the property. For example, if a rear access road is constructed in the future, and a property is split such that one parcel retains access to Sumpter Road and one gains access to the new rear access road, “Front” building types will remain only those buildings closest to Sumpter Road.

ALLOWABLE BUILDING TYPES

Building Type	Front	Rear
Accessory Dwelling Unit (ADU)		A
Artisan Studio		P
Artist Cottage		P
Barn-Style		A
Cottage Retail	P	
Detached Garage	A	
Liner Building		
Maker Space		
Mixed Use		
Retail		
Single Family Home	P	
Studio Shed / Roadside Stand	S	
Workshop	S	A

P Principal Permitted Use **A** Accessory Permitted Use **S** Special Land Use

FRONTAGE LOT: SHALLOW



Rendering of hypothetical Shallow Frontage Lot development on the east side of Sumpter Road, north of Hull Road.

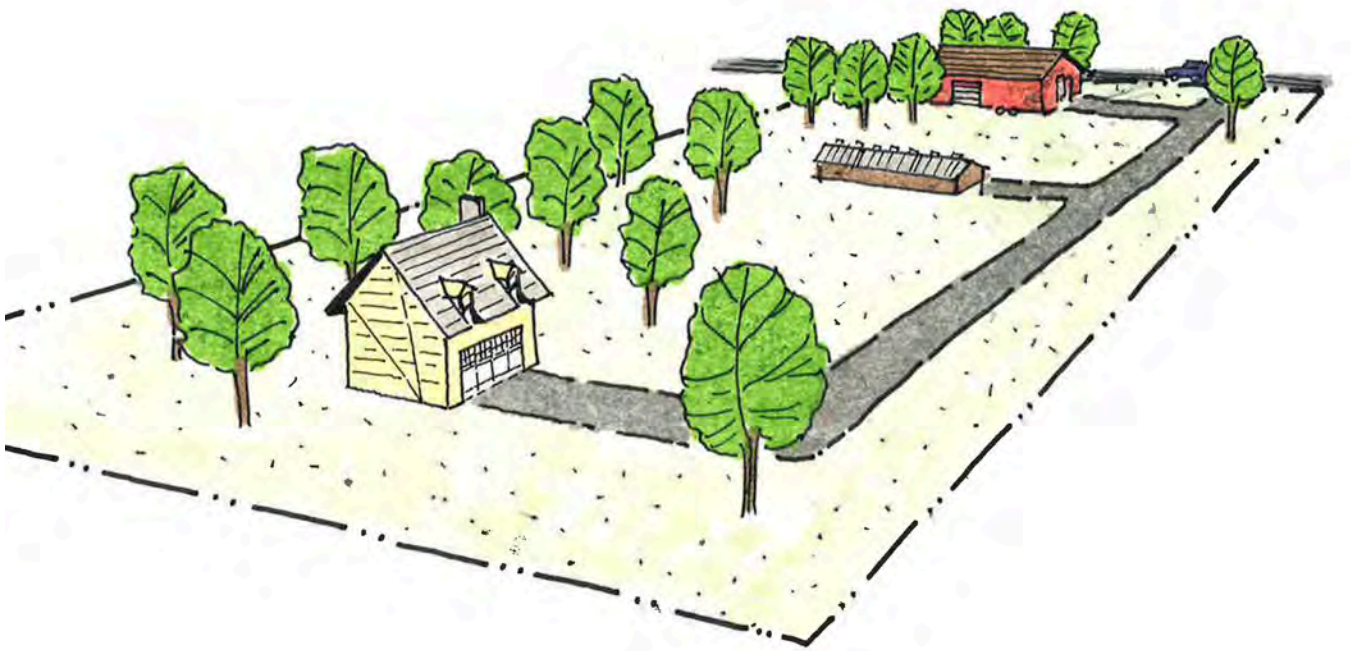
Shallow Frontage Lots are lots that abut Sumpter Road and generally have 300 feet or less of depth. These lots are currently occupied by a mixture of residential, commercial, and light industrial buildings.

Redevelopment of Shallow Frontage Lots should accommodate a variety of appropriately-scaled buildings, from single-family houses to new commercial and mixed-use buildings oriented toward pedestrians. In areas dominated by expanses of surface parking lots, liner buildings may be added to screen them and provide low-cost alternatives.

ALLOWABLE BUILDING TYPES

Building Type	
Accessory Dwelling Unit (ADU) <i>**behind principal structure</i>	A **
Artisan Studio	
Artist Cottage	
Barn-Style	
Cottage Retail	P
Detached Garage <i>**behind principal structure</i>	A **
Liner Building	P
Maker Space	S
Mixed Use	P
Retail	P
Single Family Home <i>*only if access to a residential side street exists</i>	P *
Studio Shed / Roadside Stand	
Workshop	
P Principal Permitted Use A Accessory Permitted Use S Special Land Use	

REAR LOT



Rendering of hypothetical Rear Lot development on parcels west of Sumpter Road that currently have no direct access.

Rear Lots are those lots currently inaccessible from public rights-of-way. They are exclusively located to the west of Sumpter Road, north of Hull Road. Future development on these lots is entirely dependent on the construction of new local streets to provide access.

New development in this area would be oriented toward artisinal, entrepreneurial, and very light industrial uses, clustered in smaller buildings, and preserving tree canopies where possible. Single-family houses and their accessory buildings could co-locate to create a mix of uses on individual lots.

ALLOWABLE BUILDING TYPES

Building Type	
Accessory Dwelling Unit (ADU)	
Artisan Studio	P
Artist Cottage	P
Barn-Style	A
Cottage Retail	P
Detached Garage	A
Liner Building	
Maker Space	
Mixed Use	
Retail	
Single Family Home	P
Studio Shed / Roadside Stand	S
Workshop	S
P Principal Permitted Use A Accessory Permitted Use S Special Land Use	

RURAL RESIDENTIAL PATTERNS

The areas designated Rural Settlement on the Future Land Use Map, overlaid with diagonal hashing, constitute the Rural Residential Pattern Area and are intended to accommodate residential land uses in a manner compatible with preserving surrounding open space and the existing rural atmosphere.

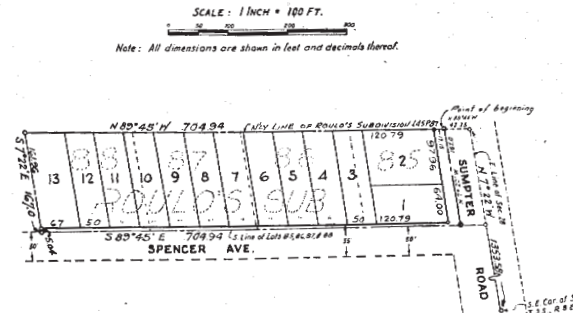
The AG zoning designation currently employed in this area allows development of single-family houses to take place at a density of 1 dwelling unit (du) per acre. If developed in this fashion, under a conventional large-lot subdivision pattern, the rural character so highly desired by the community would inevitably diminish (see illustration at top of facing page).

Therefore, a strategy must be employed to preserve the 1 du/acre development rights vested by the AG zoning district, shifting from a 1 du/acre gross density to a 1 du/acre net density. Put simply, measuring net density allows the same number of potential single-family houses to be built as with gross density - only they are clustered together on smaller lots, preserving the remainder of the land as open space.

There are many precedents for how to configure such a development. The Roulo Subdivision on the northwest corner of Sumpter and Hull Roads was platted in the 1920s, and was originally divided into 49 lots roughly .5 acre in size. Over time, these lots were split as demand grew (such as in the plat shown above), resulting in over 70 lots roughly .3 acres in size. The resulting gross density is on average 2 du/acre in the Roulo subdivision, and the corresponding net density would be, on average, greater (calculating net density in this case would only remove the area given over to local streets).

Clustering homes together in walkable neighborhoods has modern precedent as well. The New Neighborhood at Empire, in Leelenau County, Michigan (pictured below) was developed after 2005, and follows a platting and development pattern employed in the original portion of the village. Lots are mostly 50 feet wide and vary from 100 to 150 feet in depth (.12 to .2 acres), creating a human-scaled neighborhood fabric that is a natural extension of the historic village form.

REMUS SUBDIVISION OF A PART OF THE S. E. ¼ OF SECTION 28, T. 3 S., R. 8 E., VAN BUREN TOWNSHIP, WAYNE COUNTY, MICHIGAN



Using a conventional model of rural residential development at 1 du/acre, the open space immediately south of the intersection of Sumpter and Hull roads could potentially be developed in the manner at right, eliminating the open space that is greatly valued. This is not viewed as desirable.



By slightly modifying development standards, a greenway model could be employed. This model shifts to a net density of 1 du/acre, and preserves some open space. It is, however, conventionally suburban in nature and is less desirable in this area. Note the addition of a small Market on the Anchor Lot.

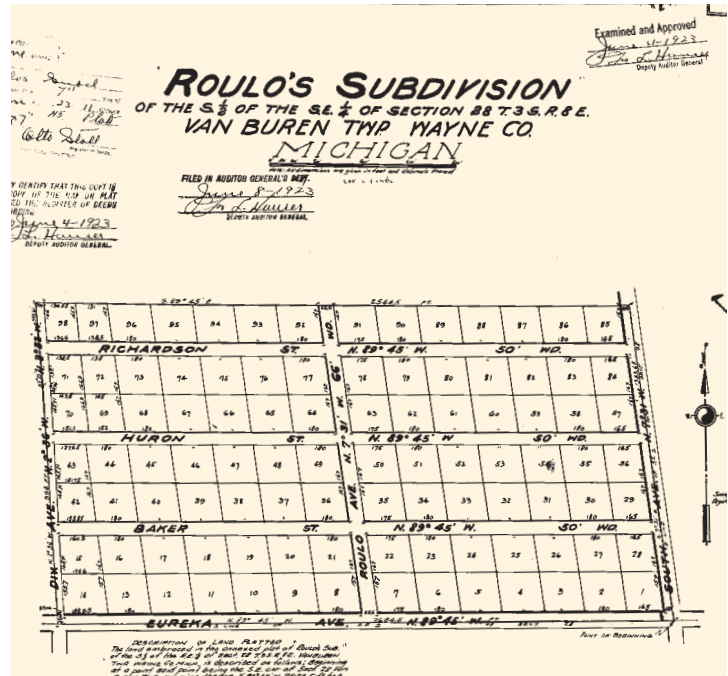


When residential development is clustered at a net density of 1 du/acre with lots averaging 0.3 acres in size, the surrounding open space is preserved in a contiguous whole, and neighborhood development takes on a form similar to that of the Roulo Subdivision immediately to the north.



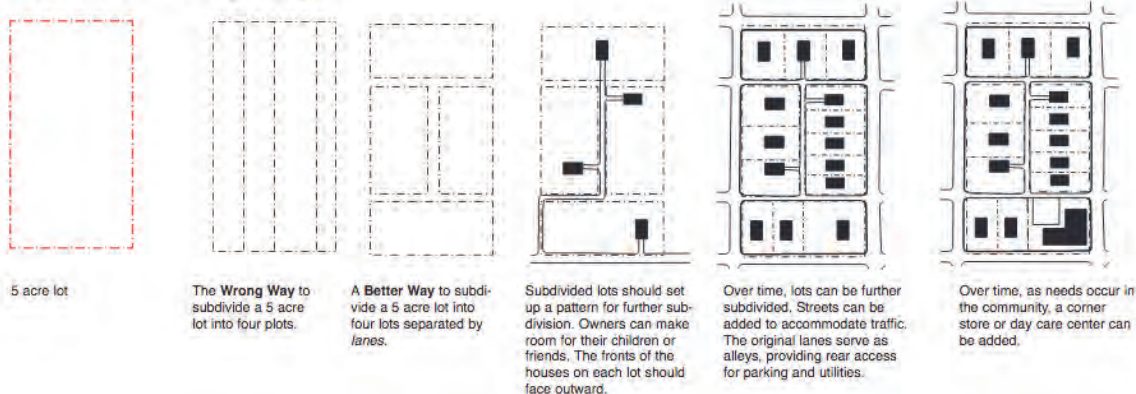
General Recommendations for Rural Residential Development

- Residential development within areas designated Rural Settlement on the Future Land Use Map that directly abut areas designated Sumpter Mixed Use shall preserve open space to the maximum extent possible, and accommodate a sense of rural character in their overall design.
- Internal street layout shall be grid-like in form, and individual lots shall be rectangular or square. Garages shall be accessed by rear alleys, and never be incorporated into the frontage of a house.
- Design of individual houses should borrow copiously from local and regional architectural precedents dating prior to the mid-20th Century. Houses should contain front porches and be based on a variety of models to avoid excessive duplication on a given block.
- Desired residential layouts will be articulated through zoning ordinance recommendations in a proposed Sumpter Road Overlay District (SROD) that will include access management and landscaping/buffering recommendations for the corridor. The Township's Planned Residential Development (PRD) District will govern overall site design standards.



P.J. Roulo submitted the above plat to divide the land at the northwest corner of Sumpter and Hull Roads in 1923. The individual lots were generally 180 feet wide by 167 feet deep, equating to about 0.5 acres. As demand for housing in the area grew, many (but not all) lots were split into 0.3 acre parcels. The general dimensions and context of the Roulo subdivision evoke a semi-rural character that can be emulated with similarly dimensioned new development.

EXAMPLE: Subdividing a Large Lot



This diagram, taken from a 2001 plan for Collier County, Florida by Dover-Kohl and Associates, demonstrates how large agricultural parcels can be divided and developed over time to create a human-scaled hamlet.

MOBILITY PLAN

The efficiency of an area's circulation system is an important component of planning for an efficient and adaptable land use pattern. A road system designed around the principles of a connected network can adapt to accommodate future development patterns as identified in this plan, while ensuring that local traffic has a choice of alternative routes in the immediate area - lessening the burden on Sumpter Road.

The Sumpter Road Corridor Area includes Sumpter Road, classified as an urban minor arterial road; Hull Road and Bemis Road, classified as urban local roads; and Bak Road and residential streets within the Roulo Subdivision, which are classified as urban local roads.

The Plan will include a potential future Railroad Street, generally paralleling the Norfolk Southern railroad right-of-way, as well as potential northerly extensions of Roulo Road and/or Bak Road. These extensions would provide access to future development in currently-landlocked parcels, or access to the rear of deep lots that front the west side of Sumpter Road. Where these potential road extensions are identified, the road design standard will be based on the anticipated demand generated by the proposed development that the road supports. Van Buren Township and Wayne County will need to work closely to identify the paving, width, drainage, and other design characteristics of these roads. It is important that these roads be designed to accommodate local users, and have design characteristics tending toward those of local access roads in rural areas, and distinctly away from those of typical suburban collector roads.

Slip streets may be developed parallel to, and on the east side of, Sumpter Road. These are intended to be low-speed, one-way roadways, that provide direct access to fronting properties. These slip streets will be encouraged to be developed as properties are developed over time, as a means to provide substantial separation between driveways directly accessing Sumpter Road (185-300' for a 35-45 MPH roadway), and to provide a buffer of landscaping and physical separation from the main roadway for people who walk or cycle alongside the east side of the corridor. This landscaped buffer between the main roadway and slip street can lend a smoother transition between the semi-rural residential character that defines the west side of Sumpter Road in this area, and the more intense land uses to the east.





Sumpter Road Mobility Plan

Van Buren Township, Wayne County, MI

February 4, 2022- DRAFT

Legend

- 1 - Potential Railroad Street
- 2 - Potential Roulo Avenue Extension
- 3 - Potential Bak Road Extension
- 4 - Potential Slip Street on East Side of Sumpter Road
- A - Potential Class 1 Bikeway / Shared-Use Path
- Parcel Boundaries
- Municipal Boundaries



0 300 600
Feet

Basemap Source: Michigan Center for Geographic Information, v. 17a.
Data Source: Van Buren Township 2021, McKenna 2021.





Sumpter Road Lot Types

Van Buren Township, Wayne County, MI

February 4, 2022- DRAFT

Legend

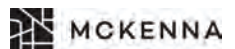
- Anchor Lots
- Rear Lots
- Frontage Lots (Deep)
- Frontage Lots (Shallow)

- Boundaries
- Municipal Boundaries




0 300 600
Feet

Basemap Source: Michigan Center for Geographic Information, v. 17a.
Data Source: Van Buren Township 2021, McKenna 2021.



ZONING PLAN

The land use classifications on the Future Land Use Map, and their corresponding descriptions, set land use policy to guide future development in the Township. The Future Land Use Plan provides general land use development principles that are consistent with the Township’s Goals and Objectives for growth. The official Zoning Map displays precise boundaries, correlated with standards and permitted uses that are adopted as law.

Future Land Use Designation	Corresponding Zoning District(s)	Zoning District Revisions (see notes on page 68)
 Sumpter Road Mixed Use	<input type="radio"/> SMU <input type="radio"/> SROD <input checked="" type="radio"/> AG	<i>New Districts – See Notes 1 and 2</i>

RECOMMENDED REVISIONS TO THE ZONING ORDINANCE AND ZONING MAP

The following revisions to the Zoning Ordinance and Map are recommended:

1. Develop new, form-based regulations for a **Sumpter Mixed Use District (SMU)**. The regulations should incorporate building types standards substantially similar to the precedent buildings illustrated in this Plan, and contain a Regulating Plan that assigns Lot Types in a manner substantially similar to those described in this Plan. A Table of Permitted Uses, by-right and by Special Land Use approval, will be created as part of this effort. Uses will include a mix of Residential, Office, Commercial, and light production uses. The SMU District will also include regulations pertaining to setbacks, alignment and spacing of buildings, in the same manner as the existing Mixed-Use (MU) District in the Township Zoning Ordinance.
2. Create a **Sumpter Road Overlay District (SROD)** that will encompass all parcels abutting Sumpter Road, and can apply to additional areas as needed in the future. Design standards regulated in the SROD will encompass:
 - Access management standards, including future provision for a local slip street on the east side of Sumpter Road north of Hull Road accompanying new development or redevelopment, as shown on the Mobility Plan Map;
 - Guidelines for future local access streets, strictly limited to those locations shown on the Mobility Plan Map;
 - Non-motorized transportation, including separate facilities for pedestrians and bicyclists as shown on the Mobility Plan Map;
 - Architectural standards, emphasizing traditional building materials such as brick and masonry block, and transparent windows;
 - Lighting, including street lighting and decorative lighting fixtures;
 - Landscaping, including street trees, buffering vegetation, swales, and plantings that accent public spaces;
 - Fencing, including outdoor accent walls and parapet walls.

PLAN IMPLEMENTATION

The recommendations of this Plan will be implemented through a collaborative effort between Township staff, the Planning Commission, and the Township Board, with input from Sumpter Road Corridor-area stakeholders.

The primary tool for implementing the plan will be an amended Township Zoning Ordinance which includes the Sumpter Road Mixed Use (SMU) Zoning District and Sumpter Road Overlay District (SROD). The first step of the Plan's implementation will be for the Planning Commission to review, refine and make recommendations on these Zoning Ordinance amendments and for the Township Board of Trustees to adopt them.

The implementation of the Plan will then proceed gradually over the next 20 years. Each private development or redevelopment decision and public improvement in the area encompassed by this Plan should then be made consistent with both the applicable zoning, and the overall Vision for the area as expressed in this Plan.

Under Michigan's Planning Enabling Act, the Master Plan recommendations will be reviewed every five years. Five years after adoption, development trends or other factors may warrant a reconsideration of certain aspects of the Plan.

Article 2 Definitions

Section 2.101 General

When not inconsistent with the context, words used in the present tense include the future tense, words need in the singular number include the plural number and words need in the plural number include the singular number. The word “shall” is always mandatory and not merely directory. Terms not herein defined shall have the meanings customarily assigned to them.

Section 2.102 Specific Terms

ACCESSORY DWELLING UNIT. A secondary house or apartment used for residential purposes that shares the building lot of a larger, primary home which is detached from the larger, primary home.

APARTMENT OR APARTMENT UNIT: A renter-occupied dwelling unit that is contained within a building which is not a single-family dwelling, such as a duplex, a mixed use building, cottage retail building, apartment house, or multiple-family high rise dwelling.

BREW PUB: An establishment associated with a license issued by the State of Michigan that authorizes the licensee to brew up to 18,000 barrels of beer per calendar year for sale on the premises and off the premises with a Specially Designated Merchant license.

COTTAGE / ARTISANAL MANUFACTURING includes the following uses and any other similar uses conducted entirely within a completely enclosed building:

- a. The assembly, fabrication, manufacture, compounding, processing, packaging, or treatment of such products as food or beverage products, hardware, toiletries, musical instruments, optical goods, toys, rubber stamps and other small molded rubber products, novelties, electrical instruments, pottery, figurines, and other ceramic products using only previously pulverized clay, where these products are not intended for mass production;
- b. The manufacture, compounding, assembling, fabrication, packaging, or treatment of custom products, articles, or merchandise not intended for mass production from the following previously prepared materials: bone, canvas, cellophane, cloth, cork, feathers, felt, fiber, fiberglass, fur, glass, hair, horn, leather, paper, plastics, metals, stone, shell, textiles, tobacco, wax, wire, wood (excluding saw and planing mills), and yarn;

MANUFACTURING AND PROCESSING (LIGHT): Includes the following uses and any other similar uses conducted entirely within a completely enclosed building:

- a. The assembly, fabrication, manufacture, compounding, processing, packaging, or treatment of such products as cutlery, food products, hardware, pharmaceuticals, toiletries, musical instruments, optical goods, toys, rubber stamps and other small molded rubber products, novelties, electrical instruments (e.g., electric or neon signs, appliances, computers, radios, phonographs, televisions and video recorders) and pottery, figurines, and other ceramic products using only previously pulverized clay;
- b. The manufacture, compounding, assembling, fabrication, packaging, or treatment of products, articles, or merchandise from the following previously prepared materials: bone, canvas,

Sumpter Road Corridor Zoning Ordinance Text Amendments
DRAFT, July 19, 2022

cellophane, cloth, cork, feathers, felt, fiber, fiberglass, fur, glass, hair, horn, leather, paper, plastics, precious or semi-precious metals or stone, shell, textiles, tobacco, wax, wire, wood (excluding saw and planing mills), and yarn;

- c. Tool and die shops, metal working machine shops involving the use of grinding or cutting tools, manufacturing shops for tools, dies, jigs, and fixtures, the manufacture, compounding, assembling, fabrication, packaging or treatment of products, articles or merchandise from sheet metal (excluding large stamping such as automobile fenders and bodies), and the manufacture or assembly of light sheet metal products including heating and ventilating equipment, cornices, eaves and gutters; and
- d. Publishing, printing, forming of boxes and cartons and manufacturing of cardboard products.

MICRO BREWERY: A manufacturer of beer which manufactures 60,000 or fewer barrels of beer a year. Can sell beer to Wholesaler licensees to sell to retailers. May also sell beer to consumers under an On-Premises Tasting Room Permit at the location where it manufactures beer. May also self-distribute beer it manufactures to retailers if it sells fewer than 2,000 barrels of beer a year. No local legislative approval required for licensure, if manufacturing only; On-Premises Tasting Room Permit requires local legislative approval.

RESTAURANT, CARRY-OUT: An establishment where food is prepared and served to a customer solely for the consumption off the premises.

RESTAURANT, DRIVE-IN OR DRIVE-THRU: An establishment where food is prepared and served on the premises for the consumption within automobiles or an establishment with combined drive-in and sit-down facilities.

RESTAURANT, SIT-DOWN: An establishment where food is prepared and served for consumption within the principal building, with or without carry-out facilities, including delicatessens, bakeries, and coffee shops.

SERVICE ROAD. Low-speed, one-way roadways adjacent and parallel to a major roadway which provide direct access to properties fronting on the major roadway.

SMALL DISTILLER: A manufacturer of spirit products that contain more than 10% alcohol by volume which manufactures 60,000 or fewer gallons of spirits a year. Can sell spirits it manufactures to the Commission to sell through Authorized Distribution Agents to retailers. May self-distribute spirits it manufactures to retailers if it sells less than 3,000 gallons of spirits a calendar year to retailers. May sell spirits it manufactures to customers under an On-Premises Tasting Room Permit at the location where it manufactures it and also through an Off-Premises Tasting Room license or Joint Off-Premises Tasting Room license. No local legislative approval required for licensure, if manufacturing only; On-Premises Tasting Room Permit requires local legislative approval.

SMALL WINE MAKER: A manufacturer of wine which manufactures 50,000 or fewer gallons of wine a year. Can sell wine to Wholesaler licensees to sell to retailers. May also sell wine to consumers under an On-Premises Tasting Room Permit at the location where it manufactures it and also through an Off-Premises Tasting Room license or Joint Off-Premises Tasting Room license May self-distribute wine it manufactures to retailers. May hold a Farmer's Market Permit that allows the sale and sampling of its wine at a farmer's market. No local legislative approval required for licensure, if manufacturing only; On-Premises Tasting Room Permit requires local legislative approval.

Sumpter Road Corridor Zoning Ordinance Text Amendments
DRAFT, July 19, 2022

Section 3.104. Permitted Uses by District

The following [Table 1](#) lists the permitted uses and special land uses in each district. Refer to [Article 2](#) for a description of the uses listed in the following [Table 1](#).

Whenever a specific development standard is included for a particular use in *Table 1*, any development must comply with the requirements of the referenced section. All development standards for specific uses are listed in [Article 5](#).

Refer to the footnotes to the Table of Permitted Uses and Special Land Uses in [Section 3.105](#). Footnotes applicable to each zoning district are indicated in parentheses after the zoning district title in the [Table 1](#).

Finally, refer to [Article 6](#) for applicable requirements not listed in [Table 1](#).

