

**AGENDA**  
**CITY OF STURGEON BAY**  
**AESTHETIC DESIGN & SITE PLAN REVIEW BOARD**

Monday, September 28, 2020

6:00 p.m.

Council Chambers, City Hall  
421 Michigan Street, Sturgeon Bay, WI

1. Roll call.
2. Adoption of agenda.
3. Approval of minutes from September 14, 2020.
4. Consideration of: Accessory buildings for Apple Storage, located at 1427 Green Bay Road.
5. Consideration of: 14' x 165' addition to the 422 Building for Fincantieri Bay Shipbuilding, located at 605 N. 3<sup>rd</sup> Avenue.
6. Adjourn.

*NOTE: DEVIATION FROM THE AGENDA ORDER SHOWN MAY OCCUR.*

Committee members

Mark Lake – Chair

Dennis Statz – Vice-Chair

Jon Burk

Cindy Weber

Mark Struck

Dave Augustson

Cheryl Frank

9/23/20  
4:00 p.m.  
CN

AESTHETIC DESIGN AND SITE PLAN REVIEW BOARD  
Monday, September 14, 2020

The Aesthetic Design and Site Plan Review Board meeting was called to order at 6:00 p.m. by Chairperson Mark Lake in the Council Chambers, City Hall, 421 Michigan Street.

**Roll Call:** Members Dave Augustson, Cindy Weber, Mark Lake, Dennis Statz, Jon Burk, and Mark Struck were present. Excused: Member Cheryl Frank. Also present were City Administrator Josh VanLieshout, Community Development Director Marty Olejniczak, Planner/Zoning Administrator Christopher Sullivan-Robinson, and Community Development Secretary Cheryl Nault.

**Adoption of Agenda:** Moved by Mr. Burk, seconded by Mr. Struck to adopt the following agenda by moving item #4 to item #6:

1. Roll call.
2. Adoption of agenda.
3. Approval of minutes from August 24, 2020.
4. Consideration of: South Yard construction plans for Fincantieri Bay Shipbuilding, located at 605 N. 3<sup>rd</sup> Avenue.
5. Consideration of: New construction for Peninsula Title, located at 512 S. Lansing Avenue.
6. Consideration of: Second floor addition for Marcus Trana, located at 43 S. 2<sup>nd</sup> Avenue.
7. Chairman approvals.
8. Adjourn.

All ayes. Carried.

**Approval of minutes from August 24, 2020:** Moved by Ms. Weber, seconded by Mr. Burk to approve the minutes from August 24, 2020, with removal of a repeated sentence on the bottom of page 1. All ayes. Carried.

**Consideration of: New construction for Peninsula Title, located at 512 S. Lansing Avenue:** Portside Builders representative Paul Shefchik presented plans for the new Peninsula Title building. The owners of the property are John and Julie May. There will be a 4 – 5 inch stone exterior, with brown wood LP Smartside siding. The siding has a stonebase paint that gives it its' hardness, along with a 30-year warranty against any fading or deterioration. There will be a pitched roof in the center, along with flatter roofs with a pitch on both ends, which is a standing seam metal roofing in a dark grayish color. As far as the site, it will be less impervious than its existing condition. The existing parking lot will be used, but some grades will be changed and some re-blacktopping will be done. Landscaped islands will be added in the parking area to add more green space. There is an approximate 15-foot tall pole light located in the parking lot that they would agree to facing the lights face downward, if necessary. In front of the doorways there is an overhang roof where recessed can lights could be added that would shine down onto the entrances and sidewalks, if requested. An easement was given to Marchants, the property next door, for use of the parking area behind their building.

Mr. Struck wondered if it was necessary to have such a high pole light in the parking lot. Most of their business is conducted during the day.

Ms. Weber pointed out that the light covered the whole big parking area.

Mr. Shefchik stated that they were hoping not to install another light.

Mr. Sullivan-Robinson stated lighting requirements are listed in the aesthetic guidelines. They need to be night sky friendly, efficient LED, and full cut-offs.

Mr. Shefchik stated that the parking lot lighting could be switched out to LED if requested by the Board.

Mr. Burk mentioned that their business is only a daytime operation. He wondered why they should have to change the lighting.

Ms. Weber complimented the design of the building, as well as the landscaping.

Mr. Shefchik stated that there will be employee parking on the lower level in the southwest corner. There is a 12-foot grade difference from the high to the low. Water will drain into the existing culvert. The existing entrances into the property will remain and no new ones will be added.

Mr. Sullivan-Robinson stated there are no landscape requirements since they are not increasing the parking area.

Mr. Shefchik stated that plant beds will be installed around the whole building. A sign will be placed in a landscaped island on the north side of property. They will come back to the Board for signage approval.

A short discussion was held. Moved by Ms. Weber, seconded by Mr. Burk to issue a certificate of appropriateness as presented. All ayes. Carried.

**Consideration of: Second floor addition for Marcus Trana, located at 43 S. 2<sup>nd</sup> Avenue:** Marcus Trana, owner of the property at 43 S. 2<sup>nd</sup> Avenue, stated that this has been a project in the making for a long time. He will be adding a second floor and white double-hung windows to his existing garage. The roof will be reused. The purpose of the second floor is for a recording studio, art gallery, and storage. He will keep the first floor car garage for parking. The existing metal siding will be replaced with a green, vertical vinyl siding to closely match the color of the house. He hopes this will be adding another property to the Arts District. He also provided letters of recommendation from the neighbors.

The Board decided that the final color of the siding should be approved by the Chair.

Mr. Trana mentioned that a future project is to remove the lap siding on the house, which contains asbestos, and to expose the original siding.

Mr. Sullivan-Robinson stated that Mr. Trana is working with the building inspector on handicap accessibility.

Mr. Trana said he is not changing any landscaping. He intends to use the yard to play concerts for family and friends. There will be no changes to the back door. The existing garage doors will remain. Nothing will be done to the footprint of the building.

Mr. Statz encouraged Mr. Trana to match the siding on the garage with what the future house will look like once the existing siding is removed. Mr. Trana responded that it will be years from now before that will be done. It is a cost issue, but he will look into the lap vinyl siding for the garage.

The Board also agreed that any future lighting should be facing downward, and should be approved by Mr. Sullivan-Robinson and the Chair, as well as any revisions, including signage.

Mr. Trana added that in regard to noise, he will be insulating from the outside so the noise will be controlled.

The Board discussed the request. Moved by Mr. Struck, seconded by Mr. Statz to issue a certificate of appropriateness as presented, subject to using a horizontal lap siding, with a sample of siding color to be approved before work is done. Carried, with Mr. Struck, Mr. Augustson, Mr. Lake, Mr. Burk, and Mr. Statz voting aye. Ms. Weber voted no. She didn't care whether or not Mr. Trana used horizontal or vertical siding.

Mr. Lake stated that before the Board gets to the next item, there was request for public comment.