Table 1: Table of Permitted Land Uses and Special Land Uses by District

Key:	■ Principal Permitted Use	● Special Land Use	[blank] Use Not Permitted													
USE	R-1A, R-2A, R-1B, and R-1C	RM (A)	RMH	AG	C (B), (C)	C-1 (C)	C-2 (C)	FS	OT (D)	M-1 (E)	M-T (F)	M-2	AP (G)	BRMU	SRMU	DEVELOPMENT STANDARD
RESIDENTIAL USES																
Accessory Caretaker Dwelling					●	●	●	●	●	●	●	●	●			Section 5.117
Apartment Houses		●												●		Section 5.103
Apartments														■	■	
Multiple Family High Rise Dwelling		●														Section 5.103
Single-Family Attached Dwelling		■												■	■	Section 5.116
Single-Family Detached Dwelling	■			■										■	■	Section 5.114
Single-Family Detached Dwelling in RM		■														Section 5.114
Single-Family Farm Dwelling Related to Agricultural Operations				■												
Mobile Home Parks			■													Section 5.126
Mobile Home Subdivisions			■													Section 5.127
Planned Residential Developments	●			●										●		
Two-Family Dwelling		■												■	■	
Accessory Dwelling Unit															■	
LODGING USES																
Bed and Breakfast	●													●	●	Section 5.106
Motels and Hotels						■	■	■						■		Section 5.121
OFFICE USES																
Financial Institution, No Drive-Thru					■	■	■		■					■	■	
Financial Institution, With Drive-Thru						●	●		●							Section 5.105
Financial Institution with Drive-Thru in rear only															●	Section 5.105
High Tech, Data Processing, and Computer Center									■	■		■				

Sumpter Road Corridor Zoning Ordinance Text Amendments
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Key: ■ Principal Permitted Use ● Special Land Use

[blank]
Use Not
Permitted

USE	R-1A, R-2A, R-1B, and R-1C	RM (A)	RMH	AG	C (B), (C)	C-1 (C)	C-2 (C)	FS	OT (D)	M-1 (E)	M-T (F)	M-2	AP (G)	BRMU	SRMU	DEVELOPMENT STANDARD
Offices of Manufacturing Agents, Sales Representatives, and Others Requiring Display Area and Limited Warehousing								■								Section 5.128
Office, Medical or Dental					■	■	■		■					■	■	
Office, Professional					■	■	■		■					■	■	
Veterinary/Animal Clinics and Hospitals				●		■	■			■				■		
Veterinary/Animal Clinics and Hospitals (no breeding, boarding or training)															■	
COMMERCIAL USES																
Automobile Rental and Leasing Agencies						●	●									Section 5.104
Automobile Wash Establishment, Automatic						●	●			●						
Automobile Wash Establishment, Self-Serve										■						
Brewpub														●	●	
Contractor Establishment, Indoor						■	■							■	■	
Drive-In Theaters							■			●		●				Section 5.113
Event Facilities					■	■	■									
Gasoline Filling Stations						●	●	●								
Gasoline Service Stations								●								
Greenhouses and Nurseries				■	●	●	●							●	●	Section 5.119
Grocery Store					■	■	■							■	■	
Health or Exercise Club or Spa						■	■							■	■	
Mini-Warehouse (Self Storage Facility)						●	●									Section 5.125
Mortuary Establishments						■	■							■	■	
Outdoor Storage of Building or Contracting Equipment and Supplies						●	●			●		●			●	Section 5.132
Open Air Business Uses						●	●									Section 5.129
Outdoor Vehicle Sales						●	●									Section 5.133
Planned Shopping Centers						●	■							●		Section 5.134
Printing Shops and Establishments						■	■		■					■	■	
Private Clubs						■	■			●				■	●	Section 5.135
Private Indoor Instructional Institutions					■	■	■							■	■	
Public Auction Rooms						■	■							■	■	
Recreational Vehicle Storage Yards						●	●			●		●				Section 5.129
Regulated Uses (Tattoo establishments, pawnshops, pool and billiard halls, and massage parlors)										●						Section 5.138
Regulated Uses (Sexually Oriented Businesses)												●	●			Section 5.138
Restaurant, Drive-Thru or Drive-In						●	●									Section 5.136
Restaurant, Carry-Out						■	■	■		■				■	■	
Restaurant, Outdoor Dining						■	■							■	■	Section 5.137
Restaurants, Sit-Down (no entertainment)					■	■	■	■		■				■	■	
Restaurant, Sit-Down (with entertainment)						■	■	■		■				■	■	
Restaurant with walk-up window															■	
Retail Sales, Indoor					■	■	■	■		■				■	■	

Sumpter Road Corridor Zoning Ordinance Text Amendments
DRAFT, July 19, 2022

Key: ■ Principal Permitted Use ● Special Land Use

[blank]
Use Not
Permitted

USE	R-1A, R-2A, R-1B, and R-1C	RM (A)	RMH	AG	C (B), (C)	C-1 (C)	C-2 (C)	FS	OT (D)	M-1 (E)	M-T (F)	M-2	AP (G)	BRMU	SRMU	DEVELOPMENT STANDARD
Service Establishment, Office, Showroom, or Workshop (No Retail Required)					■	■								■	■	
Service Establishment, Office, Showroom, or Workshop (Retail Required)					■	■	■							■	■	
Service Establishment, Personal Services					■	■	■			■				■	■	
Studios					■	■								■	■	
Truck Stops								○								
Vehicle Service, Major										○		○				Section 5.141
Vehicle Service, Minor						○	○									Section 5.141
Vehicle Showrooms					■	■										
Wholesale Sales						○	■			■	■	■				
INDUSTRIAL USES																
Accessory Outdoor Industrial Storage										■	■	■				Section 5.101
Air Freight Forwarders												○				
Cottage or Artisanal Manufacturing															■	
Distribution Centers											○	■				Section 5.112
Instructional Services, Outdoor									○							
Junk Yards												○				Section 5.122
Laboratories, Minor						■	■		■	■	■	■		■	■	
Laboratories, Major										■	■	■				
Manufacturing and Processing, Light										■	■	■				
Manufacturing and Processing, Heavy												■				
Micro Brewery														○	○	
Retail Dry Cleaning Plants and Laundries										■		■				
Small Distiller														○	○	
Small Wine Maker														○	○	
Truck Repair and Maintenance Facility, Major												○				
Truck Repair and Maintenance Facility, Minor										○		○				
Truck and Railroad Terminals												■	■			
Warehousing (Excluding Outdoor Storage and Distribution Center)							■									
Warehousing (Excluding Distribution Center)										■	■	■				
COMMUNITY, EDUCATION, AND INSTITUTIONAL USES																
Adult Day Care Center	○	○	○	○	○	○	○							○	○	Section 5.110
Adult Foster Care, Family Home	■	■		■										■	■	
Adult Foster Care, Large Group Home		○												○	○	
Adult Foster Care, Small Group Home		○												○	○	
Bus Passenger Station						■	■	■						■		
Cemeteries				■												
Child Care Centers	○	○	○	○	○	○	○							○	○	Section 5.108
Day Care or Child Care, Family Home	■	■	■	■										■	■	
Day Care or Child Care, Group Home	○	○		○										○	○	Section 5.111
Hospitals		■												○	○	Section 5.120
Senior Housing			○	○		○	○	○						○	○	Section 5.143
Places of Assembly	○	○	■	○		■	■		○					■	■	Section 5.139
Public Buildings and Uses	■				○	■	■		○					■	■	

Sumpter Road Corridor Zoning Ordinance Text Amendments
DRAFT, July 19, 2022

Key: ■ Principal Permitted Use ● Special Land Use

[blank]
Use Not
Permitted

USE	R-1A, R-2A, R-1B, and R-1C	RM (A)	RMH	AG	C (B), (C)	C-1 (C)	C-2 (C)	FS	OT (D)	M-1 (E)	M-T (F)	M-2	AP (G)	BRMU	SRMU	DEVELOPMENT STANDARD
Public utility buildings, telephone exchange buildings, electric transformer stations and substations and gas regulator stations (excluding storage yards) when necessary to serve the immediate vicinity	●	■	■	○	○	■	■			■	■	■		○	○	
Public utility buildings, telephone exchange buildings, electric transformer stations and substations and gas regulator stations (including storage yards) when necessary to serve the immediate vicinity										■	■	■				
Religious Institutions	○	○	■	○		■	■		○					■	■	Section 5.139
School, College or University and Public or Non-Profit	■	■	■		■	■	■		■					■	■	
School, College or University, Private						■	■		■					■	■	
School, Primary or Secondary	■	■	■		■	■	■							■	■	
School, Vocational or Technical						■	■		■						■	
RECREATION USES																
Campgrounds			■			○	○									Section 5.107
Country Clubs	○			○												
Golf Courses	○			○												Section 5.118
Horses for Personal, Non-Commercial Use	■			■												Section 5.123
Indoor Recreation						○	■			■		■		○	○	
Outdoor Recreation, Amusement						○	○									Section 5.130
Outdoor Recreation, Golf Driving Range	○			○												Section 5.131
Outdoor Recreation, Gun Club				○												Section 5.131
Outdoor Recreation, Private Park	○			○												Section 5.131
Private Clubhouse		■												○		
Private Swimming Pools	■	■		■										■	■	Section 7.206
Publicly-Owned Recreational Facilities	■								■					■	■	
ANIMAL AND AGRICULTURAL USES																
Agricultural Retail Sales				○												Section 5.102
Commercial Agriculture Operations and Buildings				■												
Keeping of Pets and Livestock				■												Section 5.123
Kennels and Raising of Fur Bearing Animals				○												Section 5.124
Public and Private Stables and Riding Academies				■												
Temporary Produce Sales Building				■												Section 5.140
Truck Gardening				■												
AIRPORT USES																
Airports and Related Facilities														■		
Assembly and fabrication plants which use an airplane taxiway from the main airport runway directly to the manufacturing firm.														■		
Airport Commercial and Service Establishments														■		

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Key: ■ Principal Permitted Use ● Special Land Use

[blank]
Use Not
Permitted

USE	R-1A, R-2A, R-1B, and R-1C	RM (A)	RMH	AG	C (B), (C)	C-1 (C)	C-2 (C)	FS	OT (D)	M-1 (E)	M-T (F)	M-2	AP (G)	BRMU	SRMU	DEVELOPMENT STANDARD
Transportation facilities including truck and rail terminals, bus depots, and similar uses													■			
Laboratories Related to Aviation Industry													■			
Package Expediting Services													■			
Wholesaling and Warehousing Establishments Requiring Air Transport													■			
OTHER USES																
Commercial Radio and Television Towers						●	●									Section 5.109
Drive-Thru Facility (accessory to any principal use)						●	●									
Home Occupations	■			■										■	■	
Mining, excavating, or other removal of sand, earth, minerals, or other materials naturally found in the earth				●								●				
Off-Street Parking Lots (Principal Use)													■			
Parking Garages								■					■	■	■	
Wireless Communication Facilities	●	●	●	●	■	■	■	■	●	■	■	■	■			Section 5.142

Section 3.105. Footnotes to the Table of Permitted Uses and Special Land Uses
by District

...

(A) Required Conditions of the BRMU, Belleville Road Mixed Use District

1. Apartments in the BRMU Zoning District are only permitted within building types specified in Section 3.119(F). Minimum floor area per dwelling unit (sq. ft.) in apartment units shall be the same as for apartment units in the RM zoning district as labeled in Section 4.102, Table 2.

(B) Required Conditions of the SRMU, Sumpter Road Mixed Use District

1. Apartments in the SRMU Zoning District are only permitted within building types specified in Section 3.121(F). Minimum floor area per dwelling unit (sq. ft.) in apartment units shall be the same as for apartment units in the RM zoning district as labeled in Section 4.102, Table 2.
2. Industrial uses must be at least 200' from any residential zoning district unless the Planning Commission finds that a modification from this requirement is justified due to the character of the surrounding land uses and the nature of the specific industrial use.
3. Within 100' of any residential zoning district, buildings with commercial uses are subject to the regulations of section 3.110(D) (required conditions of the C, Local Business District), unless the Planning Commission finds that a modification from this requirement is justified due to the character of the surrounding land uses and the nature of the specific commercial use.
4. No senior housing building shall exceed 5,000 square feet.
5. Commercial, community, educational and institutional uses in the SRMU zoning district are subject to the greenbelt buffering requirements for C, C-1, C-2, FS, and OT districts as described in Section 10.103(E) of this ordinance.
6. Industrial uses in the SRMU zoning district are subject to the greenbelt buffering requirements for M-1 districts as described in Section 10.103(E) of this ordinance.

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Section 3.110 C, Local Business District

(A) STATEMENT OF PURPOSE
The Local Business District intended to permit retail business and service uses which are needed to serve nearby residential areas. In order to promote such business development, uses are permitted which would not create hazards, offensive and loud noises, vibration, smoke, glare or excessive truck traffic. The intent of this district is also to encourage the concentration of local business in appropriate locations for the mutual benefit of businesses and patrons. It is intended that marginal strip business development along major streets be discouraged.

(B) PERMITTED USES	(C) SPECIAL LAND USES
<ul style="list-style-type: none"> • Service establishment, personal services • Service establishment, office, showroom or workshop (retail required) • Office, Professional • Office, Medical and Dental • Grocery store • Restaurants, Sit Down with no entertainment • Retail Sales, Indoor • School (Primary or Secondary) • School (College or University, Public or Non-Profit) • Private indoor instructional institutions. • Adult day care centers • Wireless Communication Facilities • Banks and Financial Institutions, No Drive-Thru • Accessory structures and uses customarily incidental to the above permitted uses 	<ul style="list-style-type: none"> • Publicly owned buildings and uses • Public utility buildings, telephone exchange buildings, electric transformer stations and substations and gas regulator stations but not including storage yards, when necessary to serve the immediate vicinity. • Child care centers • Greenhouses and Nurseries • Accessory Caretaker Dwelling • Senior Housing

The above list is a summary of uses permitted by right or special land use approval in the district. Refer to [Section 3.104](#) (including footnotes) for standards and requirements applicable to permitted and special land uses. In case of a conflict between the above list and the uses listed in [Section 3.104 \(including footnotes\)](#), then [Section 3.104](#) shall prevail. Refer to [Article 2](#) for definitions of uses and refer to [Article 5](#) for development standards for specific uses.

(D) REQUIRED CONDITIONS OF THE C, LOCAL BUSINESS DISTRICT
<ul style="list-style-type: none"> • Restrictions on Uses: All permitted uses shall be subject to the following restrictions: <ul style="list-style-type: none"> ○ All business or service establishments shall be for the purpose of dealing directly with consumers. All goods produced or processed on the premise shall be sold at retail on premises where produced and/or processed for customers of the premises. ○ All business activities, including servicing and processing, except for off-street parking or loading, shall be conducted within completely enclosed building. ○ No drive-in or drive-through uses shall be permitted. ○ No use involving the sales of alcohol for consumption on premises shall be permitted. ○ No meat or poultry stores where slaughtering is done on the premises shall be permitted. • Buildings. Buildings in the Local Business District shall be subject to the following limitations: <ul style="list-style-type: none"> ○ Building for Permitted Uses. No single use may occupy a building or portion of a building greater than five thousand (5,000) square feet and no building occupied by more than one (1) use may be larger than ten thousand (10,000) square feet. ○ Other Buildings. Building including one (1) or more uses permitted by right and/or permitted after special approval, which are larger in area than those permitted above shall be subject to special land use review and the following provisions: <ul style="list-style-type: none"> • Such building shall be located on a site not less than three (3) acres in area. • No single commercial use may occupy a building or portion of a building greater than ten thousand (10,000) square feet. • No commercial use building occupied by more than one (1) use may be larger than thirty thousand (30,000) square feet. <p>No main or accessory building shall be situated less than fifty (50) feet from any residential district, or from any property line which abuts a residential district or use.</p>

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- The maximum size of a building used for non-commercial or residential uses shall be dictated by compliance with all applicable ordinance regulations for setbacks, parking, greenbelts and other site design requirements.
- **Fabrication and Storage Areas.** Any such building and use shall not have more than forty percent (40%) of the floor area therein devoted to fabricating or storage areas.
- **Garages.** Garages shall be permitted only when used exclusively for the storage of passenger motor vehicles and/or commercial vehicles of not more than a gross vehicle weight rating of ten thousand (10,000) pounds for use in connection with the primary use.

(E) ARCHITECTURAL STANDARDS APPLICABLE TO ALL COMMERCIAL ZONING DISTRICTS

All buildings in commercial zoning districts shall meet the following requirements:

- Brick or another material of similar architectural quality and appearance as determined by the Planning Commission, shall be the principal material for exterior building surfaces.
- Buildings shall incorporate roof articulation, projection and relief, covered walkways into facades which include entrances or display windows or which are adjacent to parking or visible from rights-of-way.

(F) DIMENSION REGULATIONS (C, LOCAL BUSINESS DISTRICT)

Lot Standards		Minimum Setbacks	
<i>Min. Lot Area (sq. ft.)</i>	--	<i>Front Yard</i>	75 (N)
<i>Min. Lot Width (ft.)</i>	--	<i>Side Yard (one)</i>	15 (M) , (P)
<i>Min. Lot Depth (ft.)</i>	--	<i>Side Yard (total of 2)</i>	15 (M)
<i>Max. Lot Coverage (%)</i>	--	<i>Rear Yard</i>	25
<i>Min. Floor Area/Unit (ft.)</i>	--		
<i>Max. Building Height (ft.)</i>	40		
<i>Max. Building Height (stories)</i>	4		

Footnotes: Refer to [Section 4.102](#) wherever a footnote is referenced in parentheses after one of the design regulations.

Section 3.119 BRMU, Belleville Road Mixed-Use District

(A) STATEMENT OF PURPOSE
<p>The Belleville Road Mixed-Use District intended to encourage and facilitate redevelopment by implementing the following mixed-use policies of the 2020 Van Buren Township Master Plan and Belleville Road District Plan and Market Analysis:</p> <ol style="list-style-type: none"> 1. Mix of Land Compatible Land Uses. Permit a range of compatible land uses, such as residential (from single-family to multi-family), public, institutional, office, retail, personal services uses, and appropriate general business uses. 2. Walkability. Create a walkable, pedestrian-oriented development that does not conflict with motorized traffic. 3. Building Location and Site Design. Ensure that buildings have a strong relationship to the street by requiring development to be human-scale through appropriate building location and site design, including developing areas that include civic spaces and pedestrian amenities and requiring on-street parking along interior streets. 4. Use of Buildings. Allow compatible mixed uses to be located in a single building. 5. Land Assembly for Development. Because frontage land on major roads tends to have higher property values, the mixed-use standards in this Section create a strong economic incentive to combine shallower frontage land with land behind the frontage. By allowing lots to extend deeper into frontage property, typical strip development will be discouraged and a more sustainable mix of land uses will be permitted.

(B) PERMITTED USES		(C) SPECIAL LAND USES
<ul style="list-style-type: none"> • Apartments¹ • Single-Family, Attached Dwelling • Single-Family, Detached Dwelling • Two-Family Dwelling • Hotels and Motels • Financial Institution, No Drive-Through • Office, Medical or Dental • Office, Professional • Veterinary/Animal Clinics and Hospitals (no breeding, boarding, or training) • Contractor Establishment, Indoor • Grocery Store • Health or Exercise Club or Spa • Mortuary Establishment • Printing Shops and Establishments • Private Clubs • Private Indoor Instructional Institutions • Public Auction Rooms • Restaurant, Carry-Out • Restaurant, Outdoor Dining • Restaurant, Sit-Down (with or without entertainment) • Retail Sales, Indoor • Service Establishment, Office, Showroom, or Workshop (No Retail Required) 	<ul style="list-style-type: none"> • Service Establishment, Office, Showroom, or Workshop (Retail Required) • Service Establishment, Personal Services • Studios • Laboratories, Minor • Adult Foster Care, Family Home • Bus Passenger Station • Day Care Home, Family Home • Places of Assembly • Public Buildings and Uses • Religious Institutions • School (College or University, Public or Non-Profit) • School (College or University, Private) • School (Primary or Secondary) • Private Swimming Pools • Publicly-Owned Recreational Facilities • Home Occupations • Parking Garages • Accessory structures and uses customarily incidental to the above permitted uses 	<ul style="list-style-type: none"> • Apartment Houses • Planned Residential Developments • Bed and Breakfast • Greenhouses and Nurseries • Planned Shopping Centers • Adult Day Care Center • Adult Foster Care, Large Group Home • Adult Foster Care, Small Group Home • Child Care Centers • Day Care Home, Group • Hospitals • Public utility buildings, telephone exchange buildings, electric transformer stations and substations and gas regulator stations (excluding storage yards) when necessary to serve the immediate vicinity • Indoor Recreation • Private Clubhouse • Brewpub <ul style="list-style-type: none"> • Micro Brewery • Small Wine Maker • Small Distiller •

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The above list is a summary of uses permitted by right or special land use approval in the district. Refer to [Section 3.104](#) (including footnotes) for standards and requirements applicable to permitted and special land uses. In case of a conflict between the above list and the uses listed in [Section 3.104](#) (including footnotes), then [Section 3.104](#) shall prevail. Refer to [Article 2](#) for definitions of uses and refer to [Article 5](#) for development standards for specific uses.

1. *Apartments in the BRMU Zoning District are only permitted within building types specified in Section 3.119(F). Minimum floor area per dwelling unit (sq. ft.) in apartment units shall be the same as for apartment units in the RM zoning district as labeled in Section 4.102, Table 2.*

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Section 3.121. SRMU, Sumpter Road Mixed-Use District

(A) STATEMENT OF PURPOSE
<p>The Sumpter Road Mixed-Use District is intended to encourage contextually appropriate development and redevelopment by implementing the following mixed-use policies of the 2020 Township Master Plan, Sumpter Road Corridor Plan, and South Side Master Plan:</p> <p>1. Mix of Land Compatible Land Uses. Permit a range of compatible land uses within the semi-rural context of the Sumpter Road corridor, such as residential (from single-family to multi-family), public, institutional, retail, personal services, and appropriate light industrial uses.</p> <p>2. Accommodate All Modes of Transportation. Create walkable, pedestrian-oriented development that does not conflict with motorized traffic and does not inhibit the safety and comfort of non-automobile users of the corridor.</p> <p>3. Building Location and Site Design. Encourage pedestrian-scaled development through appropriate building location and site design. Accommodate the varying size of land parcels in the District by regulating these elements relative to their location on a given parcel.</p> <p>4. Use of Buildings. Allow compatible mixed uses to be located in a single building.</p> <p>5. Land Assembly for Development. Because frontage land on major roads tends to have higher property values, the mixed-use standards in this Section create a strong economic incentive to combine shallower frontage land with land behind the frontage. By allowing lots to extend deeper into frontage property, typical strip development will be discouraged and a more sustainable mix of land uses will be permitted. Clustering of new residential development with open space preservation will be encouraged.</p>

(B) PERMITTED USES		(C) SPECIAL LAND USES
<p>RESIDENTIAL USES</p> <ul style="list-style-type: none"> • <u>Apartments¹</u> • <u>Dwelling, Single-Family Residential Attached</u> • <u>Dwelling, Single-Family Residential Detached</u> • <u>Two-Family Dwelling</u> • <u>Home Occupations</u> • <u>Accessory Dwelling Unit</u> <p>OFFICE USES⁵</p> <ul style="list-style-type: none"> • <u>Financial Institution with no Drive-Thru</u> • <u>Office, Medical or Dental</u> • <u>Office, Professional</u> • <u>Veterinary/Animal Clinics and Hospitals (no breeding, boarding, or training)</u> <p>COMMERCIAL USES^{3, 5}</p> <ul style="list-style-type: none"> • <u>Contractor Establishment, Indoor</u> • <u>Grocery Store</u> • <u>Health or Exercise Club or Spa</u> • <u>Mortuary Establishment</u> • <u>Printing Shops and Establishments</u> • <u>Private Indoor Instructional Institutions</u> • <u>Public Auction Rooms</u> • <u>Restaurant, Carry-Out</u> • <u>Restaurant, Sit-Down</u> • <u>Restaurant with walk-up window</u> • <u>Retail Sales, Indoor</u> • <u>Service Establishment, Office, Showroom, or Workshop (No Retail Required)</u> • <u>Service Establishment, Office, Showroom, or Workshop (Retail Required)</u> 	<ul style="list-style-type: none"> • <u>Service Establishment, Personal Services</u> • <u>Studios</u> <p>INDUSTRIAL USES^{2, 6}</p> <ul style="list-style-type: none"> • <u>Laboratories, Minor</u> • <u>Cottage or Artisanal Manufacturing</u> <p>COMMUNITY, EDUCATIONAL AND INSTITUTIONAL USES⁵</p> <ul style="list-style-type: none"> • <u>Adult Foster Care, Family Home</u> • <u>Day Care Home, Family Home</u> • <u>Places of Assembly</u> • <u>Public Buildings and Uses</u> • <u>Religious Institutions</u> • <u>School (College or University, Public or Non-Profit)</u> • <u>School (College or University, Private)</u> • <u>School (Primary or Secondary)</u> • <u>School, Vocational or Technical</u> <p>RECREATION USES⁵</p> <ul style="list-style-type: none"> • <u>Private Swimming Pools</u> • <u>Publicly-Owned Recreational Facilities</u> <p>OTHER USES</p> <ul style="list-style-type: none"> • <u>Home Occupations</u> • <u>Parking Garages</u> 	<p>LODGING USES⁵</p> <ul style="list-style-type: none"> • <u>Bed and Breakfast</u> <p>OFFICE USES⁵</p> <ul style="list-style-type: none"> • <u>Financial Institution-with drive-through in rear only</u> <p>COMMERCIAL USES^{3, 5}</p> <ul style="list-style-type: none"> • <u>Greenhouses and Nurseries</u> • <u>Brewpub</u> • <u>Restaurant, Outdoor Dining</u> • <u>Private Clubs</u> <p>INDUSTRIAL USES^{2, 6}</p> <ul style="list-style-type: none"> • <u>Outdoor Storage of Building or Contracting Equipment and Supplies</u> • <u>Micro Brewery</u> • <u>Small Wine Maker</u> • <u>Small Distiller</u> <p>COMMUNITY, EDUCATIONAL AND INSTITUTIONAL USES⁵</p> <ul style="list-style-type: none"> • <u>Senior Housing⁴</u> • <u>Adult Day Care Center</u> • <u>Adult Foster Care, Large Group Home</u> • <u>Adult Foster Care, Small Group Home</u> • <u>Child Care Centers</u> • <u>Day Care Home, Group</u> • <u>Hospitals</u> • <u>Public utility buildings, telephone exchange buildings, electric transformer stations and substations and gas regulator stations (excluding storage yards) when necessary to serve the immediate vicinity</u> <p>RECREATION USES</p> <ul style="list-style-type: none"> • <u>Indoor Recreation</u>

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	<u>Accessory structures and uses customarily incidental to the above permitted uses</u>	
<p><i>The above list is a summary of uses permitted by right or special land use approval in the district. Refer to Section 3.104(including footnotes) for standards and requirements applicable to permitted and special land uses. In case of a conflict between the above list and the uses listed in Section 3.104 (including footnotes), then Section 3.104 shall prevail. Refer to Article 2 for definitions of uses and refer to Article 5 for development standards for specific uses. Use categories in Section 3.121(F) (Sumpter Road Mixed Use District – Building Type Standards) refer to the use categories (e.g., “Residential”) as detailed above. At the discretion of the Planning Commission, additional unlisted uses may be considered if they adhere to the building types in Section 3.121(G) and are justified due to the character of the surrounding land uses. At the Planning Director’s discretion, additional unlisted uses may be referred for special land use review under Article 12, Chapter 3 of this ordinance.</i></p>		

(D) FOOTNOTES AND REQUIRED CONDITIONS OF THE SRMU, SUMPTER ROAD MIXED USE DISTRICT

1. Apartments in the SRMU Zoning District are only permitted within building types specified in Section 3.121(F). Minimum floor area per dwelling unit (sq. ft.) in apartment units shall be the same as for apartment units in the RM zoning district as labeled in Section 4.102, Table 2.
2. Industrial uses must be at least 200’ from any residential zoning district unless the Planning Commission finds that a modification from this requirement is justified due to the character of the surrounding land uses and the nature of the specific industrial use.
3. Within 100’ of any residential zoning district, buildings with commercial uses are subject to the regulations of section 3.110(D) (required conditions of the C, Local Business District), unless the Planning Commission finds that a modification from this requirement is justified due to the character of the surrounding land uses and the nature of the specific commercial use.
4. No senior housing building shall exceed 5,000 square feet.
5. Subject to the greenbelt buffering requirements for C, C-1, C-2, FS, and OT districts as described in Section 10.103(E) of this ordinance.
6. Subject to the greenbelt buffering requirements for M-1 districts as described in Section 10.103(E) of this ordinance.

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(E) STREET TYPE STANDARDS

All streets adjacent to and within the Sumpter Road Mixed-Use District shall meet the following requirements when a site is developed under the standards of this District. If the street is existing and does not meet the following requirements, it must be upgraded to the extent possible

<u>Street Type Name</u>	<u>Street Easement or Right-of-Way Width</u>	<u>Street Pavement Width</u>	<u>Location</u>	<u>On-Street Parking</u>	<u>Minimum Required Driveway Spacing</u>
<u>Service Road</u> ¹	<u>40 ft.</u>	<u>32 ft.</u>	<u>Immediately east of and parallel to Sumpter Road, north of Hull Road as identified in the Mobility Plan within the 2022 Sumpter Road Corridor Plan. Intended for one-way, northbound traffic.</u>	<u>Yes – Diagonal, one side</u>	<u>See Article 9</u>
<u>Local Street Extended</u>	<u>66 ft.</u>	<u>30 ft.</u>	<u>West of and parallel to Sumpter Road as identified in the 2022 Sumpter Road Corridor Plan.</u>	<u>Yes – Variable configuration</u>	<u>See Article 9</u>
<u>Railroad Street</u>	<u>66 ft.</u>	<u>36 ft.</u>	<u>South of and generally parallel to the Norfolk Southern railroad right-of-way as identified in the 2022 Sumpter Road Corridor Plan.</u>	<u>Yes – Variable configuration</u>	<u>See Article 9</u>
<u>Local Arterial</u>	<u>Variable</u>	<u>Variable</u>	<u>Hull Road and Sumpter Road</u>	<u>Variable</u>	<u>See Article 9</u>

¹– As regulated in Section 9.106(F) of the Zoning Ordinance.

(F) ALLOWABLE MIXED-USE LOT TYPES¹

All existing or new platted lots within the Sumpter Road Mixed-Use District shall be classified as one of the following subareas, illustrated on the Regulating Plan (Section 3.121(l)):

Anchor Lots (A).

Rear Lots (RL).

Frontage Lots (Deep) (FL-D)

Frontage Lots (Shallow) (FL-S)

¹ Definitions and characteristics of the identified lot types are included in the 2022 Van Buren Township Sumpter Road Corridor Plan and illustrated in the Regulating Plan in 3.121(l) of this Ordinance. Individual properties' lot type classifications may be altered by conditions such as parcel divisions or combinations or the creation of new access to lots which change their classification. Such conditions shall be reviewed as needed by the Township Planning and Economic Development Director at the time of an application for development or redevelopment or at the time of a proposed property division or combination.

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(G) BUILDING TYPE SCHEDULE OF REGULATIONS						
Building Type Name	Front Setback	Side Setback	Rear Setback	Height	Allowable Lot Types – Front Locations	Allowable Lot Types – Rear Locations
<u>Mixed Use</u>	0 ft. along <u>Service Road</u> Max. 54 ft. otherwise	10' ¹	Min. 25 ft. or height of building, whichever is greater	Max. 36 ft./ 3 Stories	A FL-S	
<u>Retail</u>	0 ft. along <u>Service Road</u> Max. 54 ft. otherwise	10' ¹	Min. 25 ft. or height of building, whichever is greater	Max. 18 ft./1 story	A FL-S	
<u>Accessory Dwelling Unit</u>	Must be located at rear of principal residential unit; min. 10 ft. separation	5 ft.	Min. 25 ft. or height of building, whichever is greater	Max. 24 ft.		FL-D (Accessory) FL-S
<u>Liner</u>	0 ft. along <u>Service Road</u> Max. 54 ft. otherwise	10' ¹	Min. 25 ft. or height of building, whichever is greater	Max. 30 ft./ 2 Stories	A FL-S	
<u>Cottage Retail</u>	Max. 54 ft.	10' ¹	Min. 25 ft. or height of building, whichever is greater	Max.: 24 ft./ 2 Stories	A FL-D FL-S RL	
<u>Barn-Style</u>	Must be located at rear of principal residential unit; min. 50 ft. separation	5 ft.	Min 5 ft.	Max. 36 ft.	RL (Accessory)	A (Accessory) FL-D (Accessory)
<u>Duplex</u>	Min. 30 ft. Porches may encroach up to 12 ft. into setback	Min. 10 ft. one side / 25' two sides.	Min. 25 ft. or height of building, whichever is greater	Max. 24 ft.	FL-D FL-S ²	
<u>Single-Family House</u>	Min 30 ft.; Porches may encroach up to 12 ft. into setback	Min. 10 ft. one side / 25' two sides.	Min. 25 ft. or height of building, whichever is greater	Max. 24 ft.	FL-D FL-S ² RL	
<u>Detached Garage</u>	See Article 7, Chapter 2 (Accessory Structures and Uses)				FL-D (Accessory) FL-S (Accessory) RL (Accessory)	
<u>Workshop</u>	If located at rear of principal building: min. 10 ft. separation, Max. 54' otherwise	10' ¹	Min. 25 ft. or height of building, whichever is greater	Max. 18 ft.	RL (Special Land Use)	A (Accessory) FL-D (Special Land Use)
<u>Artisan Studio</u>	If located at rear of principal residential unit: min. 10 ft. separation, max. 54' otherwise	10' ¹	Min. 25 ft. or height of building, whichever is greater	Max. 18 ft.		A FL-D
<u>Artist Cottage</u>	If located at rear of principal residential unit: min. 10 ft. separation, max. 54' otherwise	10' ¹	Min. 25 ft. or height of building, whichever is greater	Max. 18 ft.	RL	A FL-D
<u>Maker Space</u>	0 ft. along <u>Service Road</u> , Max. 54' otherwise	10' ¹	Min. 25 ft. or height of building, whichever is greater	Max. 18 ft.	FL-S (Special Land Use) RL	
<u>Studio Shed / Roadside Stand</u>	No closer to front lot line than principal building	Min. 5 ft.	Min. 5 ft.	Max. 18 ft.	A (Accessory) FL-D (Special Land Use) RL (Special Land Use)	A (Accessory)

¹A minimum side yard setback of 10' is required, unless the Planning Commission finds that there is justification for modification from this minimum side yard setback based on the availability of access to the parcel and neighboring parcels, such as via a rear alley or cross access easement provided to or from a neighboring parcel.

²Only permitted in this lot type where there is direct access to a residential street.

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- (H) **BUILDING TYPE STANDARDS.** A range of Building Types permitted in the Sumpter Road Mixed-Use District are described and illustrated in this section. Each Building Type contains a mix of allowable uses by floor, with retail always at the ground floor level of non-residential buildings. Building frontage orientation is coordinated with street frontages in the Building Type illustrations. Such orientation of building fronts and frontages must be followed with the use of each Building Type, though architectural variation that achieves these objectives is permitted. The determination of building type shall be made by the Township's Planning Director at the time of site development or redevelopment. Upon request, prior to administrative or Planning Commission site plan approval, the Planning Director's interpretation of a building type may be appealed for architectural interpretation to the Township's Planning Commission via a written request for an appeal by the applicant to be considered at a regular meeting of the Planning Commission. If such appeal is requested, other property owners sharing a common property line with the lot or owning any lot within three (3) lots of the property directly across the street on the opposite side of the street, without regard to intersecting street lines, shall be notified regarding the proposed building type, the uses permitted under this ordinance within the proposed building type, and the option to appeal the Planning Director's interpretation of a building type to the Planning Commission at a regular meeting of the Planning Commission.

1. BUILDING TYPE STANDARDS: MIXED USE BUILDING
<p>Description. The Mixed Use Building Type is a multi-story Building Type with storefronts along all primary frontage lines. This Building Type is ideal for smaller-floorplate retail spaces that can flexibly accommodate the surrounding local trade area.</p> <p>Permitted Uses and Special Land Use. Ground floor permitted uses are restricted to non-residential uses listed in Section 3.121(B) and (C), while the upper floors may have any use permitted in the District , subject to Section 3.121(B) and (C)</p> <p>Parking. Parking in front of buildings shall be limited to a maximum of one single tier of parking and one maneuvering lane, set behind a greenbelt as required under Article 10 (Landscaping and Screening).</p>
<div style="display: flex; justify-content: space-around;">    </div>

2. BUILDING TYPE STANDARDS: RETAIL BUILDING
<p>Description. The Retail Building Type is a single-story, limited-use building with storefronts along all primary frontage lines. This Building Type is ideally configured for downtowns and in retail segments of downtown shopping streets, though they may also be used for single-use, suburban areas.</p> <p>Permitted Uses and Special Land Uses. Commercial and Office uses listed in Section 3.121 (B) and (C) only.</p>

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DRAFT, July 19, 2022

Parking. Parking in front of buildings shall be limited to a maximum of one single tier of parking and one maneuvering lane, set behind a greenbelt as required under Article 10 (Landscaping and Screening).



3. BUILDING TYPE STANDARDS: ACCESSORY DWELLING UNIT

Description. An Accessory Dwelling Unit exists as accessory to a Primary Dwelling Unit and must be located behind the corresponding Primary Dwelling Unit. Accessory Dwelling Units allow for site flexibility and increased density without changing site character. The Accessory Dwelling Unit minimum floor area is 500 sq. ft. for an efficiency unit, 700 sq. ft. for a one bedroom unit, 900 sq. ft. for a two-bedroom unit and 1,100 sq. ft. for a unit with three or more bedrooms.

Permitted Uses and Special Land Uses. Residential uses only.



4. BUILDING TYPE STANDARDS: LINER BUILDING

Description. The Liner Building Type is a Mixed Use or Retail Building Type that has been limited in depth to conceal parking behind. This Building Type is ideal for mid-block conditions, and adjacent to – or located toward the edge of – existing strip-type shopping centers.

Permitted Uses and Special Land Use. Commercial and Office uses listed in Section 3.121 (B) and (C) only.

Parking. Parking in front of buildings shall be limited to a maximum of one single tier of parking and one maneuvering lane, set behind a greenbelt as required under Article 10 (Landscaping and Screening).

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5. BUILDING TYPE STANDARDS: COTTAGE RETAIL BUILDING

Description. The Cottage Retail Building Type is a mixed-use building type with a residential building form that permits storefronts along all primary frontage lines and extends down the sides of the secondary frontage line. This building type emulates a building form that has evolved in traditional neighborhoods. It is a modestly-scaled building, including a gable and a pitched roof, usually with a storefront at the ground floor. Roofs must have a minimum slope of 4:12. Storefronts shall be located along all primary frontage lines and extend from a primary frontage a minimum of twelve (12) feet into any secondary frontage.

Permitted Uses and Special Land Use. Non-residential uses may only be permitted in the front of the building on the ground floor. Other areas of the building, including upper stories, are limited to Residential uses, in accordance with the uses listed in Section 3.121(B) and (C).

Parking. Parking in front of buildings shall be limited to a maximum of one single tier of parking and one maneuvering lane, set behind a greenbelt as required under Article 10 (Landscaping and Screening).



6. BUILDING TYPE STANDARDS: BARN-STYLE

Description. The Barn-style Building Type is an agricultural building type accessory to a Principal Building. The barn Building Type satisfies the functionality of a mix of building types and uses on a site without compromising the rural or residential character of the site. The design of the Barn Building Type includes a gambrel, gable, or shed roof profiles and all wood construction on a masonry foundation. Allowable Colors: Red, Brown, White. Allowable roof materials: Standing seam, natural finish.

Permitted Uses and Special Land Use. Non-residential uses as regulated under Section 3.121(B) and (C).

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7. BUILDING TYPE STANDARDS: DUPLEX BUILDING

Description. The Duplex Building Type is any independent building configuration consisting of exactly two residential units, usually surrounded by a private or common yard, meeting minimum room quantity and size requirements as dictated by the local building code, zoning, or both.

The units can be side-by-side (similar to rowhouses) or stacked one above the other. Similar to single-family residential buildings, duplex buildings include front porches that encroach into the established building setback and optional detached garages. A front porch of at least twelve (12) feet wide and seven (7) feet deep is required for new buildings. The minimum floor area in each duplex dwelling unit is 1,000 square feet. The duplex building type may contain certain non-residential uses but may not contain industrial uses.

Permitted Uses and Special Land Use. All non-industrial uses.

8. BUILDING TYPE STANDARDS: SINGLE-FAMILY HOUSE

Description. A Single-Family House Building Type exists as the Primary Dwelling Unit on its corresponding site and must be located forward of all other buildings on its site by a distance of minimum 10 feet. Minimum floor area is 1,250 sq. ft. The Single-Family Home Building Type preserves the existing residential character along Sumpter Road, while allowing for a mix of Building types and uses behind. New buildings should generally fit the style of Mid-Century American residential construction, such as Ranch, Prairie, Cape Cod, or Modern. Decks and patios must be to the side or rear of structures on new buildings. The single-family house building type may contain certain non-residential uses but may not contain industrial uses.

Permitted Uses and Special Land Use. All non-industrial uses.

9. BUILDING TYPE STANDARDS: DETACHED GARAGE

Description. The Detached Garage Building Type is accessory to a Primary Dwelling Unit. The Detached Garage allows for vehicular storage and accessory utility activities on a site. The detached garage will adhere to the same size, height, roof pitch, architectural, and setback standards as used for detached accessory buildings in residential zoning districts under Article 7, Chapter 2 (Accessory Structures and Uses).

Permitted Uses and Special Land Use. Vehicular Parking and residential storage.



10. BUILDING TYPE STANDARDS: WORKSHOP

Description. The Workshop Building Type is a utility building type. The design of the Workshop Building Type includes a one-story building which must consist of wood, brick, Portland Cement stucco, rock-face block or stone construction. Maximum building footprint is 5,000 sq. ft. and maximum building height is 18’.

Permitted Uses and Special Land Use. Uses other than Residential and Commercial Uses as listed in and subject to the regulations of Section 3.121(B) and (C).



11. BUILDING TYPE STANDARDS: ARTISAN STUDIO

Description. The Artisan Studio is a flexible building type that accommodates entrepreneurial activities including culinary, winemaking, craft fabrication, and other light-footprint industries. The design of the Artisan Studio Building Type includes a one-story building and must consist of wood, brick, Portland Cement stucco, rock-face block, or stone construction.

Permitted Uses and Special Land Use. Uses other than Residential Uses as listed in and subject to the regulations of Section 3.121(B) and (C).



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12. BUILDING TYPE STANDARDS: ARTIST COTTAGE

Description. The Artist Cottage is a small-floorplate building, resembling a residential cottage. It is intended to provide space for production of small goods that do not require electrical, mechanical, or plumbing equipment or fixtures of a grade atypical for residential use.

Permitted Uses and Special Land Use. Uses other than Residential Uses as listed in and subject to the regulations of Section 3.121(B) and (C).



13. BUILDING TYPE STANDARDS: MAKER SPACE

Description. The Maker Space is a building that combines a public-facing retail shopfront with an adjacent light manufacturing space. It may contain direct vehicular access to the interior via a garage bay.

Permitted Uses and Special Land Use. Uses other than Residential Uses as listed in and subject to the regulations of Section 3.121(B) and (C).

Parking. Parking in front of buildings shall be limited to a maximum of one single tier of parking and one maneuvering lane, set behind a greenbelt as required under Article 10 (Landscaping and Screening).



14. BUILDING TYPE STANDARDS: STUDIO SHED / ROADSIDE STAND

Description. The Studio Shed/Roadside Stand is intended for the sale and production of goods on a seasonal or intermittent basis. These structures are permanent or semi-permanent, and are easily adaptable for other uses. They are "drive up" in nature, rather than "drive through"- the former does not require large expanses of pavement for queuing, nor are orders taken via intercom systems.

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Permitted Uses and Special Land Use. Accessory to commercial uses listed in and subject to the regulations of Section 3.121(B) and (C).



Sumpter Road Corridor Zoning Ordinance Text Amendments
DRAFT, July 19, 2022

(I) REGULATING PLAN

A Regulating Plan designates the locations where different Building Type standards apply, based on the physical character of the area.









**Sumpter Road
Lot Types**

Van Buren Township, Wayne County, MI

February 4, 2022 - DRAFT

Legend

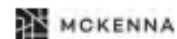
-  Anchor Lots
-  Rear Lots
-  Frontage Lots (Deep)
-  Frontage Lots (Shallow)

-  Parcel Boundaries
-  Municipal Boundaries



0 300 600
Feet

Base Map Source: Michigan Center for Geographic Information, v. 10
Data Source: Van Buren Township GIS, 2021



Article 4 Schedule of Regulations

Section 4.101 Statement of Purpose

The purpose of this Article is to provide area, height, and placement regulations for districts established by this Zoning Ordinance.

Section 4.102 Schedule of Regulations

Unless otherwise provided in this Ordinance, area, height and placement regulations under this Ordinance shall be in accordance with the Schedule of Regulations in [Table 2](#) and footnotes thereto, which schedule and footnotes hereby made a part of this [Section 4.102](#).

Table 2. Schedule of Regulations

Schedule of Regulations											
Zoning District	Lot Area, Lot Coverage, and Dwelling/Business Unit Standards					Minimum Yard Setbacks				Maximum Building Height	
	Minimum Lot Area (sq. ft.)	Minimum Floor Area Per Dwelling Unit (sq. ft.)	Minimum Lot Width (ft.)	Minimum Lot Depth (ft.)	Maximum Lot Coverage (%)	Front (ft.)	Side (ft.)	Total of 2 Sides (ft.)	Rear (ft.)	Feet	Stories
AG, Agricultural and Estates	43,560	2,000 (B)	150	(C)	15	50	10	25	35	30	2
R-1A, Single-Family Residential	20,000	1,800 (B)	100	(C)	15	30	10	25	35	30	2
R-2A, Single-Family Residential	15,000	1,800 (B)	90	(C)	20	30	10	25	35	30	2
R-1B, Single-Family Residential	10,000	1,500 (B)	80	125	30	30	10 (X)	25 (X)	35	30	2
R-1C, Single-Family Residential	8,400	1,250 (B)	70	120	30	30	10 (X)	25 (X)	35	30	2
RM, Multiple Family Residential	10 acres (Q)	See below	400	--	30	35	20 (F)	40 (F)	35	30	2.5
RM Detached Single-Family Residential, Edge Dwelling	7,200	1,250	60'	(C)	30	30	10'	20'	35	30	2 (Y)
RM Detached Single-Family Residential, Interior Dwelling	6,300	1,000	45'	(C)	30	30	5	15	25	30	2 (Y) , (Z) , (AA)
RM Attached Single-Family:											
Efficiency Units	4,200 (A)	500 (B)	(H)	(H)	--	(I)	(I)	(I)	(I)	30	2.5
One Bedroom Units (G)	4,200 (A)	700 (B)	(H)	(H)	--	(I)	(I)	(I)	(I)	30	2.5
Two Bedroom Units (G)	5,000 (A)	900 (B)	(H)	(H)	--	(I)	(I)	(I)	(I)	30	2.5
Three or More Bedroom Units (G)	6,200 (A)	1,100 (B) , (E)	(H)	(H)	--	(I)	(I)	(I)	(I)	30	2.5
RM Apartments:											
Efficiency Units	2,800 (A)	500 (B)	(H)	(H)	--	(I)	(I)	(I)	(I)	30	2.5
One Bedroom Units (G)	2,800 (A)	700 (B)	(H)	(H)	--	(I)	(I)	(I)	(I)	30	2.5
Two Bedroom Units (G)	3,500 (A)	900 (B)	(H)	(H)	--	(I)	(I)	(I)	(I)	30	2.5
Three or More Bedroom Units (G)	4,800 (A)	1,100 (B) , (E)	(H)	(H)	--	(I)	(I)	(I)	(I)	30	2.5
RM Multiple-Family High Rise:											
Efficiency Units	(J)	500 (B)	(H)	(H)	15	75 (K) , (L)	50 (K) , (L)	150 (K) , (L)	50	150	15
One Bedroom Units (G)	(J)	700 (B)	(H)	(H)	15	75 (K) , (L)	50 (K) , (L)	150 (K) , (L)	(K) , (L)	150	15
Two Bedroom Units (G)	(J)	900 (B)	(H)	(H)	15	75 (K) , (L)	50 (K) , (L)	150 (K) , (L)	50	150	15
Three or More Bedroom Units (G)	(J)	1,100 (B) , (E)	(H)	(H)	15	75 (K) , (L)	50 (K) , (L)	150 (K) , (L)	(K) , (L) 50 (K) , (L) 50 (K) , (L)	150	15
RMH, Mobile Home Park	5,550	720	55	--	50	35 (R)	25 (R)	50 (R)	35 (R)	30	2
C, Local Business	--	--	--	--	--	75 (N)	15 (M) , (P)	15 (M)	25	40	4
C-1, General Business	--	--	--	--	--	75 (N)	15 (M) , (P)	15 (M)	25	40	4
C-2, Extensive Highway Business	--	--	--	--	--	35 (N)	25 (P)	50	20	40	4
FS, Freeway Service	--	--	--	--	--	75 (N)	75 (P)	150	75	30	3

OT, Office Technology	--	--	--	--	--	(D),(T),(U)	20	40 (D)	30	45	3 (W)
							(D),(P),(V)		(D),(V)		
M-1, Light Industrial	--	--	--	--	35	50 (O)	40 (P)	80	40	30 (S)	2.5 (S)
M-2, General Industrial	--	--	--	--	35	60 (O)	50 (P)	100	50	40 (S)	4 (S)
MT, Industrial Transportation	--	(B)	--	--	35	50 (O)	50 (P)	100	50	35	3
AP, Airport	250 acres	--	--	--	--	100 (O)	50 (P)	100	100	--	--
BRMU, Belleville Road Mixed Use District	See Section 3.119 for District-Specific Regulations										
SRMU, Sumpter Road Mixed Use District	See Section 3.121 for District-Specific Regulations										

Article 6 Chapter 4. Sumpter Road Overlay District (SROD)

Section 6.401 Intent and Purpose

The Sumpter Road Overlay Zoning District (SROD) establishes regulations applicable to site development including building setbacks, design, parking, access management, landscaping, and signage. This overlay district is based upon careful evaluation, study, and plans completed as part of a master planning effort by the Van Buren Township Planning Commission. Objectives of the overlay district are as follows:

- (A) Promote development consistent with the Sumpter Road Corridor Plan, South Side Master Plan, and 2020 Van Buren Charter Township Master Plan.
- (B) Establish consistent high quality standards for site development and building appearance.
- (C) Achieve well-managed, safe, and efficient flow of motorized and non-motorized traffic, including accessibility and connectivity.
- (D) Foster an environment that is safe and friendly for non-motorized users of Sumpter Road, particularly pedestrians and bicyclists, that contributes to the Township's sustainability as a vital, attractive, economic, and healthy place to live.
- (E) Permit the combination of uses which are complementary to one another within the same structure or zone.
- (F) Enhance the built environment of the area in a manner consistent with the area's established semi-rural character, and provide a sense of place, including the creation of high quality public spaces.

The application of these standards contributes to the Township's economic development objectives and are intended to help sustain and increase the value of private property.

Section 6.402 Definitions

The following definitions shall apply specifically to the SROD, but may be applied broadly throughout the Township where, in the opinion of the Planning Director, the definition has broad applicability:

- (A) **BUILDING ENVELOPE STANDARDS (BES):** Basic parameters regulating building location and form, including the envelope, placement (in three dimensions) and certain permitted/required building elements, such as storefronts, balconies, and street walls. The BES establish both the boundaries within which things may be done, and specific things that must be done. The applicable BES for a site are determined by its subarea in the Overlay District as shown on the Regulating Plan for the Sumpter Mixed-Use District.
- (B) **CORRIDOR PLAN:** Refers to the Sumpter Road Corridor Plan as adopted by the Van Buren Township Planning Commission.

- (C) **FAÇADE FRONTAGE:** Refers to the façade of a building where the primary public pedestrian entrance must be located. The area in front of the building frontage is considered the “front yard” for the purposes of this Chapter.
- (D) **GROUND FLOOR:** The first level of a building where at least 80 percent of the finished floor elevation is within the finished floor elevation parameters established in the designated BES. The next story above the ground floor is the second floor.
- (E) **STREETSPACE:** Includes all space between building frontages (travel lanes, sidewalks, squares, pedestrian pathways, civic greens, sidewalks, parks, etc.), but not garage entries or alleys.
- (F) **UNDERLYING DISTRICT:** The underlying district is the zoning district upon which an overlay subarea district is laid. The SROD is superimposed upon the underlying zoning districts.

Section 6.403 Overlay District Concept

The Sumpter Road Overlay District (SROD) is a mapped zone that imposes a set of special requirements in addition to those of the underlying zoning district(s).

The SROD is a mandatory overlay district. Property in the SROD may continue to be used as permitted by the standard zoning district and all other legal nonconformities are permitted to continue; however, any new activity that requires site plan approval in accordance with the requirements of the Zoning Ordinance shall also comply with the requirements of the SROD contained in this Chapter. In the event there are conflicts between the requirements of the underlying zoning district and the SROD, the requirements of the SROD shall govern.

Section 6.404 Overlay District Boundaries

The boundaries of SROD shall be on the Zoning Map of the Charter Township of Van Buren. The District will include all parcels that have frontage on Sumpter Road in Van Buren Township as of [Month], 2022.

Section 6.405 Administration

- (A) **Applicability of the SROD.** All proposed uses and development in the SROD shall be reviewed the same as the same uses and development outside of the SROD, in accordance with *Article 12, Chapter 2*.

Administrative Review and Site Plan Review. Administrative Review or Site Plan Review shall be required in accordance with *Article 12, Chapter 2*. Plans must contain all of the information required in *Section 12.203* and the process shall follow the procedures of *Section 12.205*.

Special Land Use Review. For any development or use in the SROD that requires Special Land Use review, the procedures of Article 12, Section 3 shall be followed.

Site Condominium and Subdivision Development. Any proposed site condominium (Article 6, Chapter 1) or subdivision (Subdivision Control Ordinance) in the SROD shall follow the required procedures and regulations with the exception that any design requirement contained in the SROD shall take

precedence over any similar design requirement contained in *Article 6, Chapter 1* or the Subdivision Control Ordinance.

(B) Appeals and Variances. Any appeal or variance from the standards or requirements of the SROD shall be reviewed in accordance with *Article 12, Chapter 4*.

(C) Nonconformities. The requirements of *Article 14* apply to all nonconformities in the SROD.

Section 6.406 Permitted and Special Land Uses

(D) Bemis Road to Hull Road. The permitted and special land uses in the area between Bemis Road and Hull Road shall be those permitted and special land uses of the underlying zoning district.

(E) North of Hull Road. The permitted and special land uses in the area north of Hull Road shall be in accordance with the Sumpter Road Mixed Use District in Section 3.121 (B) and (C).

Section 6.407 Building Envelope Standards (BES)

(F) Standards Applicable to All Areas. The goal of the Building Envelope Standards (BES) is the creation of a healthy and vital public realm. The BES set the basic parameters governing building form and location, including the building envelope (in three dimensions) and certain required and/or permitted building elements, such as balconies, bay windows, stoops, and shop fronts. BES shall conform to requirements of this Chapter as well as the following general guiding principles:

Buildings shall be generally aligned and setbacks should generally conform to those immediately adjacent on the same block. Buildings should be used to form both the space of the street and the overall context of the district.