Hans Christian, 330 N. 3<sup>rd</sup> Avenue, stated that he has a recording studio 330 for personal and professional use. He said he is deeply concerned about Fincantieri with the recent approval of variances given by the

Zoning Board of Appeals and has decided to go to Circuit Court with it. There is an appeal period that is in effect right now until September 28th, so they have due process, 30 days, to exercise their rights. He brought up the aesthetic design guidelines, as he was concerned with building 433, since the 110-foot height would severely diminish public views. He suggested to move the building southward about 50 feet, so it would be lined up with the current building 422. The building could also be rotated 90 degrees to be made parallel with building 422. The problem with the current plans is the increased traffic for steel deliveries and other heavy machinery. He wondered if it could be required that the steel be delivered by barge. That would benefit the whole neighborhood. The building design is to be aesthetically pleasing. The Schmelzer building is located right next to the 59-foot high 420 building. Their property values will be severely affected. In regard to lighting, there have been numerous complaints, mostly from people across the water, about excessive lighting, but Fincantieri has not addressed the problem. Light pollution should be addressed.

Ms. Weber asked Mr. Christian what exactly he is appealing.

Mr. Christian responded that he is appealing the decision of the Zoning Board of Appeals granting of the two variances, height variance and setback variance.

Mr. Sullivan-Robinson explained that the ZBA approved the height and setbacks, with the condition that they provide a public improvement plan within 6 months for their parking areas and landscaping along 3<sup>rd</sup> Avenue, the appearance and use of the existing Red Oak Winery and train depot buildings, as well as other temporary steel buildings in that corridor.

**Consideration of: South Yard construction plans for Fincantieri Bay Shipbuilding, located at 605 N. 3<sup>rd</sup> Avenue:** Ryan Hoernke, representing Fincantieri Bay Shipbuilding, and lives at 908 Quincy Street, gave a description of their sister shipyard contract for the Navy's future frigates, which is primarily what has driven this project called the South Yard expansion and improvement. The 60,000 square-foot addition to the 420 building, which is the building at the former 1<sup>st</sup> Avenue and Jefferson Street, will expand and continue along the edge of the current building as approved by the ZBA. A 5-foot setback from the property line will continue west toward the water. Along the property line, behind the addition, they envision the fence to continue to the water line. The roof heights are lower than the existing building. The design will be the same, as well as colors, scheme, fake windows, and overall appearance. There is landscaping along the south property line and it is envisioned that landscaping will continue along the property line to the west. This building is currently used for modular steel construction, steel cutting, and some of the work for the existing contracts that their sister shipyard has. A portion of the building will be used for cold steel storage warehouse and the north side of the building will house the blast and prime equipment. This building overall will become the steel processing center for the program.

Mr. Hoernke then discussed the 433 building. This is a new building that will be located in the middle of the South Yard parallel to Berth Two. There is an existing building there used for some of the modular painting that will be removed, along with a quonset hut. It will be about 410 feet long, 210 feet wide, and approximately 110 feet high, with a peaked roof in the middle. It would have large megadoors on both the west water side and east landward side for units moving in and out of the building. There would be crane rails that extend past the entrance for maneuvering, flipping units, and loading onto transporters if needed. The general appearance will be the same, with a gray color scheme and black windows, with a membrane roof peaked in the middle. This building will be used for the three dimensional phase of construction of the military programs. There will be multiple workstations in each bay. The entire site from 311 to the south will be paved. Everything to the west of the former 1<sup>st</sup> Avenue will be paved. The variance that was approved stated that the buildings can be located within 3 feet of the location as presented, with a condition that within 6 months a beautification and improvement plan for 3<sup>rd</sup> Avenue is to be submitted.

Mr. Hoernke added that 422 building is 54 feet tall. The 311 building is approximately 84 feet tall and the gantry crane is 176 feet tall.

Mr. Olejniczak stated that he and Mr. Sullivan-Robinson met with Ross Schmelzer, the immediate property owner to the south. He had concerns regarding the 420 building.



Mr. Sullivan-Robinson explained Mr. Schmelzer's concerns. The first concern was drainage and flooding. Currently, the downspouts leading off of the existing building lead into a drainage easement between Bay Ship and Mr. Schmelzer's property. A lot of the roof water from the Bay Ship building contributes to a flooding issue in his parking area. The second concern is that there is an existing landscape line that goes between the south side of the building and his property. He wants to make sure that continues to the water, and what they use for vegetation does not shed leaves in the fall. Thirdly, the noise, dust, fumes, etc. needs to be considered. The exhaust needs to be filtered and noise regarding the construction of the building should be insulated and soundproofed. Lastly, his and his tenants view of the waterfront will be lost and wants the building aesthetically pleasing.

Mr. Olejniczak added that the primeline will be closer on a concrete surface and Mr. Schmelzer thought there might be some type of vibration that might occur that could impact his building during manufacturing.

Mr. Hoernke stated that the steel storage area will be the area to the right of the two truck doors in the back. Everything behind the existing building to the right is just for steel storage. To the left of the doors will be rollers that will feed to the east moving plates down the blast and prime and come out the east end of the addition short of where the existing building is. The blast and prime is the north side of the building. The north side of the existing building is the processing center. In terms of noise, one buffer is the wall. Then there is the steel storage area and another wall, then the blast and prime. The building has insulated panel walls.

Peter Glassen, representing Fincantieri Bay Shipbuilding, stated that the drainage currently goes into a pipe that flows out to the bay from the existing roof. Everything on the north side will be channeled out away from the Schmelzer property. This is the first they have heard of Mr. Schmelzer having drainage issues.

Mr. Olejniczak stated that Mr. Schmelzer indicated that some of the drainage is because of the wall being tall. If the rain is coming from the south, it is not rain on the roof draining into the gutters, it is rain hitting the building and coming down. Mr. Olejniczak thought that with the bay water being so high, the drain could be full. Graef may have some suggestions on how to move that water away from the existing pipe. The DNR will ask for some best management practices for the quality. They will also need an air quality permit from the DNR.

Mr. Olejniczak recommended that if the Board chooses to take action that it is done with two different motions, one for each building.

Ms. Weber didn't see a problem with building 433 if it is meeting all the DNR and regulatory requirements. It is such an internal building. Aesthetically, she didn't think it will affect much outside.

Mr. Lake didn't think that there was any difference between 54 feet and 110 feet tall regarding blocking the view.

Mr. Statz asked that Fincantieri do as much as they can for the noise.

Mr. Olejniczak stated the cold steel storage itself is a buffer. The noisy part of the building is a fair distance away from the Schmelzer property.

Mr. Olejniczak added that this is not recommended to Council for approval. Also, the decision of the Board can be appealed to the Council.

Mr. Hoernke added that there will be no extra signage added and from a safety standpoint lights will be added for the door entries, along with cut-off light fixtures downward facing along the building.