Mixed-use and commercial buildings shall be designed to include active front spaces. This contributes to vital and safe public space.

Vehicle storage and parking for buildings other than single-family homes, shall be located at the rear of the building(s).

Retail, where permitted, shall be located on the ground floor at the Façade Frontage because retail use has the effect of making the street active and interesting.

Section 6.408 Parking, Loading, and Access Management

(A) Parking. The purpose of these standards is to provide adequate off-street parking for sites in the SROD while maintaining attractive site designs and providing safe access to sites using all modes of transportation.

1. **General Parking.** Parking spaces shall be provided as required by Article 9 of the Van Buren Township Zoning Ordinance. Parking in front of buildings in the frontage lot-shallow

parcels under Section 3.121 shall be limited to a maximum of one single tier of parking and one maneuvering lane, set behind a greenbelt as required under Article 10 (Landscaping and Screening).

2. **Shared Parking.** Shared parking shall be provided wherever possible in accordance with *Article 9*.
3. **Bicycle Parking.** Adding bicycle parking in the Sumpter Road Corridor area is required in the SROD. One (1) bicycle parking space shall be provided for each fifteen (15) vehicle parking spaces or fraction thereof, placing an emphasis on providing short-term bicycle parking to complement the land uses commonly found in the SROD. Bicycle parking shall be conveniently located near the public entrance. The Planning Commission may increase or decrease the number of required bicycle parking spaces based on expected demand and available areas to locate bicycle parking.

(B) Vehicular Access Standards. The purpose of this subsection is to facilitate through traffic operations, ensure public safety along roadways, and protect public investment in the street system while providing property owners with reasonable access. The standards are specifically designed to apply to Sumpter Road and a limited number of local street extensions that may occur to facilitate access to landlocked parcels or as part of providing access to lots which result from the division of large undeveloped parcels.

1. **Application of Standards.** The standards contained herein shall be required in addition to, and where permissible shall supersede, the requirements of the Wayne County Roads Division (WCRD).
2. **Street Network.** All development must be linked to an interconnected network of streets, shared access drives, or other public passageways. The street network and the network of nonmotorized access shall follow the recommendations of the Mobility Plans within the 2020 Van Buren Township Master Plan and the 2022 Sumpter Road Corridor Plan, and the Rural Residential Pattern guidelines within the 2022 Sumpter Road Corridor Plan. The following requirements are intended to create a connected network of local streets that is designed to balance property access with the retention of a semi-rural context.
 - (a) **Street Connectivity.** Street connections for the continuation of the street network onto adjacent properties shall be provided when the potential exists for the continuation of those streets on adjacent parcels.
 - (b) **Public Streets.** Internal streets should be dedicated to the public, consistent with the Master Plan to ensure connectivity between adjacent parcels.
 - (c) **Cross-Access.** Blanket cross-access easements shall be provided for all shared access drives and curb cuts for properties along the east side of Sumpter Road, north of Hull Road, to ensure that the internal circulation system connects to the internal circulation system on adjacent parcels. The blanket cross-access agreement shall provide for reciprocal cross-access for connection to adjacent parcels and curb cuts without limitation. This will have the long-term effect of reducing traffic on roads,

reducing curb cuts, and encouraging more cohesive development. The cross access shall be formalized into a Service Road (also known as a Slip Street) design as described in Section 3.121 and 9.106(F) of the Zoning Ordinance and in the 2022 Van Buren Township Sumpter Road Corridor Plan.

- (d) **Sumpter Rural Settlement Street Patterns.** The following street pattern requirements apply to areas designated as Sumpter Rural Settlement future land use in the 2022 Sumpter Road Corridor Plan:
 - (i) **Dead end street prohibition.** No platted subdivision or site condominium development may be designed with any street intersecting at Hull Road, Sumpter Road, or Bemis Road which does not have provide a second point of access onto Hull Road, Sumpter Road, or Bemis Road.
 - (ii) **Rear Alleys.** Lots with less than 150' of frontage which have primary frontage on Hull Road, Sumpter Road, or Bemis Road must have access served by a rear alley or lane that provides access to the rear of the buildings.

3. Street Design.

- (a) **Design Speed.** Shared access drives and internal streets shall be designed with a maximum design speed of twenty-five (25) miles per hour.
- (b) **Alleys and Rear Access Lanes.** Alleys and lanes that provide access to the rear of buildings are encouraged.
- (c) **Street Cross-Sections.** Street design in the Gateway Mixed Use Subarea shall be in accordance with the Mixed Use District in *Section 3.121*.
- (d) **Driveways.** At the time of development or redevelopment of any site, new driveways shall be designed, and existing driveways shall be consolidated and closed, such that the minimum driveway spacing standards of Section 9.106(C) of the Zoning Ordinance are met throughout the SROD.

- 4. **Modification of Standards for Special Situations.** During site plan review the Planning Commission shall have the authority to modify the vehicular access standards of this *Section 6.408(B)* upon consideration of the standards in *Section 9.106(G)*.

- (C) **Bicycle and Pedestrian Access.** The goal of these standards is to provide adequate access to bicyclists and pedestrians in the SROD, recognizing Sumpter Road as an important gateway between the southern portions of the Township and the City of Belleville. In compliance with the Michigan Planning Enabling Act, any newly constructed public streets or shared access drives in the SROD shall provide access for bicyclists and pedestrians to the greatest extent possible.

- 1. **Bicycle Access.** New developments shall connect to the bicycle transportation network throughout the Township. Bicycle access shall be provided in accordance with the recommendations of the Non-Motorized Transportation Plan within the 2020 Van Buren Township Master Plan.

2. **Bicycle Parking.** Bicycle parking shall be conveniently located near the public entrance and well-lit to discourage theft or vandalism.
3. **Pedestrian Access.** New developments shall be designed to allow for safe and efficient pedestrian access between the entrances of buildings and the sidewalks and pedestrian network of the SROD.
4. **Crosswalks.** Pedestrian areas that cross vehicular traffic at roads or parking lots shall be clearly marked, and vehicular traffic shall be encouraged to slow down near pedestrian crossings. Accent paving shall be incorporated at key crossings that may accommodate pedestrian traffic. Where accent paving is not used, a “zebra” or “continental” crosswalk marking pattern (alternating white and black bands) shall be installed to increase visibility. Pedestrian crosswalks shall be adequately lighted, have clear sight distances, and be free from view-hindering obstructions such as foliage and poles at crosswalk entries and median refuge islands.

Section 6.409 Architectural and Site Design Standards

The architectural and site design standards of the SROD are intended to customize, simplify, and streamline improvements, renovations, and future development to fit the desired character of the SROD, and to also be consistent with the vision and goals for this area as detailed in the Sumpter Road Corridor Plan. These standards describe and illustrate building and site features that are appropriate for the Sumpter Road corridor and other areas of the SROD. These standards establish the criteria used by the Township in reviewing the design of proposed development, and are intended to require consistent high quality design and development.

(A) Design Principles and Area Character.

1. **Connectivity.** The street, driveway, and sidewalk pattern in the SROD shall be designed to maximize auto, pedestrian, and bicycle connectivity.
2. **Unifying Design.** Developments shall have a unifying design. Corporate “franchise” architecture that is not compatible with the area’s established character, in the determination of the Planning Commission, shall be modified to be compatible with the SROD design theme herein.

(B) Site Design

1. **Introduction.** Siting involves a project’s relationship to the site, the street, and adjacent buildings. For example, buildings shall be sited to provide a comfortable and safe environment for pedestrians while accommodating vehicles.
2. **Open Space.**
 - (i) **Usable Public Open Space.** Increase the usability of public open space by providing shade, street furniture, special paving for sidewalks, pedestrian

pathways, turf, accent and flowering plants, and other site amenities. Usable open space shall be provided within new developments, and be keeping with the semi-rural nature of the area.

- (ii) **Quality of Open Space.** Open space shall be meaningful, and shall not be fragmented or consist of “leftover” land.
- (iii) **Open Spaces Between Buildings.** Recognize the importance of spaces between buildings as “outdoor rooms” on the site. Encourage the use of these spaces as open space in the form of covered pedestrian arcades (walkways), outdoor seating areas, or pedestrian plazas with benches.
- (iv) **Shading.** Pedestrian areas shall include a variety of sun and shading options, with an emphasis on trees, that allow users to choose how they want to use the site and connect with it.
- (v) **Visibility.** Outdoor spaces and plazas shall be visible from the adjoining buildings to help promote safety.
- (vi) **Passive Activity.** Open spaces shall be designed with a variety of passive recreational opportunities, including sitting and relaxing, meeting and social interaction, and nodes along urban trails, bikeways, and corridors.
- (vii) **Plazas.** The use of public plazas shall be encouraged where common open space can feasibly be provided in front of or alongside buildings. Access to and from plazas shall be well lit and meet accessibility requirements under the Americans with Disabilities Act.

3. **Parking Lots and Structures.**

- (a) **Shared and Joint Parking Areas.** Shared and joint parking areas that serve multiple businesses and land uses are strongly encouraged, and should primarily be located at the rear of the developments.
- (b) **Pedestrian Access.** Parking areas shall be designed so that, whenever possible, pedestrians walk parallel to moving cars unless perpendicular crossing is required and marked.

4. **Site Amenities.** All sites shall be required to provide amenities compatible with the site’s Subarea and use.

(a) **Site Furniture.**

- (i) While site furniture shall be provided as necessary, it should be used sparingly, and be of a rustic design quality keeping with the semi-rural character of the area. Any site furniture shall be made of high quality, long-lasting materials

suitable for use in Michigan's climate, and shall be low-maintenance and graffiti-resistant.

- (b) Outdoor Dining.** All outdoor dining areas shall be encouraged with commercial uses that include the sale of food, and are a preferred amenity for such uses. Where incorporated, outdoor dining areas shall be used to bring activity to plazas/courtyards, public spaces, and shall be placed at the edges of open space, or located along building and street frontages. Outdoor dining areas shall meet the requirements of *Section 5.137* and shall be oriented away from off-site uses that are sensitive to noise or nighttime activity, such as existing residential uses.
- (c) Decorative Paving.** Decorative special paving shall be used to enhance site design but should be used as an accent and should serve a specific purpose. Decorative paving includes treatments that encompass a rural aesthetic, such as crushed stone and earth. Preferred locations for decorative paving include:

 - (i)** Traffic calming at driveways and crossings;
 - (ii)** Pedestrian crossings/sidewalks;
 - (iii)** Pedestrian plazas and courtyards;
 - (iv)** Pedestrian walkways to distinguish between paths of travel and designated sales and/or seating areas;
 - (v)** Primary building entrances.

(C) Architectural Character

- 1. Introduction.** New buildings built between Hull Road and Bemis Road shall be designed to encourage pedestrian access and street life. Design elements used to achieve this include active uses on the ground floor of mixed-use and commercial buildings, clustering of buildings in a manner compatible with a rural village or hamlet, and porches on residential houses.

New or rehabilitated buildings in the built north of Hull Road are intended to foster an environment that, while still handling large volumes of automobile traffic, is also safe for pedestrians. Design elements used to achieve this include pedestrian-scaled façade articulation, pedestrian-scaled lighting, ground-story windows, and regularly spaced building entrances.

- 2. Sumpter Road Mixed Use District consistency requirement.** Regardless of whether a property is zoned Sumpter Road Mixed Use District, new developments or major site rehabilitation in the Sumpter Road Overlay District north of Hull Road shall adapt to the building types as described in Section 3.121, based on the property's lot type as defined in Section 3.121.

3. **Building Height, Form, and Mass**

- (a) **Shade.** Awnings and overhangs shall be used in conjunction with street trees to provide shade for pedestrians.
- (b) **Solar Access.** Orient buildings to take advantage of solar access. For optimum solar conditions, the longest side of a building should face south (within 20 degrees of due south) and have relatively clear access to the sun.
- (c) **Human Scale Design.** Incorporate elements into the design of large structures that provide a transition to the human scale, particularly at the ground level. Such elements shall be provided through, but not limited to, covered walkways, building arcades, and trellises.
- (d) **Corner Buildings.** All street-facing façades of a corner building shall be treated equally in regards to building façade relief, material changes, architectural details, and window transparency, placement, and articulation. Corner buildings may be angled to meet the corner at the ground floor only.
- (e) **Design of Site with Multiple Structures.** Sites with multiple structures shall be linked visually and physically. These links can be accomplished through the use of the same or compatible building architecture, materials, colors, signage, landscaping, lighting, and site layout.

4. **Facades, Windows, and Doors**

- (a) **Building Entrances Oriented Toward the Street.** Primary building entrances shall be oriented toward the street. Building entrances shall be designed as prominent and easily identifiable and shall form a transition between the exterior and interior. Adequate lighting shall be provided at building entries for security.
- (b) **Nonresidential Building Entrances.** Nonresidential building entrances shall be designed with awnings, canopies, or similar features to protect patrons and employees from the elements. Where nonresidential building has a front yard setback of zero (0) feet, storefront doors must be recessed at least thirty-six (36) inches from the building frontage line.
- (c) **Windows.** Elements of architecture including window and door placement shall be designed to add variety and interest to the project. Where used at the ground floor, glass shall be nonreflective and transparent..
- (d) **Additional Sunlight.** Additional sunlight shall be brought into large buildings through the use of atriums or skylights.
- (e) **Primary Architectural Features.** Primary architectural features must be related to the building's structure, function and/or engineering, and shall not be merely

decorative. While some elements may be decorative, the intent of this standard is to avoid developments with architectural elements that look applied, rather than incorporated (e.g. false front treatments, partial roof forms). For example, building facades that incorporate canopies or walls with mock gables must provide a pitched roof component to provide depth and give an authentic appearance.

(f) Physical Façade Design. The physical design of building facades is required to vary at least every 40 linear feet for any building in the Sumpter Road Overlay District. This can be achieved through such techniques as:

- (i)** Division into multiple buildings;
- (ii)** Break or articulation of the façade;
- (iii)** Significant change in façade design;
- (iv)** Placement of window and door openings, or
- (v)** Position of awnings and canopies.

(g) Mullions. Mullions – “true divided light” windows or sectional windows shall be used where a divided window design is desired; “snap-in” grills or mullions shall not be used.

(h) Window Visibility. Windows shall not be blocked from inside a building due to retail display racks, plywood sheets, posters, signs, and similar means, unless window signs are permitted by the Township.

(i) Windows on Masonry Buildings. Windows on masonry buildings must be recessed at least four (4) inches from the exterior surface of their corresponding facades.

5. Roofs and Upper Story Details

(a) Roof Design. Roofs shall be given design consideration and treatment equal to that of the rest of the building.

(b) Roofline Elements. Roofline elements shall be developed along all elevations.

(c) Parapet Walls. Interest shall be created in the parapet walls along the sides and rear through the use of height variations, relief elements, and the design of scuppers, downspouts, and expansion joints, organized into the pattern of the total building design.

(d) Roof-Mounted Equipment. Roof-mounted utility and wireless communication facilities shall be integrated directly into the architecture of building(s) as opposed to freestanding locations.

6. Building Materials and Colors

- (a) **Building Materials in General.** Building materials shall be as described in Section 3.121 for buildings north of Hull Road. Projects shall be designed using durable, low-maintenance, and timeless building materials such as natural brick or stone. Building materials shall reflect the Township's desired character of Sumpter Road. Exterior building materials used for buildings in the Township shall generally fall into two categories: Primary and Accent building materials. Primary building materials should cover a majority of the exterior wall surface, and accent building materials shall enhance and support the primary building materials and building as a whole.
- (b) **Prohibited Building Materials:** Unless approved by the Planning Commission as accent building materials, plain concrete block (both painted and unpainted), plywood or T-111 panels, aluminum siding, and similar materials shall be prohibited.
- (c) **Franchise/Corporate Architecture.** The scale, design, and materials of franchise/corporate architecture shall be consistent with the standards of this article.
- (d) **Building Materials in Mixed-Use Buildings.** Mixed-use commercial developments that contain residential units on the upper stories shall utilize building materials with known vibration and sound-reduction qualities in order to minimize noise impacts.
- (e) **Reflective Materials.** Reflective materials shall not be used to clad a building; however, if reflective architectural accents must be used to protect the integrity of the building design, then the materials shall not be a nuisance to the occupants of the existing surrounding structures, or create a safety hazard to any type of traffic.
- (f) **Roof Materials.** Metal seam, asphalt shingles, green roof, concrete tile, clay tile, or a similar grade of roofing material shall be used on all visible pitched roofs.
- (g) **Removal or Concealment of Pipes, Conduits, Wires, and Obsolete or Abandoned Signs.** All building materials including pipes, conduits, wires, and obsolete or abandoned signs that no longer serve a purpose due to renovations or similar actions shall be removed and sign anchors shall be patched to match adjacent surfaces. Operational pipes, conduits, etc. must be hidden.
- (h) **Window Transparency.** On the front facades of buildings, the windows shall be clear glass. No reflective, tinted, or spandrel glass is permitted on the front façade.
- (i) **Awning Materials.** Awning materials shall consist of canvas, fabric, or thin metal. No vinyl or plastic shall be used. Awnings with a high gloss finish shall be prohibited.
- (j) **Colors.**
 - (i) **Primary Building Colors.** Primary building cladding colors shall be red and brown for brick, and other warm earth tones for other primary building materials.
 - (ii) **Prohibited Primary Colors.** Intense, bright, black, or fluorescent colors shall be used sparingly and only as accents; such colors shall not be used as the

predominant color on any wall or roof of any building. Permitted signs shall be excluded from this standard.

(iii) **Color Changes.** Material or color changes shall not occur at external corners, but may occur at interior corners, or at other logical terminations. All material or color changes shall be reviewed by the Township and Township authorization is required prior to any material or color changes.

(iv) **Color Maintenance.** Exterior building colors shall “age” well, and shall be relatively low-maintenance in terms of frequency of touch-ups and re-painting.

7. Walls and Fences

(a) **Integration with Building and Site Design.** Where walls and fences are used, they shall be integrated with the overall building and site design.

(b) **Maximum Height.** Unless otherwise permitted in the SROD, walls and fences shall not exceed three (3) feet in height in the front or side yards in order to avoid the appearance of being a “fortress”.

(c) **Wall and Fence Material.** Walls must be constructed of red or brown brick, or natural stone. Fences must be constructed of wrought iron, or durable wrought iron-look fencing, black vinyl-coated chain link fencing (in rear or side yards only), or split rail.

(d) **Prohibited Materials.** The use of razor or barbed wire, electrified, uncoated chain link, fabric, or unfinished concrete block is prohibited.

(e) **Security Fencing.** If security fencing is necessary, a design which incorporates open grillwork is encouraged, or a combination of short solid walls with pillars and/or recesses to provide architectural articulation.

8. Security

(a) **Natural Surveillance, Access Controls, and Activities.** A secure development shall be created for both sites and their occupants by minimizing opportunities for crime and undesirable activities through natural surveillance, access control, and activities.

(b) **Site Design.** Buildings and windows shall be located to maximize visibility of entryways, pathways, and parking lots. Where possible, building corners shall be designed to allow perpendicular visibility by either installing clear glass, angling off the corner, or locating the sidewalk in a way that allows perpendicular visibility.

(c) **Lighting.** Adequate security and safety lighting for pedestrians from parking spaces to all buildings entries and exits shall be provided (see Section 8.105).

(d) **Visual Barriers.** Pedestrian access or entries shall be free from visual barriers such as landscaping to maximize visibility.

- (e) **Addresses.** Street addresses for commercial, public, or multi-use residential buildings shall be required and easily visible on the front of the building both during the daytime and at night. Address signs shall comply with *Article 11*.

9. **Store Vacancy**

- (a) **Locked and Secured.** Vacant stores shall be locked and secured to prevent unauthorized trespassing during the period of vacancy, but shall not be boarded up unless required by the Township, or otherwise appear derelict or abandoned.
- (b) **Maintenance.** Owners of vacant stores shall find creative temporary alternative uses of storefront window areas such as using them as a display area for public art. Similarly, the exterior façade of vacant stores shall be maintained by the owner at the same level of quality as surrounding occupied and buildings.
- (c) **Lighting.** An adequate level of exterior security lighting shall be regularly maintained regardless of storefront occupancy status.

(D) **Lighting.** In the Sumpter Road Overlay District, site light fixtures shall be attractively designed to complement the architecture of the project and surrounding development, and shall improve the visual identification and safety of businesses and residential units. Light pollution shall be minimized and energy conservation technologies shall be used. In addition to the standards section 8.105, the following requirements apply in the Belleville Road Overlay District.

1. **Safety and Visual Interest.** Lighting shall be used to provide safety and visual interest.
2. **Effect of Landscaping.** When placing light fixtures and luminaries, consideration shall be given to effects of landscape growth on lighting function. Landscaping such as trees and shrubs shall be placed and maintained so that they does not obscure or deteriorate onsite illumination.
3. **Maximum Illumination.** Decorative accent lights and fixtures greater than the minimum required illumination level section 8.105 for adjoining parking lots may be approved by the Planning Commission at vehicle driveways, entry throats, pedestrian paths, plaza areas, and other activity areas.
4. **Attractive Design.** Lights fixtures shall be attractively designed to complement the architecture of the project, improve visual identification of businesses, and create an inviting atmosphere for passersby.
5. **Maximum Height Near Single-Family Residences.** If located within fifty (50) feet of a single-family residential lot, parking lot fixtures shall not exceed twenty (20) feet in height.

6. **Parking Lot Light Fixtures.** Light fixtures within parking lots that are not located in a landscape island or median shall be designed with raised bases to protect them from damage by vehicles.
7. **Decorative Up-lighting.** Decorative up-lighting that enhances landscape features and building architecture is encouraged as long as it does not compete with street lighting and signs.
8. **Scale.** Street lighting shall be scaled for the pedestrian while still meeting vehicular needs. On local streets and within sites, fixtures shall be primarily oriented towards pedestrian's needs. On the major thoroughfares, light fixtures illuminate both pedestrian areas and roadways.
9. **Parking Lot Lighting.** Lighting for a parking lot or structure shall be evenly distributed and provide pedestrians and drivers with adequate visibility and safety level at night.
10. **Light Color.** The light source used in outdoor lighting shall provide a white light for better color representation and to create a more pedestrian friendly environment. Low pressure sodium lamps are prohibited.
11. **Shielding.** All lighting shall be placed and shielded so as to not interfere with the vision of persons on adjacent roads or property.
12. **Energy Efficiency.** Energy efficient lighting, such as LED lighting, shall be used for exteriors whenever possible.

Section 6.410. Landscaping and Screening

- (A) Intent.** Landscaping within the Sumpter Road Overlay District (SROD) is intended to provide a visually appealing experience which maintains a pedestrian-oriented vision for the area and which maintains the rural character of the area surrounding the District. Landscaping aids in a unified and consistent visual theme for the District.

Therefore, compliance with the following specific standards shall be required in conjunction with any site development, redevelopment, alteration or expansion or re-occupancy of a vacant building on a SROD site.

- (B) Applicability.** The standards of this Section apply in addition to the requirements of the underlying zoning district and those of *Article 10* of the Zoning Ordinance. In the event of a conflict, the more specific requirements of this Section shall apply. Further, in consideration of the overall design and impact of a landscape plan, the Planning Commission may modify or adjust the landscape requirements applicable to a proposed plan provided that the adjustment is consistent with the intent of this BROD and the Zoning Ordinance.

(C) Landscape Design Standards.

1. **General Landscaping.**

- (a) General site landscaping shall be provided as required under Article 10 and Sections 3.121(D)(5) and 3.121(D)(6) of the Zoning Ordinance.
2. **Sumpter Road Frontage/Streetscape.** In addition to the requirements of Section 10.103(A), all streets and shared access drives shall be landscaped as follows:
- (a) Sumpter Road streetscape as required herein shall be provided:
 - (i) Along all Sumpter frontage, and
 - (ii) Along other streets and shared access drives within the boundaries of the SROD.
 - (b) **Frontage Landscaping Requirements.**
 - (i) **Street Trees.** In addition to the trees required in the frontage area in Section 10.103(A), deciduous street trees shall be planted within the right-of-way of any streets (i.e., between the sidewalk and the street), access drives, or internal roads, private or public, in an amount equal to at least one (1) deciduous street tree per each forty (40) linear feet or fraction thereof of frontage, except in the case of properties where single family homes or duplexes have frontage on and face Sumpter Road
 - (ii) **Perennial Beds.** Perennial beds, planted in groups, shall be planted in twenty percent (20%) of the required landscape frontage area defined in *Section 10.103(A)*.
 - (c) **Design Flexibility.** Within the intent of this Chapter, the Planning Commission may approve alternatives it determines to be necessary to accommodate peculiar circumstances or unforeseen problems, or to carry out the spirit, intent and purposes of this Chapter. Further, where streetscape elements have been accomplished along Belleville Road by means of earlier development which substantially accomplishes the overall design objectives, the Planning Commission may modify the requirements to reflect those earlier improvements.
 - (d) **Zero Lot Line Buildings.** With the exception of required right-of-way landscaping, front yard landscaping is not required on private property along zero lot line frontages and where building facades are located at the right-of-way line.
3. **Sumpter Rural Settlement Landscaping.** The following landscaping requirements apply to areas designated as Sumpter Rural Settlement future land use in the 2022 Sumpter Road Corridor Plan:
- (a) **Cluster Housing Landscaping.** Where housing is constructed in a site condominium or platted subdivision within the SROD, and housing is clustered to a net density exceeding 2 units per acre within a larger development, open space must be preserved along at least 50% of the frontage of Sumpter Road.
 - (b) **Greenway Landscaping.** Where housing is constructed in a site condominium or platted subdivision within the SROD and is not considered Hamlet style housing, lots which are parallel to Sumpter Road and have primary frontage on a street that is

perpendicular to Sumpter Road shall be separated by a green space which is one of the following:

- (i) A greenbelt which is 30' minimum in width which contains either preserved woodland or planted trees, or a combination of both. The preserved woodland and/or planted trees shall have a tree density which is equal to one (1) deciduous or evergreen tree for each ten (10) linear feet or portion thereof of road frontage, and if planted shall be planted in a staggered double row; OR
- (ii) A greenbelt which is 30' minimum in width which contains native perennial and grass plants and contains a surface stormwater management swale or ditch.

Article 9 Parking, Loading, and Access Management

Section 9.106 Driveways and Access Management

(A) Service Road or Shared Driveway Design Standards.

1. **Location.** Shared driveways or service roads shall generally be parallel or perpendicular to the front property line and may be located either in front of, adjacent to, or behind, principal buildings. In considering the most appropriate alignment for a service road, the Planning Commission shall consider the setbacks of existing building and anticipated traffic flow for the site.
2. **Access Easement.** Shared driveways and service roads shall be within an access easement recorded with the Wayne County Register of Deeds, which permits traffic circulation between properties. This easement shall be sixty (60) feet wide, except an access easement parallel to a public street in front of the building may be forty (40) feet wide. The required width shall remain free and clear of obstructions, and shall not be used for parking unless approved by the Planning Commission.
3. **Driveway Storage Length.** A driveway storage area shall be provided between the intersection of the service drive with an arterial road and an internal circulation lane. The depth of the storage area shall be sufficient to accommodate expected vehicle queues. As a guideline, the minimum storage area should be at least sixty (60) feet. A larger or smaller storage area may be required depending upon the trip generation characteristics of uses served by the drive.
4. **Maintenance.** Each property owner using a shared driveway or service drive shall be responsible for its maintenance. The Planning Commission shall require a copy of the maintenance agreement between all property owners responsible for the shared facility to ensure that an agreement is in place to provide adequate maintenance.

...

Article 10 Landscaping and Screening

Section 10.103 Landscaping Design Standards

Except as otherwise specified in the general requirements for each zoning district or for the specific land use, all landscaping shall conform to the following standards. The clear vision triangle area requirements of [Section 7.108](#) shall also be met. Existing landscaping that meets the requirements of this Article may be used to comply with the following standards.

...

- (C) **Greenbelt Buffering.** The intent of greenbelt buffering is to reserve an appropriate area of a lot to screen and buffer the lot and its use(s) based on its zoning district and the zoning district of the adjacent lots, regardless of whether the adjacent lots are developed. A greenbelt as specified in [Table 7](#) below shall be provided for all uses requiring Site Plan Review and Administrative Review ([Article 12, Chapter 2](#)).

Table 7. Required Greenbelt Specifications

Developing Zoning District	Adjacent To				
	AG, R-1, and R-2 Districts	RM and RMH Districts	C, C-1, C-2, FS, and OT Districts	M-1 District	MT, M-2, and AP Districts, and Railroad and Utility rights-of-way
AG, R-1, and R-2 Districts	<i>a</i>	<i>b</i>	<i>b</i>	<i>b</i>	<i>b</i>
RM and RMH Districts	<i>b</i>	<i>a</i>	<i>b</i>	<i>b</i>	<i>b</i>
C, C-1, C-2, FS, and OT Districts	<i>b</i>	<i>b</i>	<i>a</i>	<i>b</i>	<i>b</i>
M-1 District	<i>c</i>	<i>c</i>	<i>b</i>	<i>a</i>	<i>b</i>
MT, M-2, and AP Districts	<i>c</i>	<i>c</i>	<i>c</i>	<i>b</i>	<i>a</i>

- a. A ten (10) foot wide buffer, with one (1) tree per thirty (30) linear feet. Agricultural lots shall be exempt from this requirement where they abut agricultural and detached single-family residential lots. For proposed single-family residential uses, the greenbelt buffering requirements shall apply only to Subdivision and Site Condominium ([Article 6, Chapter 1](#)) development, including Planned Residential Developments ([Article 6, Chapter 2](#)).*
- b. A twenty (20) foot wide buffer, with one (1) tree per twenty (20) linear feet (minimum of 50% evergreen trees). A six (6) foot high masonry wall or opaque fence may be installed in lieu of the required trees, subject to Planning Commission review and approval.*
- c. A sixty (60) foot wide buffer, with a staggered double row of evergreen trees spaced fifteen (15) feet on center on a six (6) foot high berm (see [Section 10.103\(H\)](#)) with a flat horizontal area at the crest to be at least three (3) feet in width. The planting shall be in a manner where the evergreen trees provide eighty percent (80%) opacity within three (3) years of planting, measured from the top of the berm. After three (3) years, if this opacity is not achieved then additional evergreen trees and/or shrubs shall be planted to achieve eighty percent (80%) opacity at the time of their planting. A six (6) foot high masonry wall or opaque fence may be installed in lieu of the required berm and trees.*



MEMO

TO:	Van Buren Township Planning Commission
FROM:	Dan Power– Director of Planning and Economic Development
RE:	Proposed Landscaping Modifications for Menards Gate Addition
DATE:	July 22, 2022

Planning Commissioners:

Applicant Menards, Inc. seeks to install a gate and canopy addition to the north end of their existing building located at 10010 Belleville Road (parcel ID number 83 061 99 0005 726). The parcel is zoned C-2 – Extensive Highway Business District. The proposed gate and canopy will provide a route for an express lane to provide customers access for pickup of product orders and the lumber yard. See the project plans described more in detail on the following plan submittal pages.

The proposed addition does not meet the threshold for requiring formal site plan review with the Van Buren Township Planning Commission. However, it will alter landscaping in a manner that prompts Planning Commission review. The extended pavement and gate addition will impact an existing greenbelt along the north end of the site. A portion of the site's northern boundary borders the Meadows of Van Buren Condominium development, which is zoned RM – Multiple Dwelling Residential District. Roughly 160' long section of the site's northern greenbelt that is adjacent to this site will be reduced to a width of less than 20 feet due to the proposed addition. This is contrary to section 10.103(E) of the Van Buren Township Zoning Ordinance, which requires a 20' wide greenbelt with one (1) tree per linear feet where C-2 zoning is adjacent to RM zoning. However, the Planning Commission has the authority to modify this requirement under Section 10.106 of the Zoning Ordinance where the proposed adjustment is keeping with the intent of Article 10 of the Van Buren Township Zoning Ordinance (Landscaping and Screening).

For the following reasons, I **support** the Planning Commission granting this requested modification:

- The greenbelt reduction will be minimal, with a setback of between 10' (10' deficiency) and 17' (3' deficiency) being maintained along the impacted 160'-long section;
- The applicant has identified that the nearest multifamily building will be 180' away from the impacted area; and
- The applicant is committing to add trees to meet the 1 tree per 20 linear feet requirement and fill in the missing space as required.

As a condition of this approval, I recommend that the Planning Commission require that the applicant work with the Township Planning Director to ensure they replant 1 tree per 20 linear feet in the affected area.

Thank you for allowing me to provide this recommendation.

Sincerely,

A handwritten signature in black ink, appearing to read "Dan Power". The signature is fluid and cursive, with the first name "Dan" and last name "Power" clearly distinguishable.

Dan Power, AICP
Planning and Economic Development Director
Charter Township of Van Buren Public Services Department



BELLEVILLE, MI:
CITY OF BELLEVILLE
6 MAIN STREET
BELLEVILLE, MI 48111
734-697-9323



PROJECT TITLE
BELLEVILLE - TYLER ROAD, VAN BUREN TOWNSHIP, MI 48111



SCALE --	DRAWN BY POM	DATE 12/17/20
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COVER

Path and File Name: F:\Designdiv\Proto5-sites\A-2017 SITES\Belleville - Van Buren Township, MI\2022 Remodel\Phase 1 - Express Lane\Construction Drawings\COVER.dwg

BUILDING CODE:	2015 MICHIGAN BUILDING CODE, 2015 IBC 2014 NATIONAL ELECTRICAL CODE 2015 MICHIGAN FIRE CODE 2015 MICHIGAN EXISTING BUILDING CODE 2015 MICHIGAN ENERGY CONSERVATION CODE (90.1-2013) 2009 ACCESSIBLE AND USEABLE BUILDINGS AND FACILITIES, ANSI 117.1-2009	
BUILDING USE:	ACCESSORY	
OCCUPANCY:	(U) UTILITY, MISC.	
CONSTRUCTION TYPE:	VB	ATTACHED TO MAIN BUILDING FOR
EXISTING BUILDING AREA:	<u>1,350 SQ. FT.</u>	TOTAL OF 243,959 S.F.
TOTAL BUILDING AREA:	1,350 SQ. FT.	+ 1,350 S.F.
		245,309 S.F. UNLIMITED AREA
BUILDING OCCUPANT LOAD:	1,350/ 300 = 4.5	
EXITING REQUIRED:	4.5 x .15 = 0.675"	

	PRECAST		SHEET REVISION NUMBER
	ROUGH WOOD		WINDOW TAG
	FINISHED WOOD		ROOM TAG
	STEEL		DOOR TAG
	CONCRETE BLOCK		ELEVATION MARK
	POURED CONCRETE		SECTION REFERENCE
	GRANULAR FILL		ELEVATION REFERENCE
	EARTH		
	RIGID INSULATION		
	BATT INSULATION		
	DETAIL NUMBER		
	SHEET NUMBER		

INSPECTION OF FABRICATORS:

1. APPLICABLE ELEMENT (FABRICATOR CERTIFICATION REQUIREMENTS)

2. WHEN SPECIAL INSPECTIONS ARE REQUIRED BY BUILDING OFFICIAL:

2.1. FABRICATOR SHALL FOLLOW THE INSPECTION PROCEDURES. THE SPECIAL INSPECTOR SHALL VERIFY THAT THE FABRICATOR MAINTAINS DETAILED FABRICATION AND QUALITY CONTROL PROCEDURES THAT PROVIDE A BASIS FOR INSPECTION CONTROL OF THE WORKMANSHIP AND THE FABRICATORS ABILITY TO CONFORM TO APPROVED CONSTRUCTION DOCUMENTS AND REFERENCED STANDARDS, THE SPECIAL INSPECTOR SHALL REPORT THE RESULTS OF THE INSPECTION TO THE BUILDING OFFICIAL CONCERNING THE ADEQUACY RELATIVE TO THE CODE REQUIREMENTS FOR THE FABRICATORS SCOPE OF WORK.

3. WHEN SPECIAL INSPECTIONS ARE NOT REQUIRED BY BUILDING OFFICIAL:

3.1. UPON COMPLETION OF FABRICATION, THE APPROVED FABRICATOR SHALL SUBMIT A CERTIFICATE OF CONFORMANCE TO THE BUILDING OFFICIAL, STATING THAT THE WORK WAS PERFORMED IN ACCORDANCE WITH THE APPROVED CONSTRUCTION DOCUMENTS.

SPECIAL INSPECTION	FREQUENCY	REFERENCE STANDARD
SOILS:		
1. SITE PREPARATION – VERIFY THAT THE SITE SUBGRADE SOILS ARE PROPERLY PREPARED	CONTINUOUS	GEOTECHNICAL ENGINEERING REPORT
2. FILL PLACEMENT 12" THICK OR GREATER—VERIFY MATERIAL BEING USED AND LIFT THICKNESS	CONTINUOUS	
3. EVALUATION OF IN-PLACE DENSITY OF COMPACTED FILL 12" THICK OR GREATER	PERIODIC	
4. SUB-GRADE IMPROVEMENTS INVOLVING SOIL MIXING, COMPACTION, GROUTING, DYNAMIC COMPACTION, OR PLACEMENT OF STONE COLUMNS	CONTINUOUS	
CONCRETE:		
1. INSPECTION OF REINFORCING STEEL, INCLUDING PRESTRESSED TENDONS, AND PLACEMENT	PERIODIC	ACI 318: Ch. 3.5, 7.1–7.7
2. INSPECTION OF REINFORCING STEEL WELDING AS REQUIRED BELOW		
A. VERIFICATION OF WELDABILITY OF REINFORCING STEEL OTHER THAN ASTM A 706	PERIODIC	AWS D1.4 ACI 318: 3.5.2
B. REINFORCING STEEL—RESISTING FLEXURAL AND AXIAL FORCES IN INTERMEDIATE AND SPECIAL MOMENT FRAMES, AND BOUNDARY ELEMENTS OF SPECIAL REINFORCED CONCRETE SHEAR WALL, AND SHEAR REINFORCEMENT	CONTINUOUS	
C. SHEAR REINFORCEMENT	CONTINUOUS	
D. OTHER REINFORCING STEEL	PERIODIC	
3. INSPECT BOLTS TO BE INSTALLED IN CONCRETE PRIOR TO AND DURING PLACEMENT OF CONCRETE	CONTINUOUS	
4. INSPECT EPOXY SET ANCHORS AND EXPANSION ANCHORS INSTALLED IN HARDENED CONCRETE	CONTINUOUS	PRODUCT ICB0 REPORT
5. VERIFYING USE OF REQUIRED DESIGN MIX	PERIODIC	ACI 318: Ch. 4, 5.2–5.4
6. SAMPLING FRESH CONCRETE PRIOR TO AND DURING PLACEMENT OF CONCRETE	CONTINUOUS	ASTM C 172 ASTM C 31 ACI 318: 5.6, 5.8
7. INSPECTION OF CONCRETE PLACEMENT FOR PROPER APPLICATION TECHNIQUES	CONTINUOUS	ACI 318: Ch. 5.9, 5.10
8. INSPECTION FOR MAINTENANCE OF SPECIFIED CURING TEMPERATURE AND TECHNIQUES	PERIODIC	ACI 318: Ch. 5.11, 5.13
9. VERIFICATION OF IN-SITU CONCRETE STRENGTH OF FOUNDATIONS PRIOR TO LOAD APPLICATION	CONTINUOUS	
STEEL CONSTRUCTION:		
1. MATERIAL VERIFICATION OF HIGH-STRENGTH BOLTS, NUTS, AND WASHERS:		
A. IDENTIFICATION MARKINGS TO CONFORM TO ASTM STANDARDS SPECIFIED IN THE APPROVED CONSTRUCTION DOCUMENTS	PERIODIC	APPLICABLE ASTM MATERIAL SPECIFICATIONS: AISC SECTION A3.3
B. MANUFACTURERS CERTIFICATE OF COMPLIANCE REQUIRED		
2. INSPECTION OF HIGH-STRENGTH BOLTING		
A. IDENTIFICATION MARKINGS TO CONFORM TO ASTM STANDARDS SPECIFIED IN THE APPROVED CONSTRUCTION DOCUMENTS	PERIODIC	AISC SECTION M2.5
B. MANUFACTURERS CERTIFIED MILL TEST REPORTS REQUIRED	CONTINUOUS	
3. MATERIAL VERIFICATION OF STRUCTURAL STEEL:		
A. IDENTIFICATION MARKINGS TO CONFORM TO ASTM STANDARDS SPECIFIED IN THE APPROVED CONSTRUCTION DOCUMENTS		ASTM A 6 OR ASTM A 568
B. MANUFACTURERS CERTIFIED MILL TEST REPORTS REQUIRED		
4. MATERIAL VERIFICATION OF WELD FIELD MATERIALS:		
A. IDENTIFICATION MARKINGS TO CONFORM TO AWS STANDARDS SPECIFIED IN THE APPROVED CONSTRUCTION DOCUMENTS		AISC SECTION A3.5
B. MANUFACTURERS CERTIFIED MILL TEST REPORTS REQUIRED		
5. INSPECTION OF WELDING:		
A. STRUCTURAL STEEL		
1) COMPLETE AND PARTIAL PENETRATION GROOVE WELDS	CONTINUOUS	AWS D1.1
2) MULTI-PASS FILLET WELDS	CONTINUOUS	
3) SINGLE-PASS FILLET WELDS GREATER THAN $\frac{5}{16}$ " (7.9MM)	CONTINUOUS	AWS D1.3
4) SINGLE-PASS FILLET WELDS LESS THAN $\frac{5}{16}$ " (7.9MM)	PERIODIC	
5) FLOOR AND DECK WELDS	PERIODIC	
6. INSPECTION OF STEEL FRAME JOINT DETAIL FOR COMPLIANCE WITH APPROVED CONSTRUCTION DOCUMENTS		
A. DETAIL SUCH AS BRACING AND STIFFENING	PERIODIC	
B. MEMBER LOCATIONS		
C. APPLICATION OF JOINT DETAILS AT EACH CONNECTION		
WOOD FRAMING:		
1. INSPECTION OF STRUCTURAL WOOD FRAMING FOR COMPLIANCE WITH APPROVED CONSTRUCTION DOCUMENTS.		
A. MEMBER SIZE, GRADE, LOCATION	PERIODIC	
B. CONNECTIONS AND JOINT DETAILS	PERIODIC	
C. BRACING	PERIODIC	
2. INSPECTION OF WOOD TRUSSES FOR COMPLIANCE WITH CONSTRUCTION DOCUMENTS AND TRUSS DRAWINGS		
A. DIMENSIONS/US LOADING/ SPACING	PERIODIC	
B. BRACING AND CONNECTIONS	PERIODIC	
C. CONNECTIONS TO SUPPORT MEMBERS	PERIODIC	

A.R.	ANCHOR RODS	JT.	JOINT
A.F.F.	ANCHOR FINISH FLOOR	L.L.	ANGLE
AL.	ALTERNATE	L.V.	LIVE LOAD
ANCH.	ANCHOR	MAX.	MAXIMUM
ANCH.	BUILDING	MFR. OR MANUF.	MANUFACTURER
BLDG.	BUILDING	M.H.	METAL HALIDE
B.	BUILDING LINE	MIN.	MINIMUM
BM.	BEAM	MM	MIDWEST MANUFACTURING
B.O.	BOTTOM OF	MTL.	METAL
BOIT.	BOTTOM	N.I.C.	NOT IN CONTRACT
BRG.	BEARING	N.O. #	NUMBER
C.	CONTROL JOINT	N.S.	NON SHRINK
C.	CENTER LINE	N.T.D.	NOT TO SCALE
CL.	CLEAR	O.C.	ON CENTER
C.M.R.	CONCRETE MASONRY UNIT	O.D.	OUTSIDE DIAMETER
COL.	COLUMN	O.H.	OVERHEAD
CONC.	CONCRETE	OPNG. OR OPG.	OPENING
CONST.	CONSTRUCTION	OPP.	OPPOSITE
CONT.	CONTINUOUS	LBS. OR #	POUNDS
CONTRACTOR	CONTRACTOR	P.	PLATE
DET.	DETAIL	P.L. OR T	POLY.
D.A. OR Ø	DIAMETER	POLY.	POLYETHYLENE BARRIER
DIM.	DIMENSION	PREFAB. OR PFB.	PREFABRICATED
DIST.	DISTANCE	P.S.F.	POUNDS PER SQUARE FOOT
D.L.	DEAD LOAD	P.S.I.	POUNDS PER SQUARE INCH
D.W.	DRAINAGE	QTY.	QUANTITY
D.F.	DRAW FACE	R. OR RAD.	RADIUS
E.	EACH WAY	REINF. OR RE.	REINFORCE OR REINFORCING
E.A.	EACH	REP.	REPRESENTATIVE
EL. OR ELEV.	ELEVATION	REQ'D	REQUIRED
ELECTRICAL	ELECTRICAL	SCHED.	SCHEDULE
ENT.	ENTRANCE	SECT.	SECTION
EQ.	EQUAL	SER	STRUCTURAL ENGINEER OF RECORD
EXG.	EXISTING	S.F. OR SQ. FT.	SQUARE FOOT
EXP.	EXPANSION	SHT.	SHEET
EXT.	EXTERIOR	SM.	SIMILAR
FIN.	FINISH	SPEC.	SPECIFICATIONS
FL. OR FLR.	FLOOR	STL. OR ST.	STEEL
FT.	FOOT OR FEET	STRUCT.	STRUCTURAL
FTG.	FOOTING	TEMP.	TEMPORARY
GDN. OR FND.	FOUNDATION	THK.	THICK
GAGE.	GAGE OR GAUGE	THRU	THROUGH
G.C.	GENERAL CONTRACTOR	T.O.	TOP OF
GEN.	GENERAL	T&G.	TONGUE & GROOVE
HEAVY DUTY	HEAVY DUTY	TRTD.	TREATED
HGT. OR HT.	HEIGHT	T.S.	TEE STEEL
HORIZ.	HORIZONTAL	TYP.	TYPICAL
H.P.S.	HIGH PRESSURE SODIUM	UNL.S.	UNLESS NOTED OTHERWISE
HSS	HOLLOW STRUCTURAL SECTION	VERT.	VERTICAL
I.D.	INSIDE DIAMETER	W/	WITH
IN.	INCH	W/	W/
INSUL.	INSULATION	WWR	WELDED MRE REINFORCEMENT

NO.	TITLE
COVER	COVER SHEET
G-050	EXISTING AND DEMO SITE PLAN
G-051	PHASE WORK SITE PLAN
G-052	SITE DETAILS
GA-1	PLAN AND DETAILS – FLOOR
GA-2	ELEVATIONS – ARCHITECTURAL
GS-3	PLAN AND DETAILS – FOOTINGS AND CONCRETE SLAB
GU-4	PLAN AND DETAILS – ELECTRICAL



LEGEND

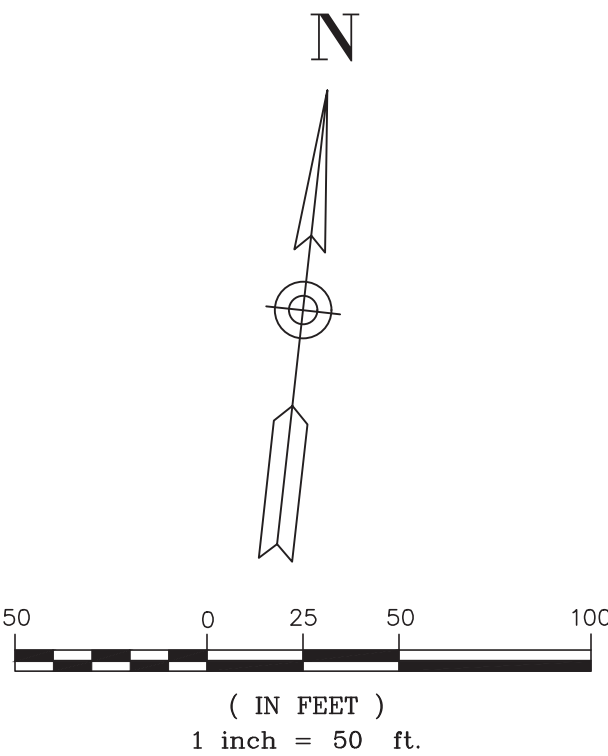
- PROPERTY LINE
- EXISTING FENCELINE RACKING TO REMAIN
- EXISTING FENCELINE RACKING TO BE REMOVED

KEYED DEMO NOTES (ALSO REFER TO REMODEL SPECIFICATIONS)

- 1 REMOVE FENCELINE RACKING (SALVAGE RACKING)
- 2 SAW CUT STEPPED CONCRETE
- 3 EXISTING GRASS AREA TO BE REMOVED AS NEEDED FOR YARD EXPANSION. VERIFY WITH PROJECT MANAGER. VERIFY WITH CIVIL SET
- 4 EXISTING LANDSCAPING TO BE REMOVED AND TRANSPLANTED. FOR NEW LOCATION, REFER TO SHEET G-051
- 5 REMOVE EXISTING CURB AND GUTTER AND ASPHALT. REFER TO DETAIL 1/G-052.
- 6 REMOVE EXISTING SIDEWALK. REFER TO CIVIL SET

GENERAL NOTES

- 1. NO PHASE CAN START WITHOUT THE PREVIOUS PHASE 100% COMPLETED. NO PUNCH LIST ITEMS.
- 2. CONTRACTOR TO SUPPLY, INSTALL AND REMOVE ALL TEMPORARY FENCING. INSTALL AS NEEDED TO SECURE THE YARD, IN THE SCOPE OF WORK, AND AS SHOWN ON THE PLAN. FENCING NEEDED TO SECURE THE YARD WILL BE 10'-0" TALL CHAIN LINK FENCE WITH POST CORED INTO THE PAVEMENT. FENCE INSIDE OR OUTSIDE OF THE SECURED YARD, AROUND WORK AREAS AND AROUND THE CONTRACTORS STAGING AREA WILL BE 6'-0" TALL CHAIN LINK FENCE PANELS ON STANDS.
- 3. VERIFY LOCATION OF CONTRACTOR STAGING AREA. KEEP NEAT AND CLEAN AND INSTALL 6'-0" TALL CHAIN LINK FENCE PANELS ON STANDS AROUND STAGING AREA. CONTRACTOR TO ACCESS THROUGH THE FRONT FOR ALL CONSTRUCTION ACTIVITIES. STORE MANAGER WILL HAVE A KEY AND CONTRACTOR TO COORDINATE LOCKING AT NIGHT.