Mr. Glassen stated there will be dark-sky lights installed for vessels. They will replace the lights that shine across the bay with downlights. It is still a project they are working on.

Discussion continued. Moved by Ms. Weber, seconded by Mr. Burk to issue a certificate of appropriateness as presented for building 433. All ayes. Carried.

Mr. Sullivan-Robinson wanted the Board to keep in mind that the 420 building will likely be shortened in some way on the water side since it is encroaching a setback into the navigable water. It could be shortened by 15-20 feet.

Mr. Glassen added that every time it rains and the storm sewer backs up, they have the same problem as Mr. Schmelzer. That is a very low area.

Mr. Olejniczak stated that piping the water directly into the bay seems reasonable as long as there is some kind of insurance that will work and the water won't back up.

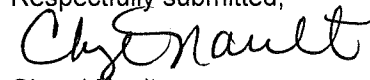
Ms. Weber agreed with Mr. Schmelzer's concerns, but the DNR and other regulatory bodies are doing their due diligence to make sure those issues are addressed.

After further discussion, it was moved by Ms. Weber, seconded by Mr. Burk to issue a certificate of appropriateness as presented for building 420. Ms. Weber, along with Mr. Burk's second, amended the motion to include subject to all lighting is to be downward facing and that the vegetation planted along the south side of the building be preferably evergreen arborvitae type plants, and any changes are to be approved by the Chair. All ayes. Carried.

Mr. Hoernke added that this project will be continuing employment and not necessarily additional jobs. There is estimated 100,000 production hours, with up to 400,000 production hours. They would like to have building 420 completed by the end of March, 2021 and building 433 completed by the end of July, 2021.

**Adjourn:** Moved by Mr. Struck, seconded by Mr. Augustson to adjourn. All ayes. Carried. The meeting adjourned at 7:51 p.m.

Respectfully submitted,



Cheryl Nault  
Community Development Secretary



# MEMO

To: Aesthetic Design and Site Plan Review Board  
From: Christopher Sullivan-Robinson  
Date: September 23, 2020  
Subject: Apple Storage Expansion

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Wolf River Partners, LLC is request approval for the construction of 4 additional storage buildings at their facility located at 1427 Green Bay Rd. This plan was seen back in 2018 when the committee approved building 1 and 2. The dimensions vary between the proposed buildings, but it is the same style of construction, materials, and lighting as previously approved. The City Engineer has already reviewed the stormwater management plan and didn't have any issues. The plan calls for a wet pond toward the southeast corner of the property. Any lighting to be used should be have full cutoff fixtures and be energy efficient. The zoning code is met with the proposal.

Based on committee guidelines and review criteria please review the following:

Site layout:

**The existing natural topographic and landscape features of a site should be incorporated into a development plan. Such plan shall include all prudent and necessary steps required to protect the natural environment of the site and surrounding areas during and after construction.**

Notes: There are wetlands to the south / southwest of this property none of which affect the proposed constructions. The applicant has worked with the DNR and the City Engineer with regards to stormwater management. Some vegetation will be removed to the south east for the installation of a wet pond.

**Where grading is necessary for the construction of structures and paved areas the grading should blend into adjacent properties. Abrupt changes in grade are discouraged.**

**Stormwater drainage shall be designed so as not to alter the natural drainage systems or cause flooding or erosion on neighboring properties.**

Notes: Stormwater management shows a wet pond be placed on the south east side of the property when the other storage additions are installed.

Access, circulation and parking:

**The interior circulation of the site shall be designed to provide for the convenient and safe flow of pedestrian and non-pedestrian traffic through the site and to/from public streets or sidewalks.**

Notes: They are proposing similar lane widths to the other storage buildings. They currently have one access driveway coming off the north east corner of the property and no additional entrances are proposed.

**Permeable surfaces, bioswales, rain gardens, and other forms of stormwater runoff prevention for parking and on-site traffic areas are encouraged.**

Notes: There is a depression/low area that will run partially on the west property line and a wet pond will be constructed on the south east property line. The driveway is expected to be paved within 24 months.

Materials and color:

**The use of identical building materials on all sides of a building that are visible from public streets is encouraged.**

Landscaping:

**The appearance of paved areas should be enhanced through landscaping.**

Notes: Areas of opportunity would be around the pond, the depression, and along the east and west property lines. There are some trees being removed to install the storm pond. It should be required to maintain a vegetative screen to the manufactured home park to the east.

**Vegetative landscaping shall be used to soften the appearance of blank walls. The use of native and indigenous plant species is encouraged over exotic species.**

Notes: Any removed vegetation should be encouraged to be replaced in other areas of the property. Native and indigenous plants should be used.

Lighting:

**Exterior light fixtures shall be shielded such that direct beams of light are not cast skyward or onto adjoining properties. Overly intensive lighting is discouraged. Energy efficient lighting is encouraged.**

Notes: The plan calls for shielded security lights, which are the same lights used on other buildings within the site.

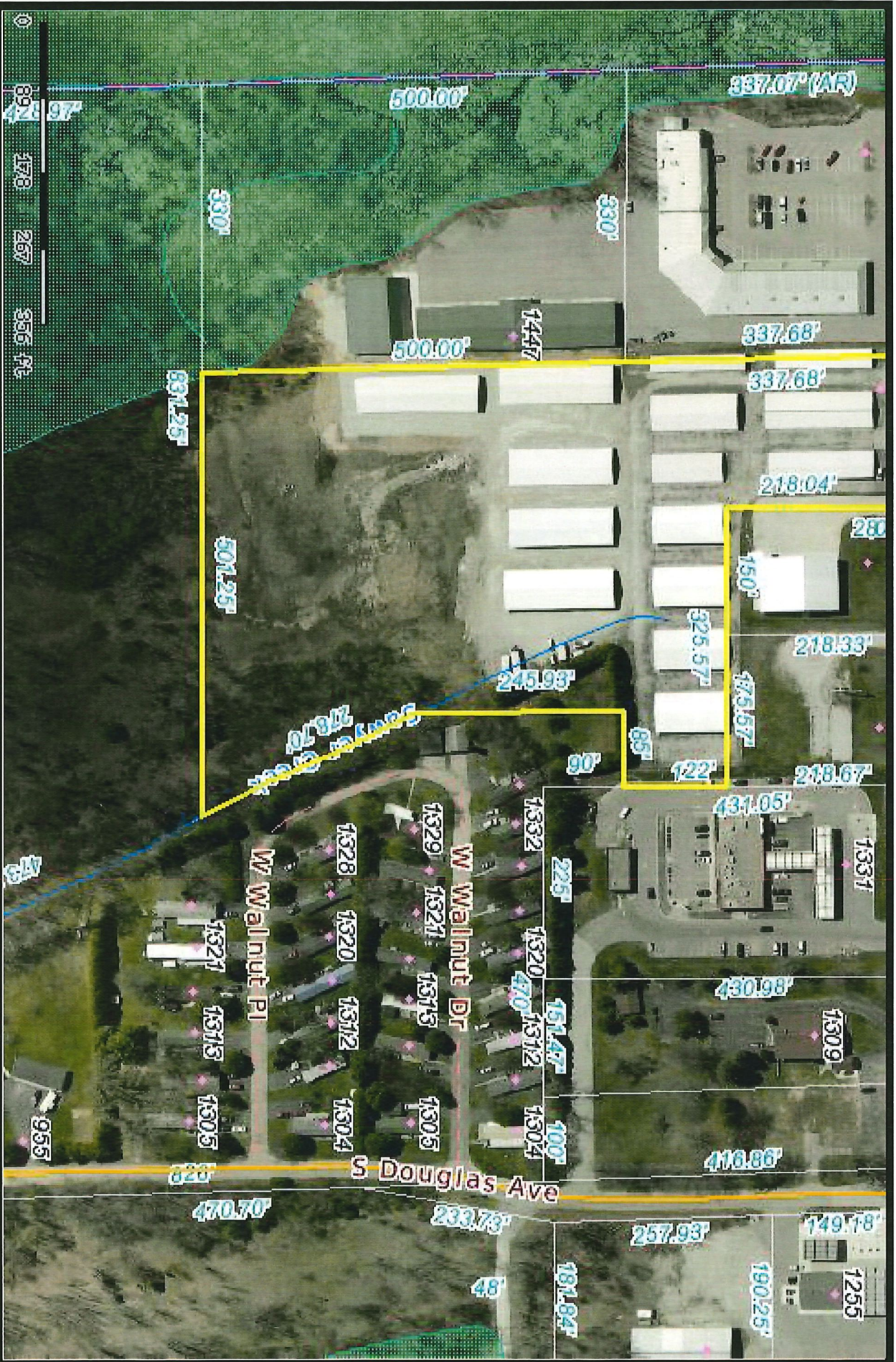


Printed 09/23/2020 courtesy of Door County Land Information Office

( //www.co.door.wi.gov )

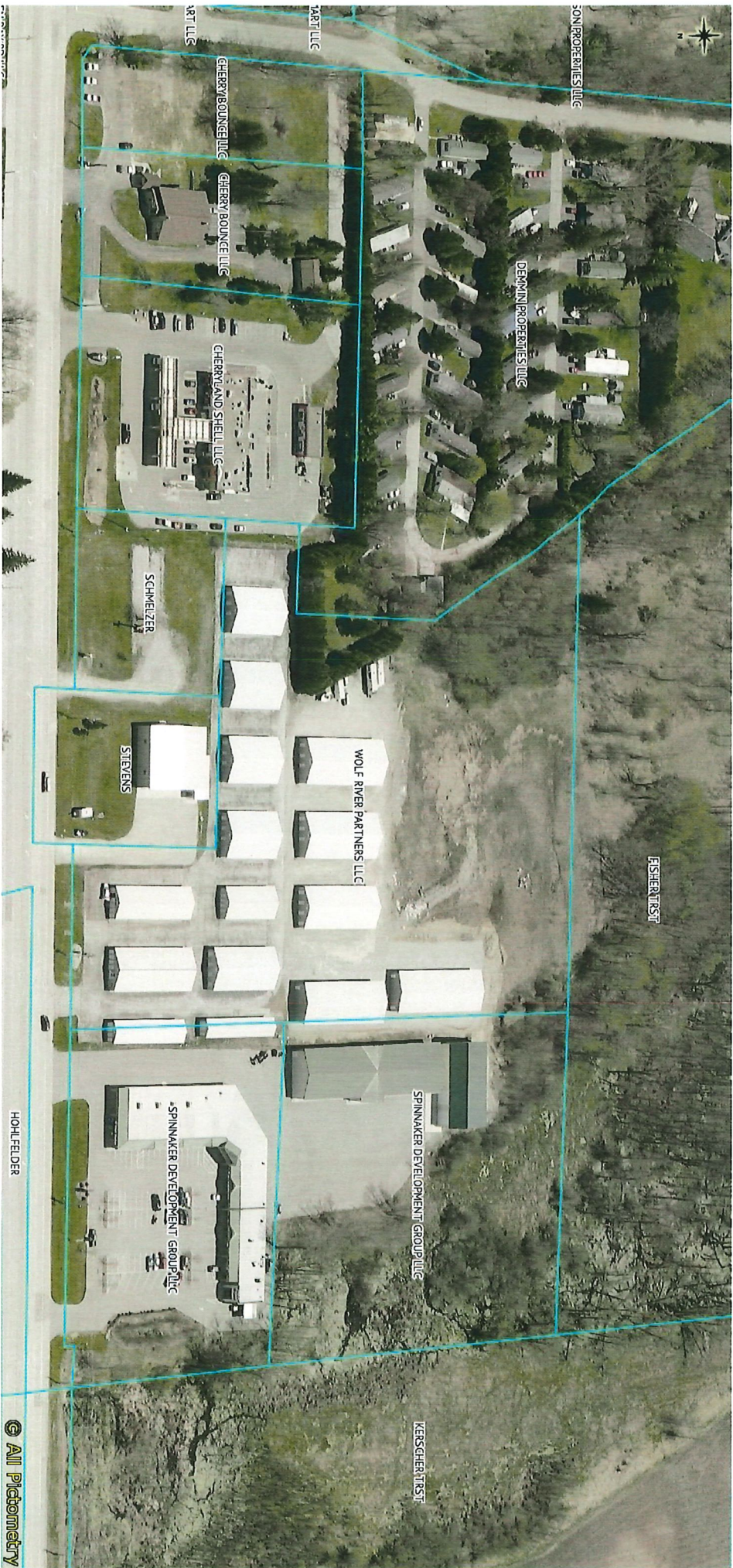


**... for all seasons!**



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05/13/2019

**CITY OF STURGEON BAY**  
**AESTHETIC DESIGN & SITE PLAN REVIEW BOARD**  
**APPLICATION FOR *CERTIFICATE OF APPROPRIATENESS***

**Name:** Apple Storage

**Owner of Premises:** Wolf River Partners, LLC

**Address or Legal Description of Premises:**

1427 Green Bay Road

**Statement of Specific Item Requested for Approval:**

Buildings 1 & 2 were approved by this committee in august of 2018. This request is for approval of the final four  
buildings on the site. Buildings 3 & 4 will be 44' x 143' and will consist of 26 units with 10' x 10' roll-up doors.

Building 5 will be similar except that it will be 50' wide. Building 5 will be 50' x 91'. All buildings will be unheated,  
slab on grade foundations with metal wall panels over a 2x6 wood frame. The maximum roof height is 15' 7".

All buildings will be constructed to the rear (south) end of the property and will be behind the existing buildings

And not visible from the highway.