1 DEMO AND EXISTING SITE PLAN
G-050



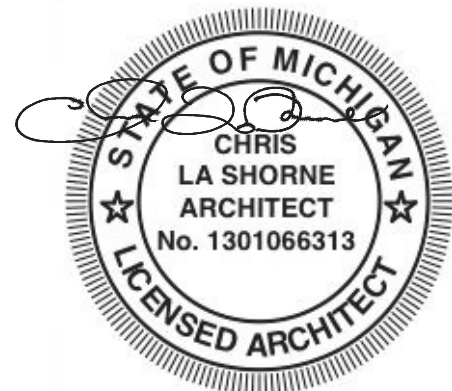
BUILDING TYPE
GATE CANOPY

CAD DWG NAME
_Belleville_SITES_GATE

PROJECT TITLE
BELLEVILLE - TYLER ROAD, VAN BUREN TOWNSHIP, MI 48111

SHEET TITLE
EXISTING AND DEMO SITE PLAN

REVISIONS	NO.	DATE	DESCRIPTION	BY

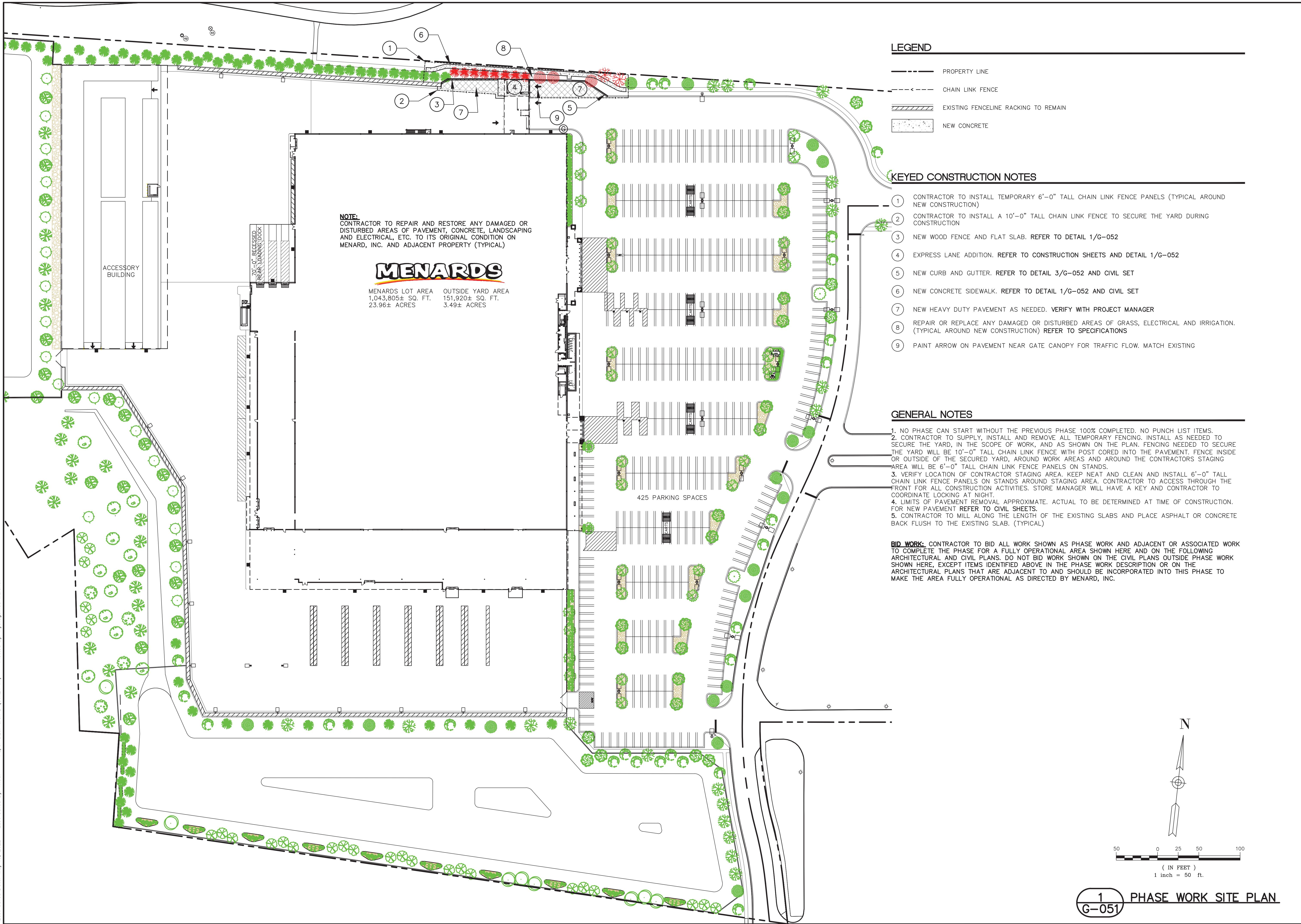


SCALE
AS NOTED

DRAWN BY
KWK

DATE
12/17/20

SHEET NO.
G-050



LEGEND

- PROPERTY LINE
- - - CHAIN LINK FENCE
- EXISTING FENCELINE RACKING TO REMAIN
- NEW CONCRETE

- KEYED CONSTRUCTION NOTES**
- 1 CONTRACTOR TO INSTALL TEMPORARY 6'-0" TALL CHAIN LINK FENCE PANELS (TYPICAL AROUND NEW CONSTRUCTION)
 - 2 CONTRACTOR TO INSTALL A 10'-0" TALL CHAIN LINK FENCE TO SECURE THE YARD DURING CONSTRUCTION
 - 3 NEW WOOD FENCE AND FLAT SLAB. REFER TO DETAIL 1/G-052
 - 4 EXPRESS LANE ADDITION. REFER TO CONSTRUCTION SHEETS AND DETAIL 1/G-052
 - 5 NEW CURB AND GUTTER. REFER TO DETAIL 3/G-052 AND CIVIL SET
 - 6 NEW CONCRETE SIDEWALK. REFER TO DETAIL 1/G-052 AND CIVIL SET
 - 7 NEW HEAVY DUTY PAVEMENT AS NEEDED. VERIFY WITH PROJECT MANAGER
 - 8 REPAIR OR REPLACE ANY DAMAGED OR DISTURBED AREAS OF GRASS, ELECTRICAL AND IRRIGATION. (TYPICAL AROUND NEW CONSTRUCTION) REFER TO SPECIFICATIONS
 - 9 PAINT ARROW ON PAVEMENT NEAR GATE CANOPY FOR TRAFFIC FLOW. MATCH EXISTING

GENERAL NOTES

1. NO PHASE CAN START WITHOUT THE PREVIOUS PHASE 100% COMPLETED. NO PUNCH LIST ITEMS.

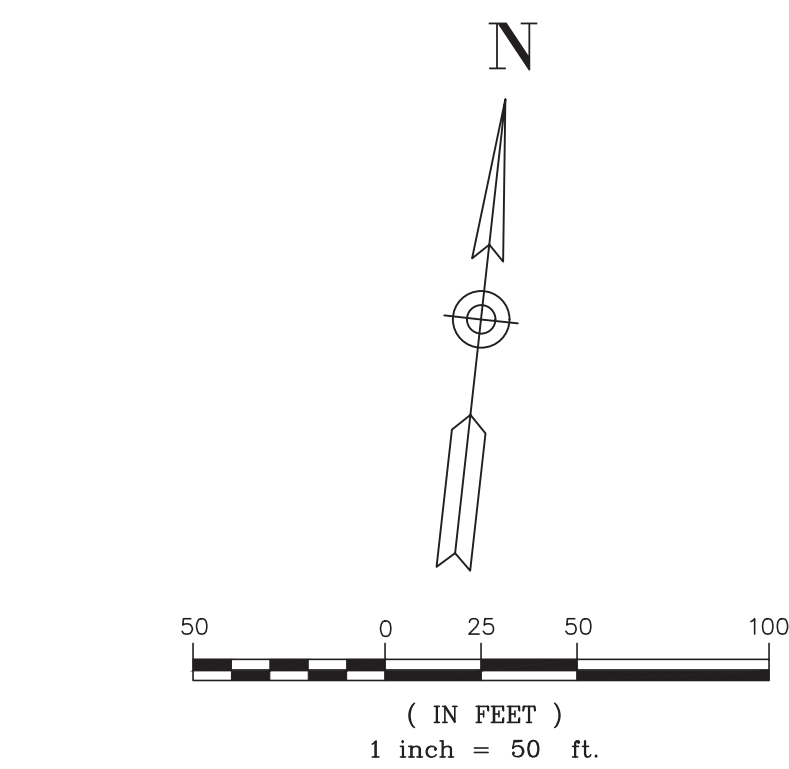
2. CONTRACTOR TO SUPPLY, INSTALL AND REMOVE ALL TEMPORARY FENCING. INSTALL AS NEEDED TO SECURE THE YARD, IN THE SCOPE OF WORK, AND AS SHOWN ON THE PLAN. FENCING NEEDED TO SECURE THE YARD WILL BE 10'-0" TALL CHAIN LINK FENCE WITH POST CORED INTO THE PAVEMENT. FENCE INSIDE OR OUTSIDE OF THE SECURED YARD, AROUND WORK AREAS AND AROUND THE CONTRACTORS STAGING AREA WILL BE 6'-0" TALL CHAIN LINK FENCE PANELS ON STANDS.

3. VERIFY LOCATION OF CONTRACTOR STAGING AREA. KEEP NEAT AND CLEAN AND INSTALL 6'-0" TALL CHAIN LINK FENCE PANELS ON STANDS AROUND STAGING AREA. CONTRACTOR TO ACCESS THROUGH THE FRONT FOR ALL CONSTRUCTION ACTIVITIES. STORE MANAGER WILL HAVE A KEY AND CONTRACTOR TO COORDINATE LOCKING AT NIGHT.

4. LIMITS OF PAVEMENT REMOVAL APPROXIMATE. ACTUAL TO BE DETERMINED AT TIME OF CONSTRUCTION. FOR NEW PAVEMENT REFER TO CIVIL SHEETS.

5. CONTRACTOR TO MILL ALONG THE LENGTH OF THE EXISTING SLABS AND PLACE ASPHALT OR CONCRETE BACK FLUSH TO THE EXISTING SLAB. (TYPICAL)

BID WORK: CONTRACTOR TO BID ALL WORK SHOWN AS PHASE WORK AND ADJACENT OR ASSOCIATED WORK TO COMPLETE THE PHASE FOR A FULLY OPERATIONAL AREA SHOWN HERE AND ON THE FOLLOWING ARCHITECTURAL AND CIVIL PLANS. DO NOT BID WORK SHOWN ON THE CIVIL PLANS OUTSIDE PHASE WORK SHOWN HERE, EXCEPT ITEMS IDENTIFIED ABOVE IN THE PHASE WORK DESCRIPTION OR ON THE ARCHITECTURAL PLANS THAT ARE ADJACENT TO AND SHOULD BE INCORPORATED INTO THIS PHASE TO MAKE THE AREA FULLY OPERATIONAL AS DIRECTED BY MENARD, INC.



1
G-051 PHASE WORK SITE PLAN

STORE DESIGN DEPARTMENT
5101 MENARD DRIVE
EAU CLAIRE, WI 54703

BUILDING TYPE
GATE CANOPY

CAD DWG NAME
_Belleville_SITES_GATE

PROJECT TITLE
BELLEVILLE - TYLER ROAD, VAN BUREN TOWNSHIP, MI 48111

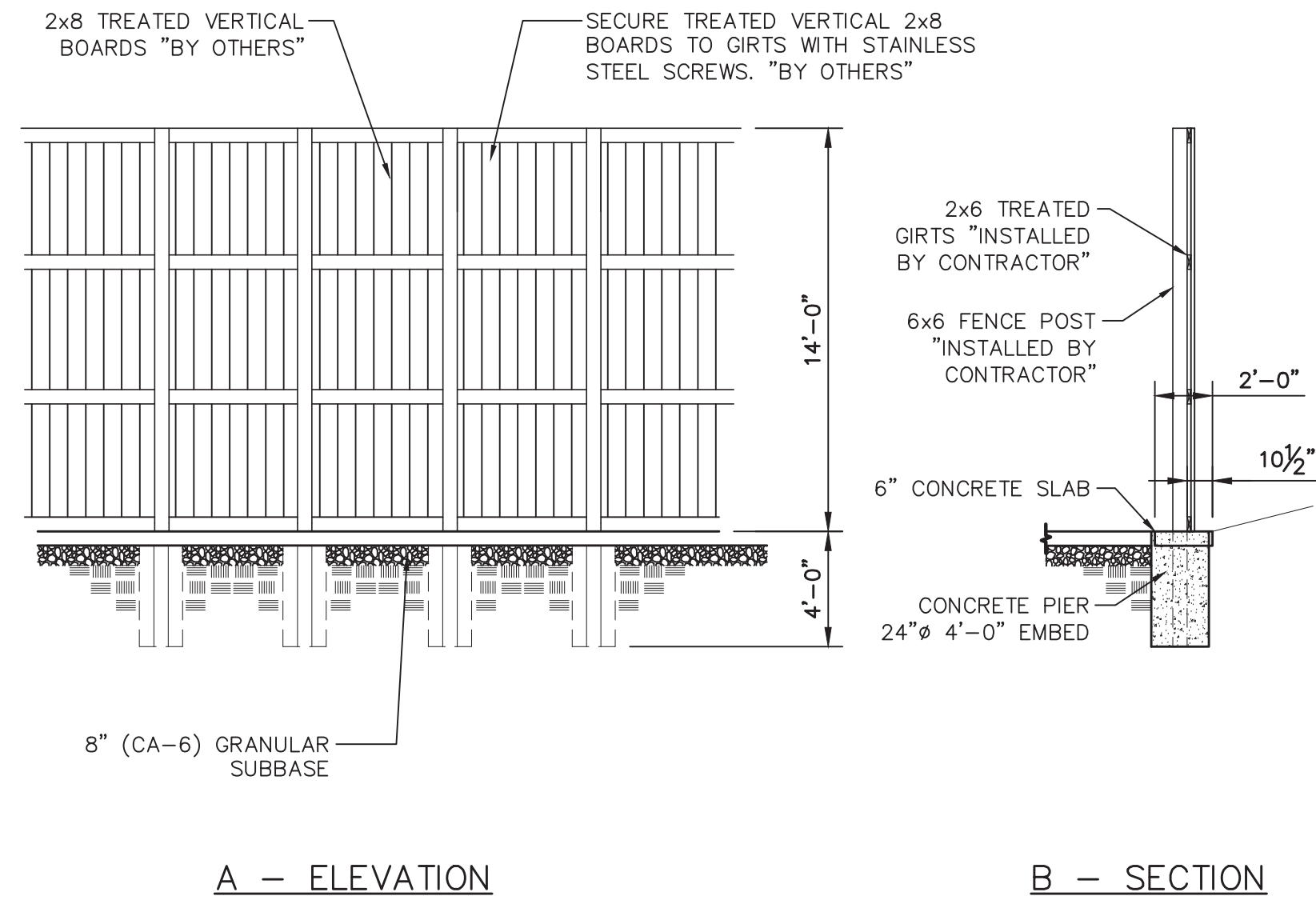
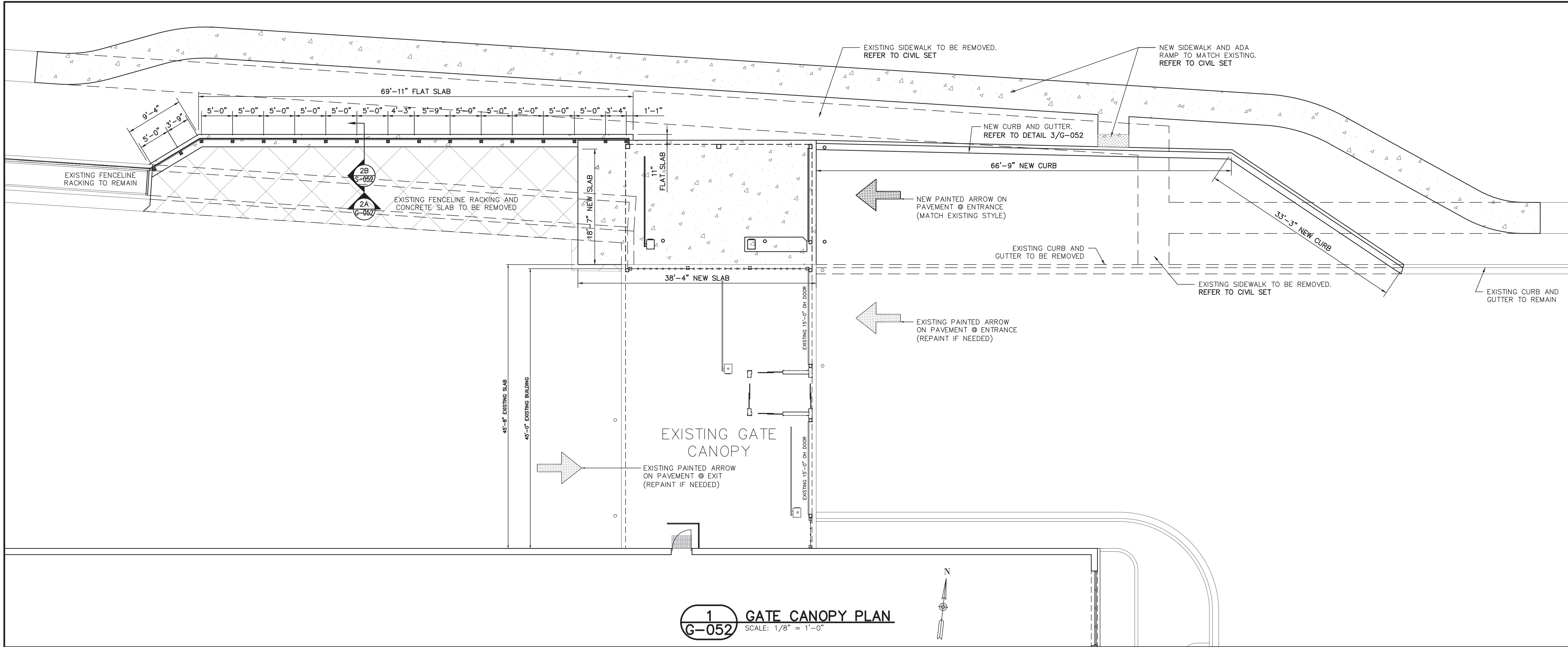
SHEET TITLE
PHASE WORK SITE PLAN

REVISIONS	
NO.	DESCRIPTION

SCALE AS NOTED	DRAWN BY KWK	DATE 12/17/20
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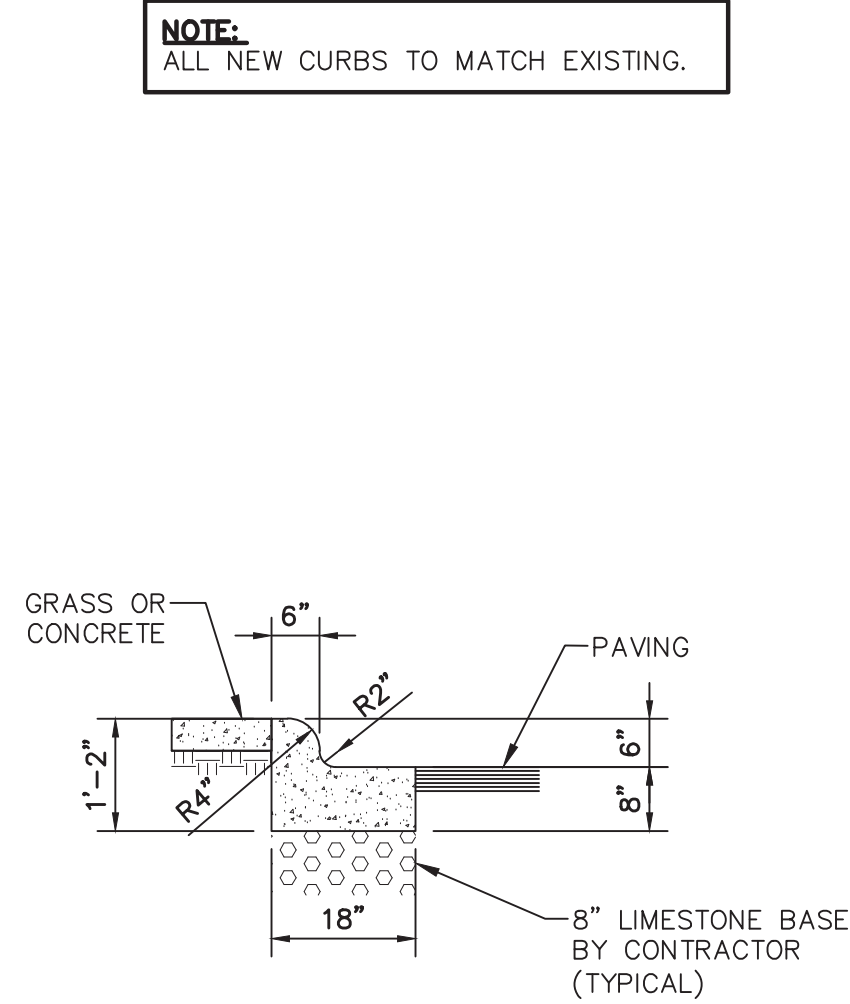
SHEET NO. **G-051**

Last Saved Date: May 19, 2022
Last Saved By: kioth
Paper Size: arch D (36x24 in.)
Plotted by: Pythagorus Marshall



NOTE:
SECURE 2x6 #3 SPF TREATED GIRTS TO FENCE POSTS WITH (3) STAINLESS STEEL SCREWS PER GIRT ON EACH FENCE POST.