9-16-20  
**Date**

Steven J. Punt (AGENT)  
**Applicant**

**Date Received:** 9-18-20  
**Staff Signature:** CN  
**Date Approved/Denied:** \_\_\_\_\_





Proposed buildings to be similar to existing buildings. Gray siding with dark gray trim, white doors and white roof.

Buildings will be served by underground electric.

Drives and parking to be initially crushed aggregate base but will be paved within 24 months.

Security lighting will be installed on the ends and sides of the buildings. Lights will be located under the eaves and will be shielded to only provide downcast light.

A stormwater management plan was approved by the Wisconsin DNR and includes surface drainage, storm sewer and a detention pond to provide treatment and reduce runoff volumes. Construction of the stormwater pond was completed earlier this summer.



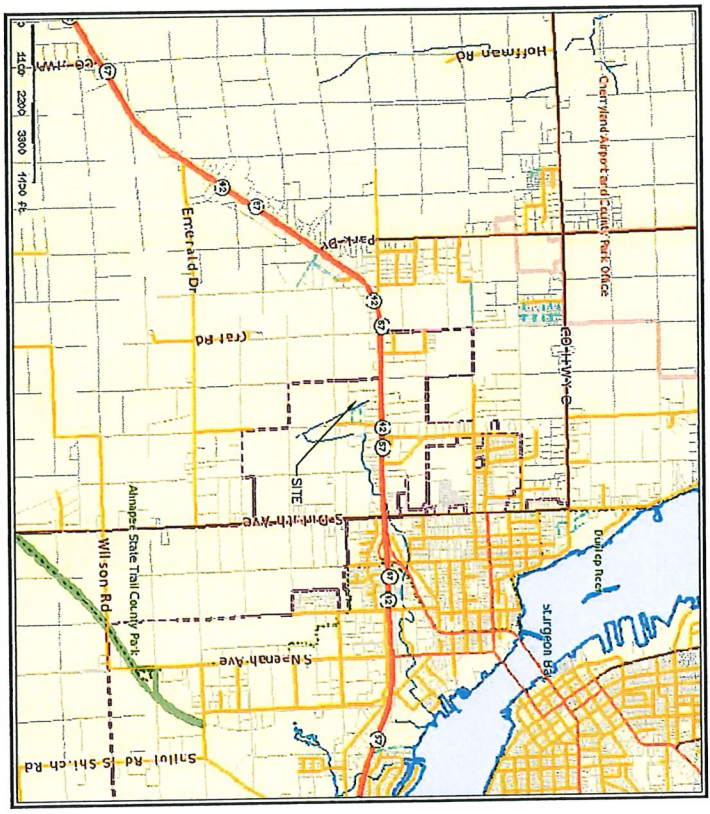
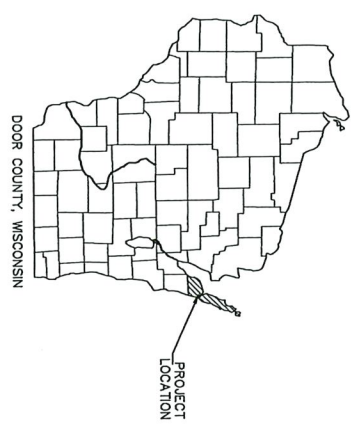


Security lighting installed on sides and ends of buildings to be shielded to provide downcast light only.



# Apple Storage GRADING, DRAINAGE, & EROSION CONTROL PLANS

JOB NUMBER  
**22651**



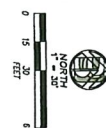
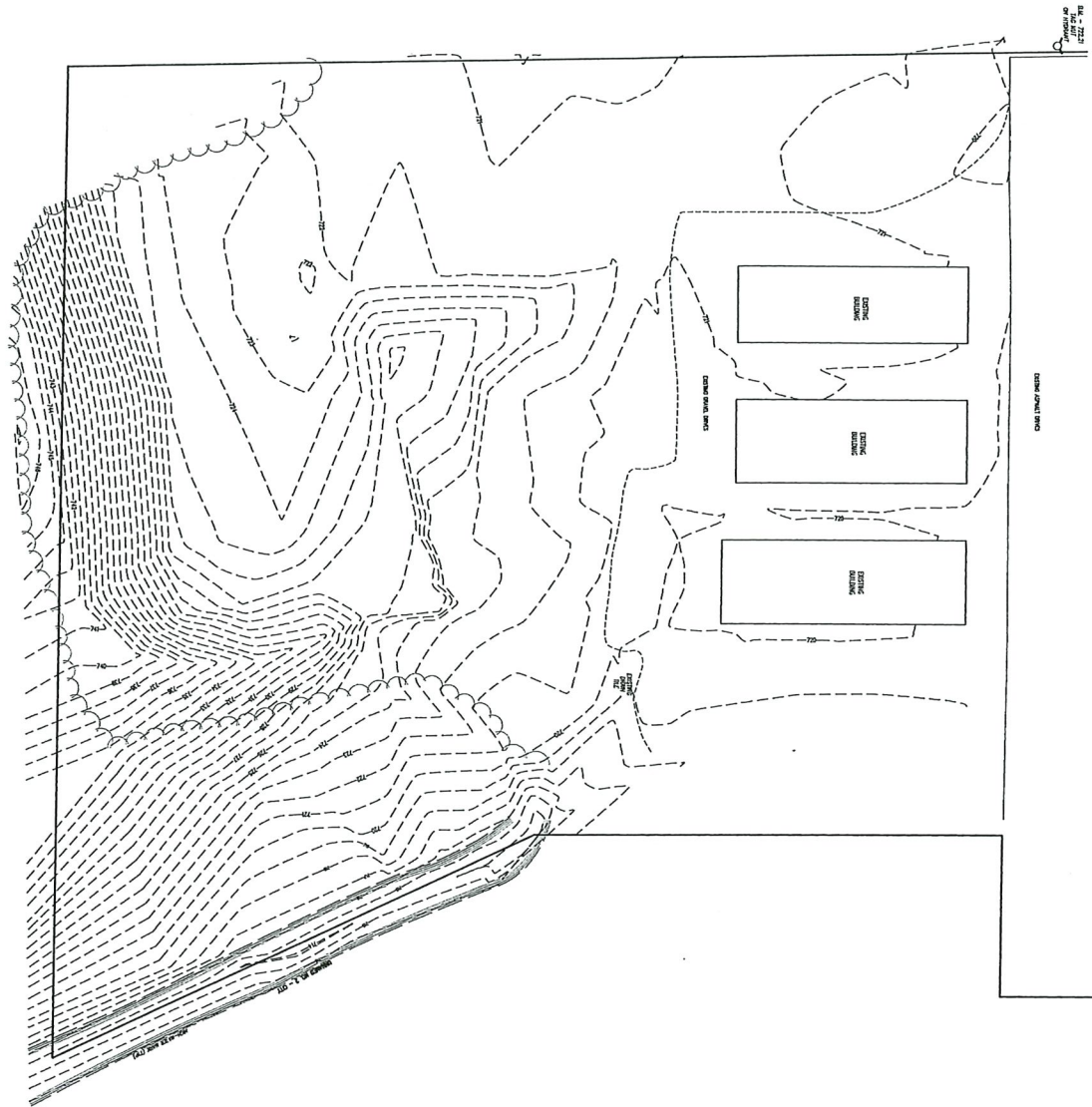
LOT 2, VOLUME 12, CERTIFIED SURVEY MAPS, PAGE 222  
THE NE 1/4 OF THE NW 1/4  
SECTION 13, TOWNSHIP 25 NORTH  
RANGE 25 EAST  
CITY OF STURGEON BAY, DOOR COUNTY, WISCONSIN