2 WOOD FENCE SECTION AND ELEVATION
G-052 SCALE: 3/16" = 1'-0"



3 CONCRETE CURB AND GUTTER SECTION
G-052 SCALE: 1/2" = 1'-0"

MENARDS
STORE DESIGN DEPARTMENT
5101 MENARD DRIVE
EAU CLAIRE, WI 54703

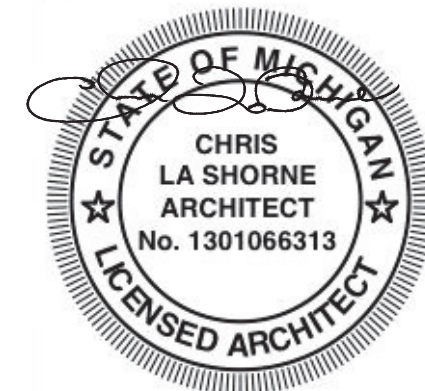
BUILDING TYPE
GATE CANOPY

CAD DWG NAME
_Belleville_SITES_GATE

PROJECT TITLE
BELLEVILLE - TYLER ROAD, VAN BUREN TOWNSHIP, MI 48111

SHEET TITLE
SITE DETAILS

NO.	DATE	DESCRIPTION

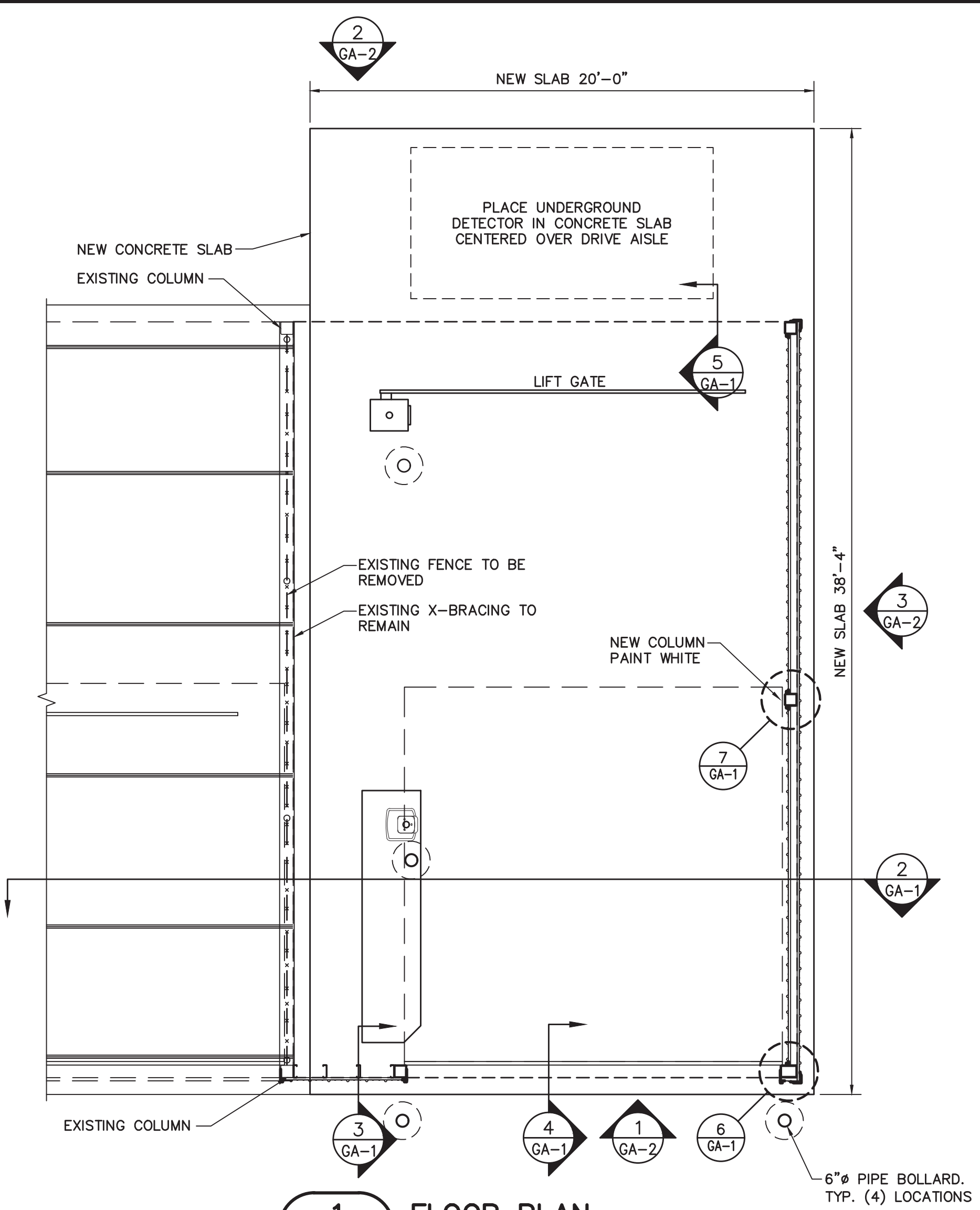


SCALE
AS NOTED

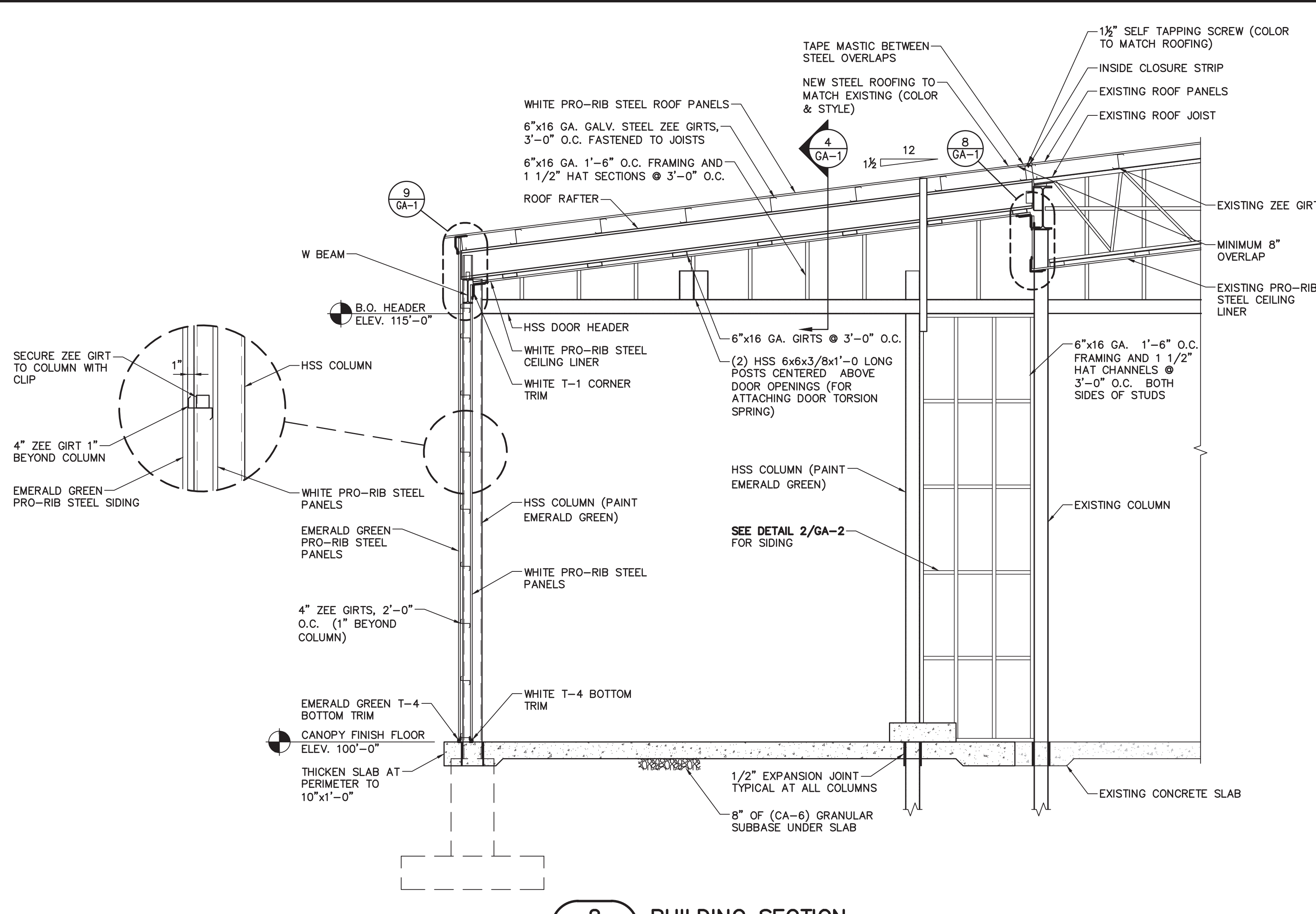
DRAWN BY
KWK

DATE
12/17/20

SHEET NO.
G-052

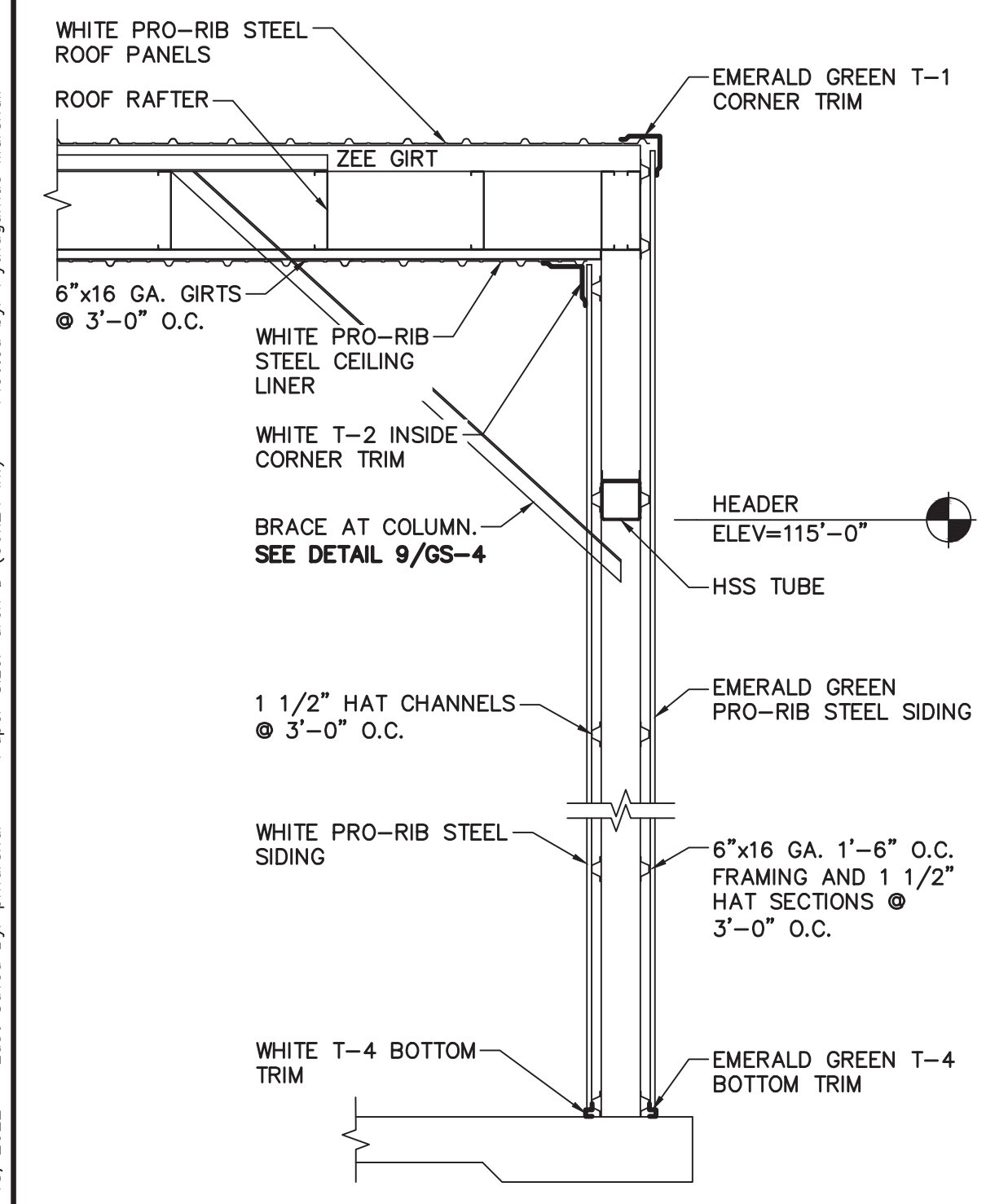


1 FLOOR PLAN
GA-1 SCALE: 1/4" = 1'-0"

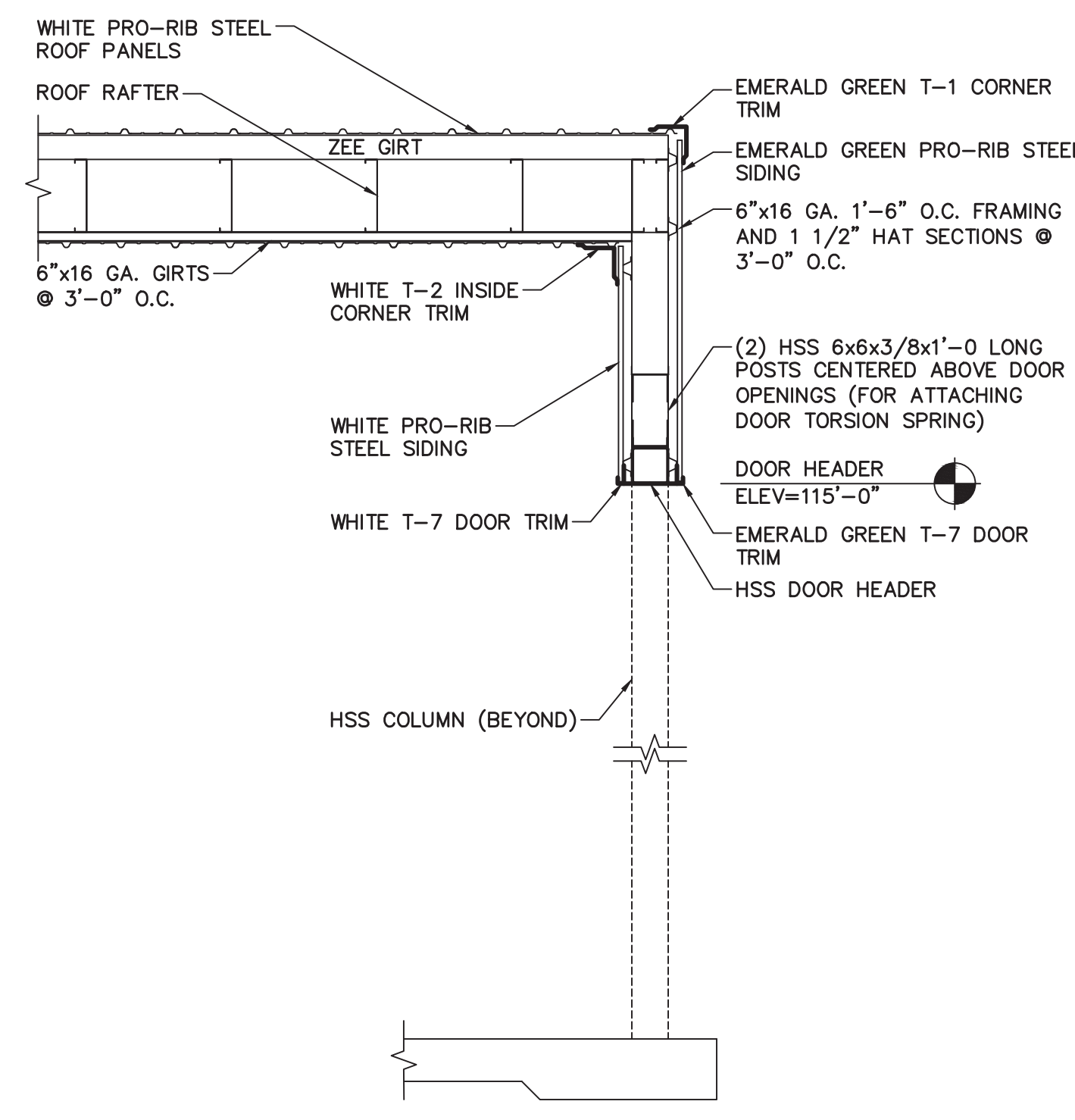


2 BUILDING SECTION
GA-1 SCALE: 3/8" = 1'-0"

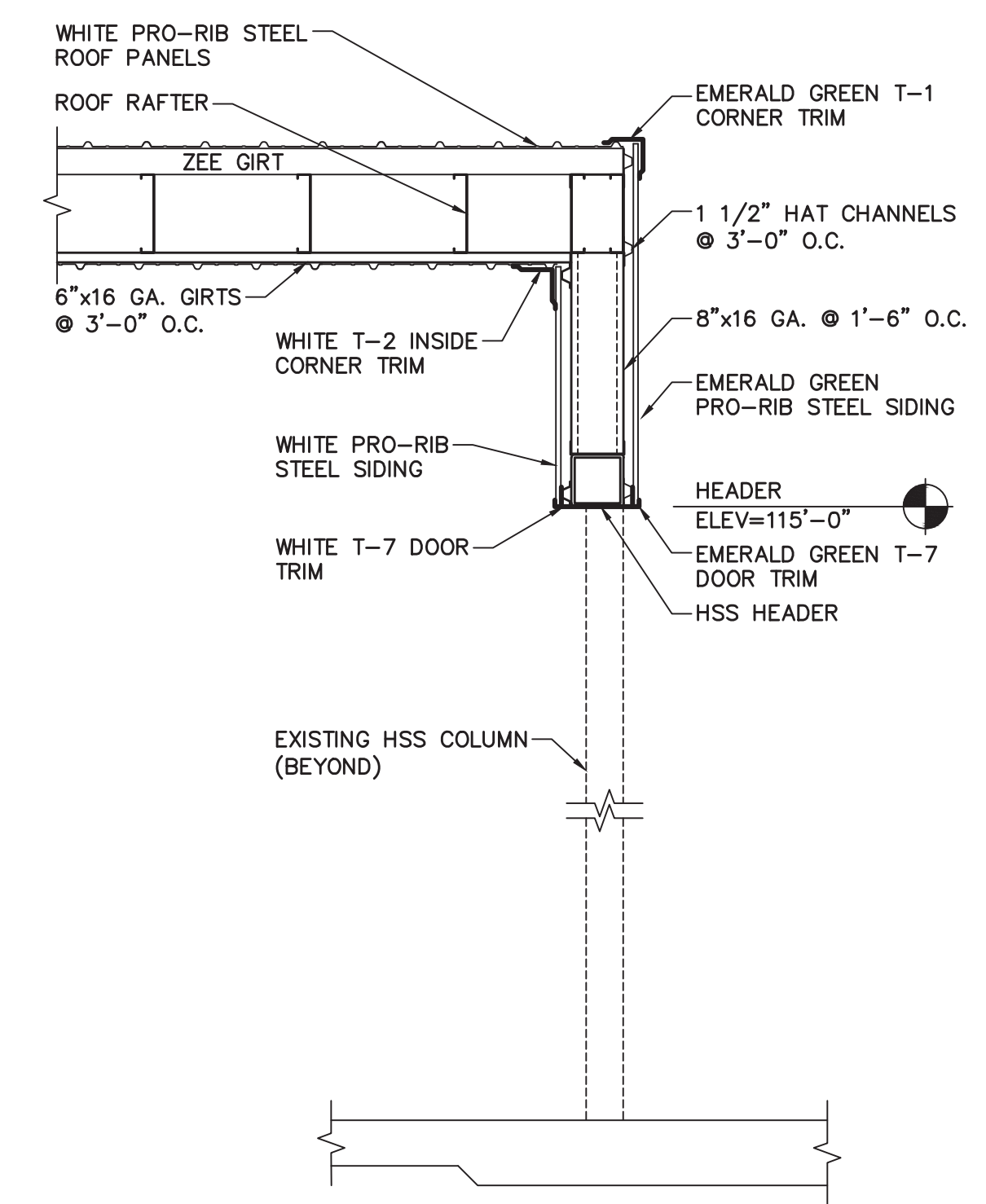
Plotted by: Pythagorus Marshall
Paper Size: arch D (36x24 in.)
Last Saved Date: May 18, 2022
Last Saved By: pmarshal



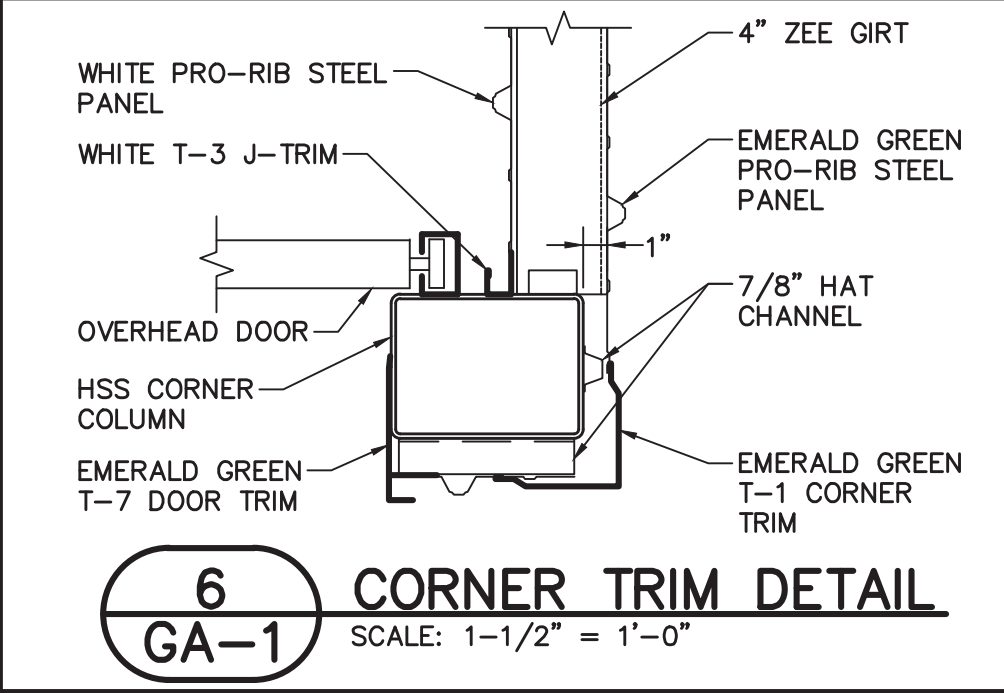
3 WALL SECTION
GA-1 SCALE: 1/2" = 1'-0"



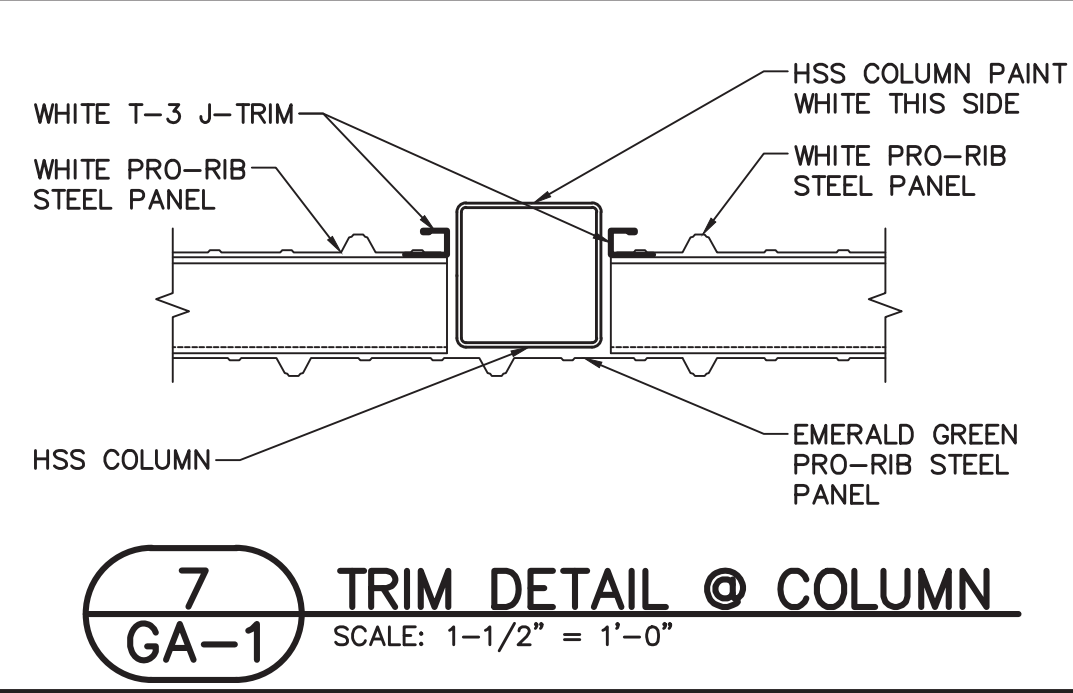
4 WALL SECTION AT OVERHEAD DOOR
GA-1 SCALE: 1/2" = 1'-0"



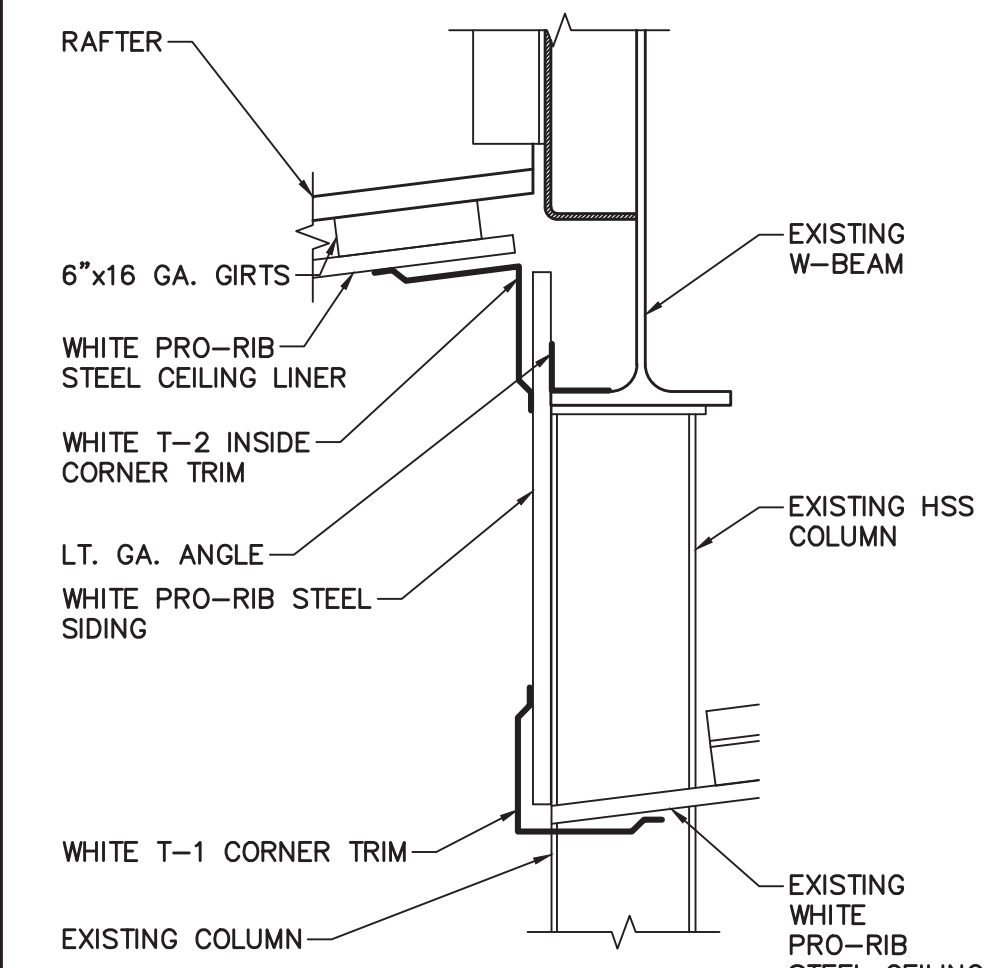
5 WALL SECTION
GA-1 SCALE: 1/2" = 1'-0"



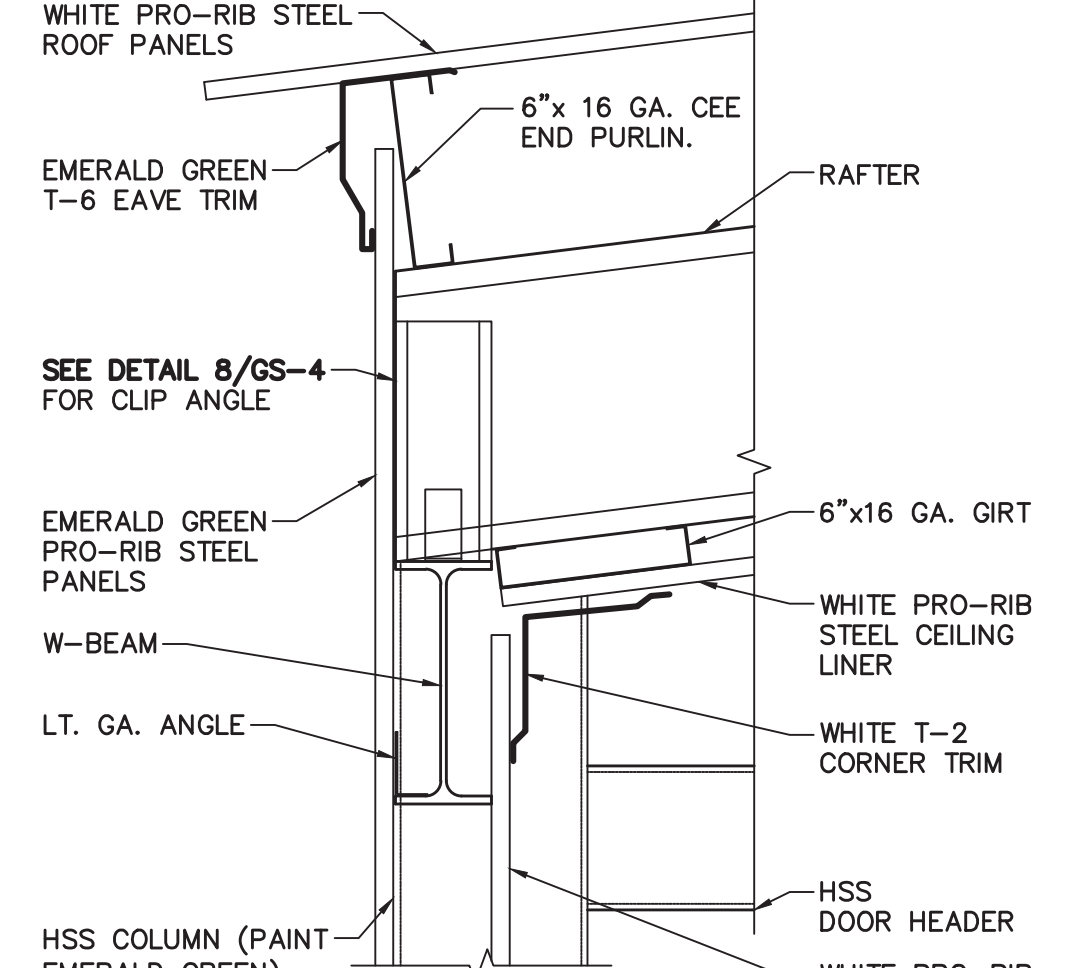
6 CORNER TRIM DETAIL
GA-1 SCALE: 1-1/2" = 1'-0"



7 TRIM DETAIL @ COLUMN
GA-1 SCALE: 1-1/2" = 1'-0"



8 TRIM DETAIL
GA-1 SCALE: 1-1/2" = 1'-0"



9 EAVE TRIM DETAIL
GA-1 SCALE: 1-1/2" = 1'-0"

MENARDS
STORE DESIGN DEPARTMENT
5101 MENARD DRIVE
EAU CLAIRE, WI 54703

BUILDING TYPE
GATE CANOPY

CAD DWG NAME
GA-1

PROJECT TITLE
BELLEVILLE

SHEET TITLE
PLAN AND DETAILS - FLOOR

REVISIONS

NO.	DATE	DESCRIPTION

STATE OF MICHIGAN
LA SHORNE
ARCHITECT
No. 1301066313
LICENSED ARCHITECT

SCALE
1/4"=1'-0"

DRAWN BY
POM

DATE
12/17/20

SHEET NO.

GA-1

107 Path and File Name: F:\Design\div\Proto5-sites\A-2017 SITES\Belleville - Van Buren Township, MI\2022 Remodel\Phase 1 - Express Lane\Construction Drawings\GA-1.dwg Date Plotted: May 26, 2022



Architectural elevation drawing of a building facade. The drawing shows a sloped roofline and a vertical wall section. Key features include:

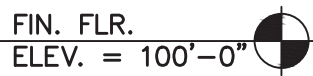
- Roofline:** A sloped roof with a 20'-0" addition. The roof pitch is indicated as 1 1/2".
- Signage:**
 - "EMERALD GREEN T-1 CORNER TRIM" at the roofline.
 - "EMERALD GREEN T-7 TRIM" on the vertical wall.
 - "WHITE PRO-RIB STEEL SIDING INSIDE FRONT WALL TO COVER ELECTRICAL CONDUIT" on the vertical wall.
 - "NEW OVERHEAD DOOR (BEYOND)" on the vertical wall.
 - "NEW WHITE T-4 BOTTOM TRIM" at the base of the vertical wall.
 - "THANK YOU FOR SHOPPING **MENARDS**" sign on the sloped roof.
 - "Out" sign on the sloped roof.
 - "CLEARANCE SIGN" and "NEW SIGN" on the vertical wall.
 - "DO NOT ENTER" signs on the ground level.
 - "STOP" sign on the ground level.
- Structural Details:**
 - "EXISTING HSS COLUMN" on the vertical wall.
 - "EXISTING BUILDING" and "EXISTING SIGN" labels.
 - "B.O. STEEL ELEV. = 115'-0"
- Dimensions:**
 - 20'-0" ADDITION
 - 11'-9"
 - 13'-9" (TO CMU WALL)
 - 35", 39", 44", 48", 53", 57", 62"
 - 187", 187"
- Notes:**
 - ALL SIGNS TO HAVE ADEQUATE BLOCKING FOR INSTALLATION.

2 REAR ELEVATION
GA-2 SCALE: 1/4" = 1'-0"

NOTES:
ALL SIGNS TO HAVE ADEQUATE BLOCKING
FOR INSTALLATION.

NEW "OUT" SIGN TO BE INSTALLED BY
CONTRACTOR.

ALL CLEARANCE SIGNS TO BE NEW AND
PLACED TO THE LEFT SIDE OF O.H. DOOR.