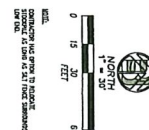
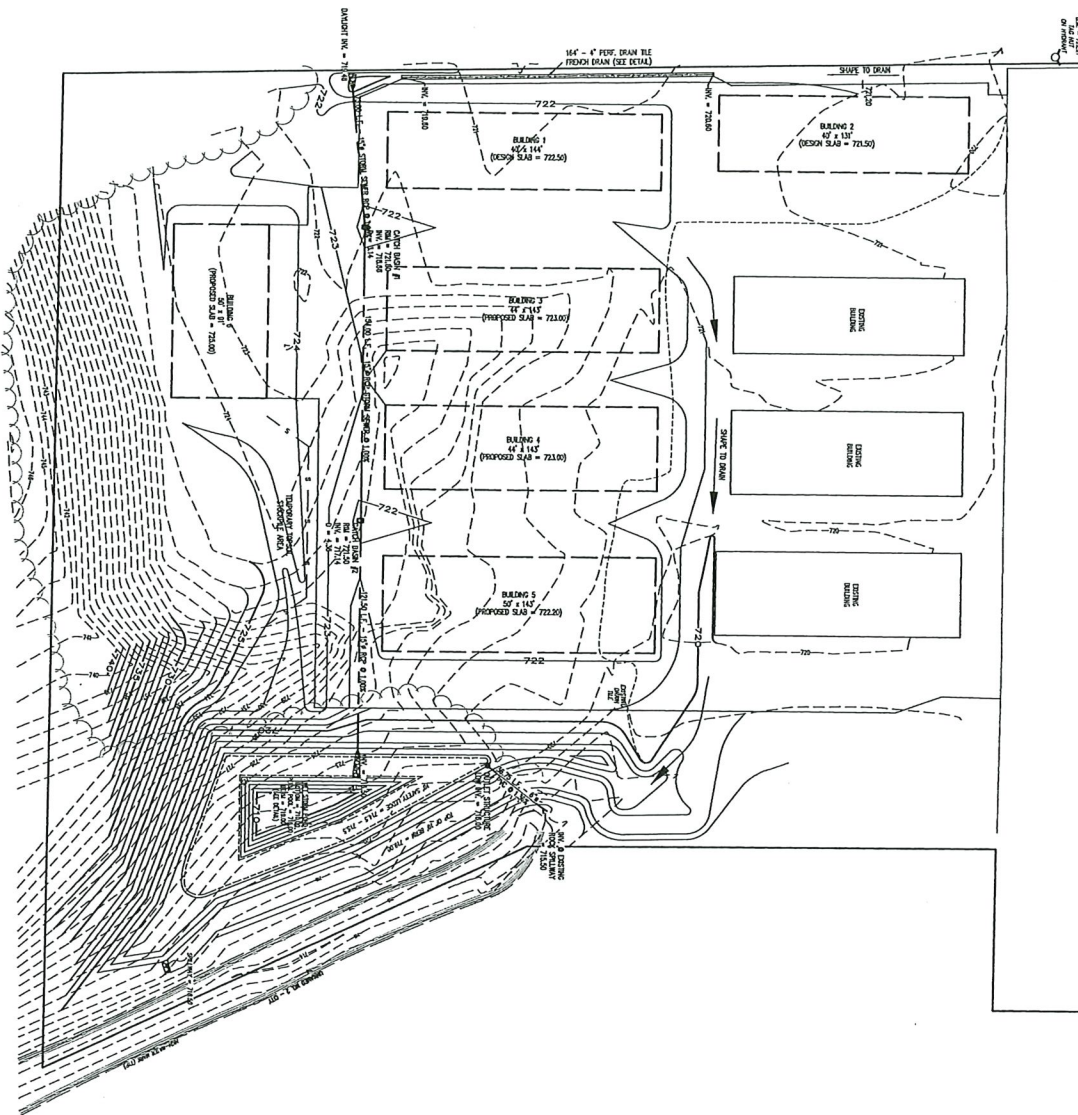
281-8613000-0004  
1427 GREEN BAY RD  
STURGEON BAY, WI 54235  
WOLF RIVER PARTNERS, LLC  
1574 BERNARDINE DRIVE  
STURGEON BAY, WI 54235

**INDEX OF SHEETS**

SHEET NO.	DESCRIPTION
C100	COVER & INDEX SHEET
C101	EXISTING SITE PLAN
C102	GRADING PLAN
C103	CONSTRUCTION PLAN
C200	CONSTRUCTION DETAILS
C201	STORM POND DETAILS
C202	







NOTES:  
1. ALL ELEVATIONS ARE IN FEET.  
2. THE GRADE SHOWN ON THIS PLAN IS THE PROPOSED GRADE.  
3. THE GRADE SHOWN ON THIS PLAN IS THE PROPOSED GRADE.  
4. THE GRADE SHOWN ON THIS PLAN IS THE PROPOSED GRADE.

REUSE OF DOCUMENTS  
THIS DOCUMENT HAS BEEN DEVELOPED FOR  
A SPECIFIC APPLICATION AND NOT FOR  
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GRADING  
PLAN

Apple Storage



312 N. 5TH AVE.  
P.O. BOX 105  
STURGEON BAY, WI.  
54235  
PHONE: (920)743-8211

FILE NO. 17-0001-0001	PROJECT DIRECTOR JOHN STANLEY	DRAWN BY JES
PLAN NO. 631-0001	DESIGNED BY P.J.H.	DATE 4-22-20
REVISION 05-11-20	SCALE 1" = 50'	CAD NUMBER 22651





**BASED ON A 90 DAY SCHEDULE**

**ANTICIPATED START: MAY 1, 2020**

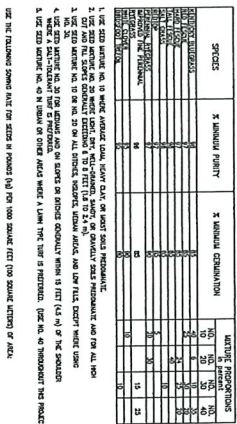
**ANTICIPATED FINISH: AUGUST 1, 2020**

- ## Stabilization Specifications
- (REFER TO EROSION CONTROL PLAN FOR APPLICABLE AREAS)