EXISTING ROOF PANEL

254" 254" 254" 254" 254" 254" 254" 254" 254" 254"

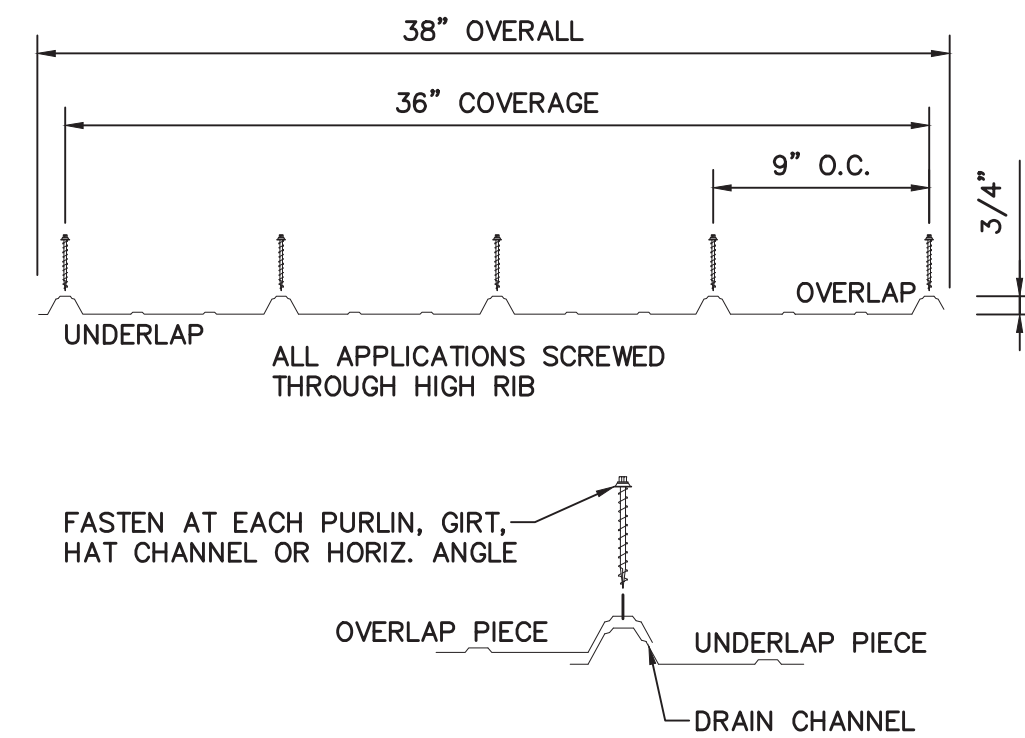
254"

6" UNDER LAP OF EXISTING PANEL

6" OVERHANG

STEEL PANEL NOTES:

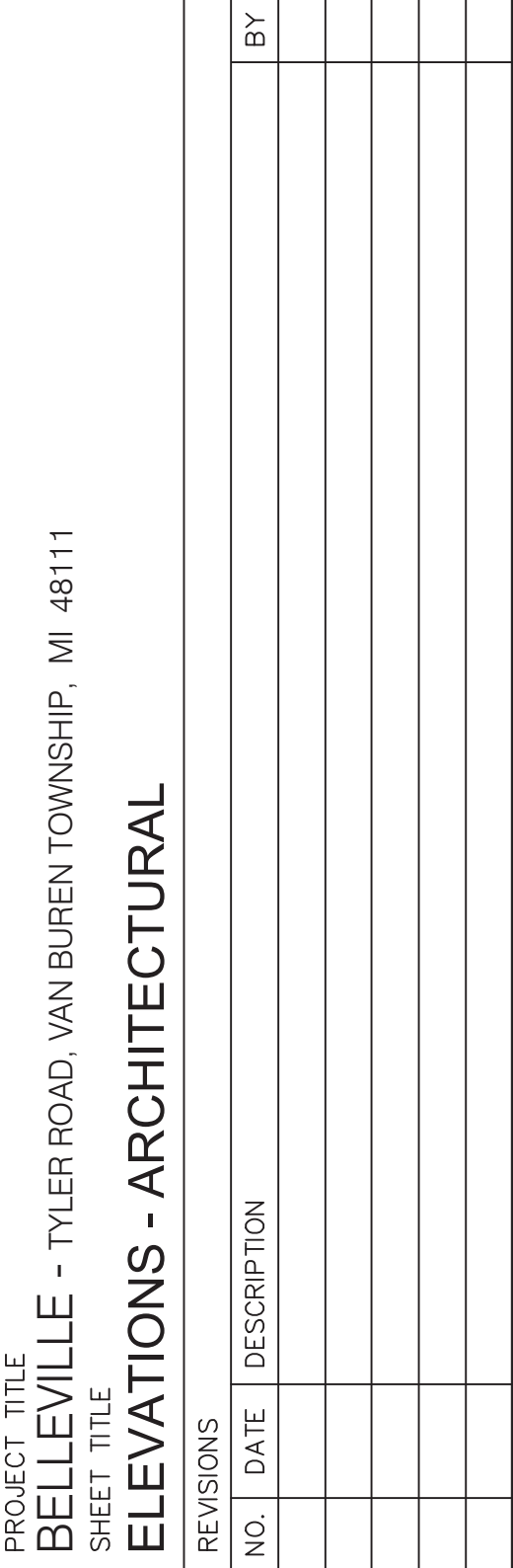
1. ALL WALL APPLICATIONS SCREWED THROUGH ALL HIGH RIBS IN A UNIFORM MANNER 2" SELF TAPPING SCREWS FOR ROOF AND 1 1/2" SCREWS FOR SIDE WALLS.
2. ALL STEEL SIDING SHOULD BE LAPPED AWAY FROM LINE OF SIGHT START FROM REAR OF BUILDING AND WORK YOUR WAY FORWARD.
3. ALL STEEL SIDING AND ROOF STEEL TO BE "MIDWEST MANUFACTURING" PREMIUM PRO-RIB (U.N.O.)
4. (TYP.) COLORS ARE "MM" EMERALD GREEN AND "MM" WHITE (U.N.O.)
5. ALL "MM" STEEL TO BE SUPPLIED BY MENARD, INC. (U.N.O.)



6
GA-2

TYPICAL STEEL PANEL

SCALE: 1-1/2" = 1'-0"



PROJECT TITLE
BELLEVILLE - TYLER ROAD, VAN BUREN TOWNSHIP, MI 48111
SHEET TITLE

ELEVATIONS - ARCHITECTURAL



SHEET NO.

GA-2



MCKENNA

July 20, 2022

Dan Power
Director of Planning and Economic Development
Charter Township of Van Buren
46425 Tyler Road
Van Buren Township, Michigan 48111

Subject: Side Entry Garage and Building Separation Requirement

Dear Mr. Power:

Per your request, we have reviewed zoning ordinances in other municipalities to see if their regulations have minimum requirements for side entry garage and what (if any) the separation requirements are between residential buildings. The issue has come up in recent discussions with prospective developers regarding completion of long-incomplete residential PRD's and new proposals.

The Township's PRD Ordinance was written a long time ago and no PRD projects have been reviewed in the past decade. Home building and residential development patterns have gone through various market driven changes in this duration. As a result, some of the current regulations make proposed new attainable housing projects located on smaller lots un-feasible. We reviewed the regulations of other comparable municipalities and have summarized our findings as follows:

Community: Lyon Township

Lyon Township does not regulate number/percentage of side entry garage lots. Side entry garage is an option offered for residential single-family subdivisions as follows:

- (1) To accommodate side entry garages in a conventional single-family residential development, planned development and average lot size development in the R-0.5 and R-0.3 zoning districts, houses may be offset, with a minimum side setback of five feet on one side, provided that the minimum distance between houses shall be 30 feet. This option shall be permitted only on blocks where, prior to any housing construction, the minimum side setbacks and garage orientation have been designated for every lot on the approved final preliminary plat or site condominium plan.
- (2) To accommodate side entry garages in a single-family residential planned development or average lot size development in the R-1.0 zoning districts, houses may be offset, with a minimum side setback of 15 feet, provided that the minimum distance between houses shall be 40 feet. This option shall be permitted only on blocks where, prior to any housing construction, the minimum side setbacks and garage orientation have been designated for every lot on the approved final preliminary plat or site condominium plan.

In addition to the above, they have a PUD ordinance which states Any detached single-family structure shall be located at least 30 feet from any other detached single-family structure and shall provide a minimum side yard setback of 15 feet on both sides. To accommodate side entry garages in a single-family subdivision or site condominium, houses may be offset, with a minimum side setback of ten feet on one side, provided that the minimum distance between houses shall be 30 feet. This option shall be permitted only on blocks where,

HEADQUARTERS
235 East Main Street
Suite 105
Northville, Michigan 48167

○ 248.596.0920
F 248.596.0930
MCKA.COM

Communities for real life.

prior to any housing construction, the minimum side setbacks and garage orientation have been designated for every lot on the approved final preliminary plat or site condominium plan. This requirement conflicts with option (1) above.

Assessment: Side entry garages are not mandated but are an option if desired. Building separation requirements varies between 20 and 40 feet; however, the Planning Commission and Board of Trustees have waived setback standards and modified building separation requirements in recent years to 10-20 feet.

Community: Plymouth Township

Plymouth Township does not require a set percentage of side entry garages. It is just mentioned as an optional feature and is left up to Planning Commission discretion. The ordinance requires only 10 feet separation between single-family, two-family and cluster housing development.

Assessment: Side entry garages are not mandated but are an option if desired. The Township, however, has faced a lot of drainage issues between abutting properties because of the small separation requirement.

Community: Canton Township

Canton Township does not mandate side entry garage percentages. However, the Ordinance does provide a provision for it in the Schedule of Regulations:

Side yard setbacks in the R-2 and R-3 district. Side yard setback requirements in the R-2 and R-3 district may be modified to permit the construction of a single-family home with attached garages utilizing a side facing, or courted entrances. To utilize these modified yard requirements the following conditions must be met: (1) the minimum width of all lots/units within the development must be 85 feet, and (2) the subdivision covenants and restrictions (or in the case of a condominium project, the master deed) must clearly reference these modified setbacks, specifically prohibit the construction of homes with garage doors that face the public street upon which the lot fronts, and prohibit garage doors that face the rear line of any lot.

In the case of a home with a side facing or courted garage, a minimum side yard setback of five feet shall be maintained; and provided further: (1) that in each such instance the combined side yards on such lot shall total at least 30 feet; (2) the five-foot side yard shall not abut a five-foot side yard or [on] any adjacent lot; (3) the minimum distance between dwellings with attached garages facing the rear side lot line on adjacent lots shall not be less than 30 feet; and, (4) the minimum distance between dwellings on adjacent lots shall not be less than 20 feet.

Assessment: Side entry garages are not mandated but are an option if desired. However, minimum building separation in any single-family residential district is 20 feet.

Community: Pittsfield Township

Pittsfield Township does not mandate side entry garages or regulate them. Accessory structures are regulated based on attached or detached only.



Building separation requirement in single family residential districts varies depending on lot size from 10 feet to 40 feet. However, the separation for detached single family dwellings in a PUD or open space development is 10 feet.

Assessment: Side entry garages are not mandated but are an option if desired. However, minimum building separation in any single-family residential district is 10 feet.

Community: City of Novi

The City of Novi does not mandate side entry garages. However, the accessory structures provision of the ordinance states that Wherever possible, side entry garages shall be encouraged in residential districts.

With regard to building separation for single family residential dwelling in a cluster or open space development, the ordinance states:

Each cluster of attached or detached one family homes shall be separated from any other cluster of one-family homes by a distance determined by the number of homes in opposing clusters as set forth in a scale; except that the Planning Commission after review of site development plans, may modify the strict application of the distance in those instances where it is found that a natural amenity would be destroyed or topographical or soil conditions limit a practical dimensional separation of clusters.

The ordinance further states: Where a non-residential use abuts a one-family residential district, the minimum building setback shall be five (5) feet for each foot of building height. The placement of residential uses adjacent to one-family and multi-family dwelling districts is encouraged. For interior buildings within a site, buildings with a front-to-front relationship shall have a minimum separation of thirty (30) feet. All other interior buildings shall have a minimum separation of fifteen (15) feet (thirty (30) feet for buildings thirty (30) feet or more in height) unless otherwise provided for in this District.

Assessment: Side entry garages are not mandated but strongly encouraged. The language regarding building separation is difficult to interpret and apply and not user-friendly.

Community: Ypsilanti Township

Ypsilanti Township requires all new subdivisions and Planned Residential Developments to have 40% of lots to have side entry garages or recessed garages. Section 1909 of the Ordinance for PD's states:

Residential architecture. Residential facades shall not be dominated by garages; at least 40 percent of residential units shall have side entry garages or recessed garages where the front of the garage is at least five feet behind the front line of the living portion of the principal dwelling. The intent of encouraging recessed or side entry garages is to enhance the aesthetic appearance of the development and minimize the aesthetic impact resulting from the close clustering of units allowed under these regulations.

Similarly, Section 04.03 of the Subdivision Ordinance states: A minimum of forty percent (40%) of residential units shall utilize side entry garages, rear-entry detached garages accessed from a common alley, or recessed garages where the front of the garage is set back a minimum of five feet (5') from the front building line of the dwelling.

Building separation distances for single family residential dwellings vary from 10 feet to 24 feet. These distances can be modified in a PD



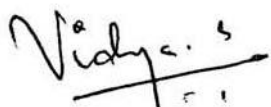
Assessment: Flexible ordinance language addresses garages by requiring side entry or recessed garages. Minimum building separation varies with a minimum separation distance of 10 feet.

Recommendation

In all the Ordinances we compared, side entry garages were a desirable option but not a mandate. Ypsilanti Township ordinance offered a creative alternative of side entry or recessed garage placement. Building separation requirements varied significantly, with a majority have a separation requirement of only 10 feet or allowing for Planning Commission and Board of Trustees to reduce that distance based on preservation of natural features or other factors.

The comparison data can provide a starting point for the Planning Commission to discuss the existing provisions in the Van Buren Zoning Ordinance. We are available to answer any of your questions or provide additional research.

Respectfully,
McKENNA



Vidya Krishnan
Senior Principal Planner





MEMO

TO: Van Buren Township Planning Commission
FROM: Dan Power– Director of Planning and Economic Development
RE: Discussion of Zoning Ordinance Text Amendments: Gasoline Filling and Service Stations
DATE: July 8, 2022

The Planning Commission will consider Proposed amendments to the following sections of the Van Buren Charter Township Zoning Ordinance:

1. Section 3.111. C-1, General Business District, sub-section (B) Permitted Uses; move gasoline filling stations to sub-section (C) Special Land Uses.
2. Section 3.112. C-2, Extensive Highway Business District, making change to C-1 district will automatically move gasoline filling stations from sub-section (B) Permitted Uses to sub-section (C) Special Land Uses.
3. Section 3.113. FS, Freeway Service District, sub-section (B) Permitted Uses; move gasoline filling stations and gasoline service stations to sub-section (C) Special Land Uses.
4. Create new Section 5.144. Gasoline or Automobile Filling Stations under Article 5. Development Standards for Specific Uses.

The Planning Commission is requested to hold a public hearing on these proposed Zoning Ordinance text amendments and consider recommending their approval to the Van Buren Township Board of Trustees.

For your reference, I have included the latest draft version of the proposed amendments and a copy of the minutes of the last discussion on this topic which was held at the regular Planning Commission meeting on June 8th, 2022.

Thank you for your consideration.

Sincerely,

A handwritten signature in black ink, appearing to read 'Dan Power', with a stylized flourish at the end.

Dan Power, AICP
Planning and Economic Development Director
Charter Township of Van Buren Public Services Department



May 24, 2022

Planning Commission
Charter Township of Van Buren
46425 Tyler Road
Belleville, Michigan 48111

Subject: Automobile filling stations

Dear Commissioners:

The Planning Commission at its meeting held on April 27, 2022 considered a zoning text amendment to re-classify gasoline filling stations in the Township Zoning Ordinance.

The ordinance currently allows the use as a principal permitted use in the C-1 (Local Business), C-2 (Extensive Highway Business) and FS (Freeway service) districts. There are no specific site design standards governing the use either. We reviewed zoning ordinances in other municipalities to see how the use is classified. The municipalities chosen were based on their location with a major arterial, freeway or trunkline traversing through it such as I-275, I-94, I-96, US-24, US-23, US-12 etc., which are generally seen with a lot of filling stations. Based on our review, Van Buren Township is the only municipality that permits gas stations as a principal use, with the exception in the City of Novi. Gasoline fillings stations are considered as a use that requires placement consideration because of concerns related to access, circulation, fire hazard, odor, hours of operation etc.

We presented a text amendment which also included a reference to EV (Electric Vehicle) charging stations. The Planning Commission expressed concern about handicap accessibility being affected, pedestrian walkways being blocked, long duration parking at these spots and the possibility of a row of EV charging stations on a site and directed us to look into the subject. Upon further research we believe the EV charging stations may need an ordinance to regulate them separately from gasoline filling stations. We have collected some information and hope to present it as a separate text amendment in the future.

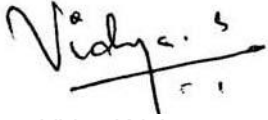
At this time the amendment is limited to gasoline filling stations only. Following are the proposed changes:

ZONING ORDINANCE CHANGES

1. §3.111. C-1, General Business District, sub-section (B) Permitted Uses; move *gasoline filling stations* to sub-section (C) Special Land Uses.
2. §3.111. C-2, Extensive Highway Business District, making change to C-1 district will automatically move gasoline filling stations from sub-section (B) Permitted Uses to sub-section (C) Special Land Uses.
3. §3.113. FS, Freeway Service District, sub-section (B) Permitted Uses; move *gasoline filling stations* to sub-section (C) Special Land Uses.
4. Create new Section 5.144. Gasoline or Automobile Filling Stations under Article 5. Development Standards for Specific Uses.

The item is on the Planning Commission's agenda for scheduling of a public hearing at this time, prior to making a recommendation to Township Board of Trustees. We will be present at the Planning Commission meeting to answer any questions and concerns.

Respectfully,
McKENNA

A handwritten signature in black ink, appearing to read "Vidya Krishnan", with a horizontal line drawn across the middle of the signature.

Vidya Krishnan
Senior Principal Planner

Cc: Dan Power, Director of Planning and Economic Development

Section 5.144 Gasoline or Automobile Filling Stations

- (1) **Minimum lot area and width:** There is no minimum lot area and width requirement; however, sites are required to comply with all setback, landscaping greenbelt and parking requirements as established in the Ordinance.
- (2) **Front yard setback** measured from any right-of-way line:
Canopy: 40 feet.
Pump and/or pump island: 50 feet.
Building: 85 feet.
- (3) **Side or rear yard setback:**
Canopy: 30 feet.
Pump and/or pump island: 40 feet.
Building: 30 feet.
- (4) **Side or rear yard setback abutting residential zoned property:**
Canopy: 40 feet.
Pump and/or pump island: 50 feet.
Building: 40 feet.
- (5) **Ingress and egress.** The following requirements shall be applicable for ingress and egress:
 - (a) Ingress and egress drives shall be a minimum of 30 feet and a maximum of 36 feet in width. Curb radii shall be adequate to accommodate the turning radii of delivery trucks. There shall be a clear unobstructed driveway area provided to the loading area which is not in conflict with other vehicular patterns on the site.
 - (b) No more than one such drive or curb opening shall be permitted for every 75 feet of frontage (or fraction thereof) along any street. The nearest edge of any such drive shall be located at least 25 feet from the nearest point of any property zoned or used for residential purposes.
 - (c) Ingress and egress for drives shall be designated and located in a manner that does not create a safety hazard or traffic nuisance because of its location in relation to other ingress and egress drives, its location in relation to the traffic generated by other buildings or uses, its location near a vehicular or pedestrian entrances or crossings, or similar concerns.
 - (d) Driveway design and placement must be in harmony with internal circulation and parking design such that the entrance can absorb the maximum rate of inbound traffic during a normal peak traffic period.
 - (e) There must be sufficient on-site storage to accommodate at least two queued vehicles waiting to enter or exit without using a portion of the public right-of-way obstructing existing vehicle sight distance, or otherwise interfering with street traffic.
 - (f) Provisions for circulation between adjacent parcels are required when possible through coordinated or joint parking systems.

- (g) Driveways shall be designed to accommodate all vehicle types having occasion to enter and exit the site, including delivery and refueling vehicles. There shall be clear delineation and/or separation, where appropriate, of entry and exit lanes within driveways.
- (h) Loading and unloading and refueling activities shall not hinder vehicle ingress or egress.
- (6) **Driveway spacing.** Driveway spacing will be determined as a function of operating speeds of the adjacent public road. Spacing will be determined according to the following minimum standards or to standards adopted by the appropriate governing road jurisdiction, whichever is more stringent:

Speed Limits (mph)	Minimum Spacing (feet)
25	105
30	125
35	150
40	185
45	230

Spacing is based on average vehicle acceleration and deceleration rates and is considered necessary to maintain safe traffic operation. Spacing will be measured from the midpoint of each driveway. In the event that a particular parcel or parcels lack sufficient road frontage to maintain adequate spacing, the applicant shall have one of two options:

- (a) A modification from minimum spacing can be sought from the planning commission, but in no case can the variance be greater than that which is necessary to provide safe and efficient access to the site.
- (b) The adjacent landowners may agree to establish a common driveway. In such cases, the driveway midpoint shall be the property line between the two parcels and shall meet standard specifications. The township may require maintenance agreements between owners of a common driveway to cover such issues as snow plowing, resurfacing liability, etc.
- (7) **Number of driveways:** A maximum of one two-way driveway opening or a pair of one-way driveway openings shall be permitted to a particular site from each adjacent public road. Based on the recommendation of the appropriate governing road jurisdiction and/or the township engineer, that an additional driveway is in the interest of good traffic operation, the planning commission may permit one additional driveway entrance along a continuous site with frontage in excess of 300 feet or two additional driveway entrances along a continuous site with frontage in excess of 600 feet. Common driveways shall be considered to be one driveway.
- (8) **Pumps.** All gasoline pumps shall be arranged so that motor vehicles do not park on or extend over abutting landscaped areas, sidewalks, streets, buildings, or adjoining property while being served.

- (9) **Off-street parking and loading.** Required off-street parking for automobile filling multiuse stations offering additional services shall be provided in accordance with the standards set forth in section 9.102 and shall be computed on the basis of each separate use as set forth in the schedule of off-street parking.
- (10) **Canopy structure.** Canopy structures shall be designed and constructed in a manner which is architecturally compatible with the principal building and the overlay district in which it is located in, if any. Support columns of any proposed canopy structure shall be covered with brick to match the principal building. Required fire protection devices under the canopy shall be architecturally screened so that the tanks are not directly visible from the street. The screens shall be compatible with the design and color of the canopy.
- (12) **Fueling operations.** Fueling operations designed to accommodate tractor trailer trucks shall be prohibited except on sites adjacent to an interstate freeway.
- (13) **Pedestrian circulation.**
- (a) Automobile filling/mixed use stations shall be designed in a manner which promotes pedestrian and vehicular safety.
 - (b) The parking and circulation system within each development shall accommodate the safe movement of vehicles, bicycles, pedestrians and refueling activities throughout the proposed development and to and from surrounding areas in a safe and convenient manner.
 - (c) Pedestrian walkway patterns shall be considered as an integral component of site design and shall be located to connect areas or points of pedestrian origin and destination. Where it is necessary for the pedestrian access to cross drive aisles, crossings shall emphasize and place priority on pedestrian access and safety. The pedestrian crossings must be well-marked and clearly visible, using pavement treatments, signs, striping, signals, and lighting.
 - (d) Clear identification and delineation between the drive-thru facility and parking lot shall be provided. Drive-thru facilities shall be designed in a manner which promotes pedestrian and vehicular safety, and do not interfere with access to parking and maneuvering lanes.
- (14) **Signs.** All signage installed on the site, including the canopy and pumps shall comply with the regulations set forth in Article 11 of this ordinance.

**CHARTER TOWNSHIP OF VAN BUREN
PLANNING COMMISSION
NOTICE OF PUBLIC HEARING**

Notice is hereby given that the Charter Township of Van Buren Planning Commission will hold a public hearing on **July 13, 2022 at 5:30 p.m.** The meeting will be held in person at Van Buren Township Hall, located at **46425 Tyler Road, Van Buren Township, MI 48111**, in the Board Room. Members of the public may additionally participate in the meeting electronically per guidance provided by the Wayne County Local Public Health Department for meetings of governmental bodies held under public act 228 of 2020, effective January 1, 2022. Instructions for attending the meeting electronically are provided in this notice. The Planning Commission will consider the following request:

Zoning Ordinance Text Amendments: Proposed amendments to the following sections of the Van Buren Charter Township Zoning Ordinance:

1. Section 3.111. C-1, General Business District, sub-section (B) Permitted Uses; move gasoline filling stations to sub-section (C) Special Land Uses.
2. Section 3.112. C-2, Extensive Highway Business District, making change to C-1 district will automatically move gasoline filling stations from sub-section (B) Permitted Uses to sub-section (C) Special Land Uses.
3. Section 3.113. FS, Freeway Service District, sub-section (B) Permitted Uses; move gasoline filling stations and gasoline service stations to sub-section (C) Special Land Uses.
4. Create new Section 5.144. Gasoline or Automobile Filling Stations under Article 5. Development Standards for Specific Uses.

Members of the public may access the agenda materials via the Township website – www.vanburen-mi.org beginning **July 8, 2022**. On the meeting website, members of the public will also gain access to **means of participating in the electronic meeting. On the agenda for the meeting**, a unique Zoom weblink and dial-in phone number with a meeting ID for meeting videoconference or teleconference access will be provided. The meeting will be available to view live on the Van Buren Township YouTube Channel which can be reached from the live meeting link located on the Van Buren Township website home page (www.vanburen-mi.org). Closed captioning will be available after YouTube fully renders meeting video.

A complete **procedure for public comment by electronic means** is provided on a guide which is accessible on the Van Buren Township website (www.vanburen-mi.org). Participants may also choose to submit written comments that will be read into record during public comment by the Chairperson. Submit any written comments or questions about the meeting via e-mail to dpower@vanburen-mi.org.

To comply with the Americans with Disabilities Act (ADA), any citizen requesting accommodation to attend a meeting, and/or to obtain this notice in alternate formats, please contact the Clerk's Office at 734.699.8900 extension 9205.

Posted: June 20, 2022

Published: June 23, 2022

ITEM #4: DISCUSSION: GASOLINE FILLING STATION AND GASOLINE SERVICE STATION ZONING ORDINANCE TEXT AMENDMENT.

Director Power gave a brief presentation. The Planning Consultant has provided draft Zoning Ordinance amendment language based on the feedback received at the April 27, 2022 meeting. Director Power deferred the presentation to Vidya Krishnan of McKenna Associates.

Vidya Krishnan of McKenna Associates presented her letter dated 5-24-22. McKenna Associates presented a text amendment which included a reference to EV (Electric Vehicle) charging stations. The Planning Commission expressed concern about handicap accessibility being affected, pedestrian walkways being blocked, long duration of parking at these spots and the possibility of a row of EV charging stations on a site and directed McKenna Associates to look into the subject further. Upon further research, EV charging stations may need an ordinance to regulate them separately from gasoline filling stations. McKenna has collected information and hopes to present it as a separate text amendment in the future. At this time the amendment is limited to gasoline filling stations only and the following are the proposed changes:

1. §3.111. C-1, General Business District, sub-section (B) Permitted Uses; move gasoline filling stations to sub-section (C) Special Land Uses.
2. §3.111. C-2, Extensive Highway Business District, making change to C-1 district will automatically move gasoline filling stations from sub-section (B) Permitted Uses to sub-section (C) Special Land Uses.
3. §3.111. FS, Freeway Service District, sub-section (B) Permitted Uses; move gasoline filling stations to subsection (C) Special Land Uses.
4. New Section 5.144. Gasoline or Automobile Filling Stations under Article 5. Development Standards for Specific Uses.

The item is on the Planning Commission's agenda to schedule a public hearing, prior to making a recommendation to the Township Board of Trustees.

Commissioners agreed they are ready to schedule the public hearing.

No comments from the audience or remote viewers.

Motion Jahr, Cullin second schedule the public hearing for the Gasoline Filling and Gasoline Service Station Zoning Ordinance text amendment.

Roll Call:

Yeas: Barr, Culling, Jahr and Kelley.

Nays: None.

Excused: Budd and Atchinson.

Motion Carried.