(REFER TO EROSION CONTROL PLAN FOR APPLICABLE AREAS)

[illegible]WIS DOT Standard Specifications  
Section 630

## Section 630



(IF NEEDED)

- ## Gradation Specs

Sieve Sizes	Percentage Passing
1-INCH	100
3/4-INCH	80-100
3/8-INCH	20-100
No. 4	0-10
No. 8	0-5

## 1. Elliptical and Rectangular

1. USE DATE BY SPRING AND SUMMER PLANTINGS.
2. USE WINTER WEED OR RICE FOR FALL PLANTINGS STARTED AFTER SEPTEMBER 1.
3. APPLY TURBOGRASS SEEDING AT 3 POUNDS PER 1000 SQUARE FEET OF AREA, EXCEPT ANNUAL WEEDS WHICH MAY BE APPLIED AT 2 POUNDS PER 1000 SQUARE FEET OF AREA.

## STRAW BALE DITCHES

PER WET DETENTION POND DNR TECHNICAL STANDARD 1003

3 MATERIAL SHALL BE CRUSHED STONE CHIPS MEETING THE

Sieve Sizes	Percentage Passing
1-INCH	100
3/4-INCH	80-100
3/8-INCH	20-100
No. 4	0-10
No. 8	0-5

## MA 200

<u>SIDE SIZE</u>	<u>PERCENTAGE PASSING BY WEIGHT</u>
2-1/4 INCH	95-100
No. 4	65-80

- Fiber 18m  
No. 200 35-05

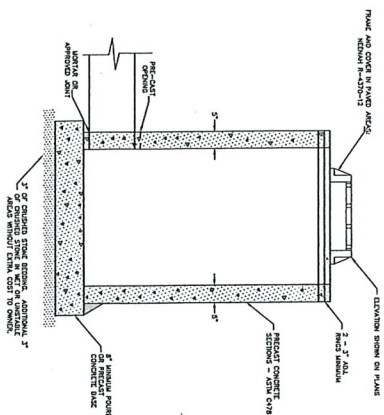
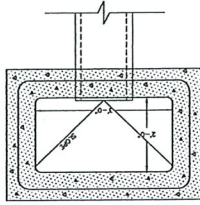
STRAW BALE DITCH  
NO SCALE

THIS DOCUMENT  
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GENERAL USE.  
USED WITHOUT  
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USE IS THE SO  
UNAI

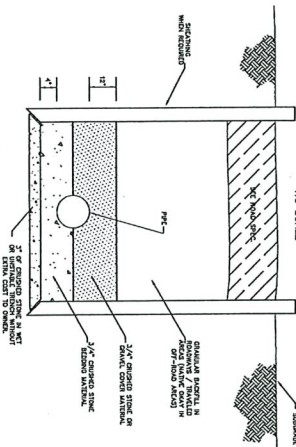
# Driveway Cross-Section



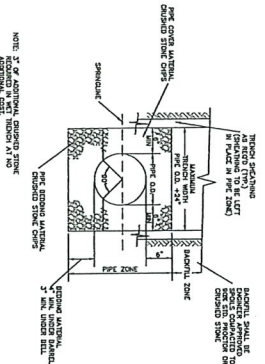
# Catch Basin



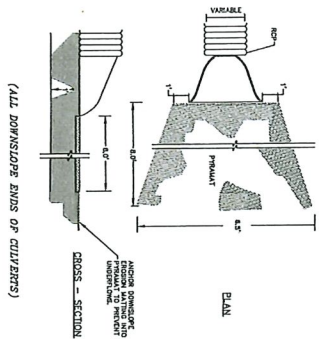
# Standard Single Trench Section



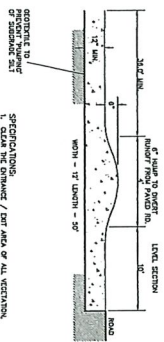
# Culvert Bedding Detail



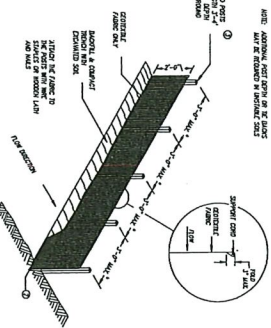
# Culvert Rip-rap Detail



# Temporary Stone Tracking Pad Detail



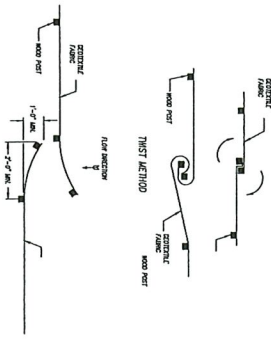
# Silt Fence Detail



# Silt Fence



# JOINING TWO LENGTHS OF SILT FENCE



# GENERAL NOTES

1. Silt fence shall be constructed in accordance with the following:
2. Silt fence shall be constructed in accordance with the following:
3. Silt fence shall be constructed in accordance with the following:
4. Silt fence shall be constructed in accordance with the following:
5. Silt fence shall be constructed in accordance with the following:
6. Silt fence shall be constructed in accordance with the following:
7. Silt fence shall be constructed in accordance with the following:
8. Silt fence shall be constructed in accordance with the following:
9. Silt fence shall be constructed in accordance with the following:
10. Silt fence shall be constructed in accordance with the following:

CONSTRUCTION DETAILS

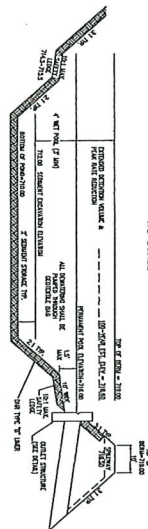
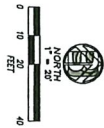
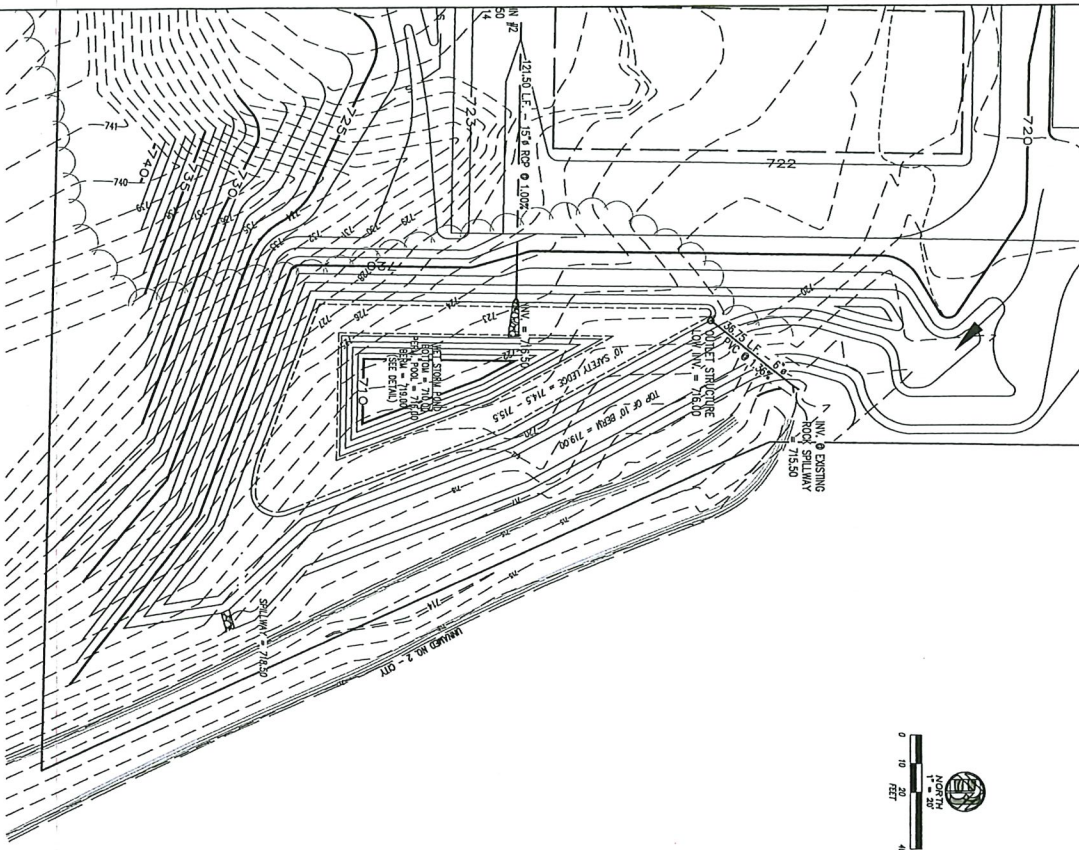
Apple Storage

BAUDHUM SURVEYING & ENGINEERING  
312 N. 5TH AVE.  
P.O. BOX 105  
STURGEON BAY, WI 54783  
PHONE: (920)743-6211

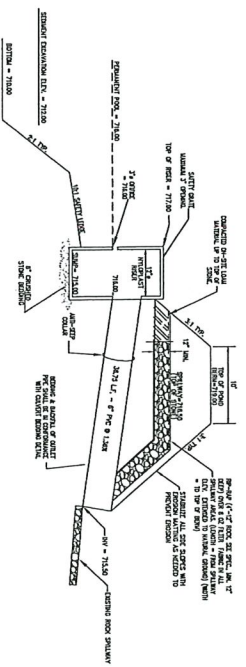
DATE 11/11/2010	PROJECT DIRECTOR JIM TAYLOR	DATE 11/11/2010
DESIGNED BY P. J. H.	DESIGNED BY P. J. H.	DESIGNED BY P. J. H.
REVIEWED BY C. J. H.	REVIEWED BY C. J. H.	REVIEWED BY C. J. H.
SCALE NONE	SCALE NONE	SCALE NONE

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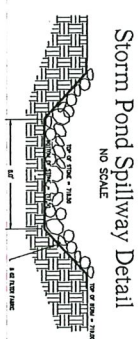




Storm Pond Detail  
NO SCALE



Pond Outlet Structure Detail  
NO SCALE



Storm Pond Spillway Detail  
NO SCALE

SITE ADDRESS:  
APPLE STORAGE  
BUILDING #1  
1427 GREEN BAY RD.  
STUGEON BAY, WI 54235

OWNER'S MAILING ADDRESS:  
RICH BIERMANN  
P.O. BOX 13154  
GREEN BAY, WI 54307

PROJECT INFORMATION	
COUNTY LOCATION:	BROWN
PROJECT TYPE:	NEW BUILDING
BUILDING AREA:	40'-0" x 144'-0" = 5,760SF
MEAN ROOF HEIGHT:	10'
NO. STORIES:	1
OCCUPANCY GROUP:	S-1, STORAGE
CONSTRUCTION TYPE:	VB
UNSPRINKLERED, UNHEATED	
OCCUPANCY LOAD:	UNOCCUPIED
REQUIRED SEPARATION:	NOT REQUIRED
ALLOWABLE HEIGHT:	1 STORY
ALLOWABLE AREA:	9,000 SF
ROOF SLOPE:	2.5:12
ROOF SURFACE:	SLIPPERY - NOT USED

SNOW DESIGN DATA	
GROUND SNOW LOAD, $P_g$	= 50.0 PSF
SNOW LOAD IMPORTANCE FACTOR, $I_s$	= 1.0
SNOW LOAD EXPOSURE FACTOR, $C_e$	= 1.0
SLOPED ROOF/FLAT ROOF FACTOR, $C_s$	= 1.0
ROOF THERMAL FACTOR, $C_t$	= 1.2
ROOF SNOW LOAD (BALANCED)	= 42.0 PSF
ROOF SNOW LOAD (UNBALANCED)	= PER SPS 362.1 608
N/A PSF WINDWARD	N/A PSF LEeward

EARTHQUAKE DESIGN DATA	
SEISMIC USE GROUP:	1
SITE CLASS:	D
SEISMIC DESIGN CATEGORY:	A

WIND DESIGN DATA	
BASIC WIND SPEED:	115.0 MPH
WIND LOAD RESISTANCE DESIGN:	ASCE 7-10
WIND LOAD EXPOSURE CATEGORY:	C
WIND LOAD IMPORTANCE FACTOR, $I_w$	= 1.0

SOIL DATA	
SOIL CLASS:	CLASS 4-FIRM
PRESUMED SOIL LOAD BEARING VALUE:	2,000 PSF

DRAWING INDEX	
A1	TITLE SHEET, CODE INFORMATION
A2	FLOOR PLAN, ELEVATIONS, AND DETAILS
A3	BUILDING SECTION AND DETAILS
A4	FOUNDATION PLAN, SITE PLAN AND DETAILS

#### GENERAL NOTES AND SPECIFICATIONS

1. SITE WORK: THE BUILDING SITE SHALL BE GRADED TO PROVIDE DRAINAGE AWAY FROM BUILDING.
2. CONCRETE STRENGTH: 3,000 PSI, IN 28 DAYS. SLUMP NOT OVER 4".
3. LUMBER: ALL LUMBER TO BE #4S #2 SPRUCE-PINE-FIR OR EQUAL UNLESS NOTED OTHERWISE ON PLANS.
4. PLUMBING: IN NOT PART OF THIS DRAWING AND SHALL BE INSTALLED IN ACCORDANCE WITH ALL STATE AND LOCAL CODES.
5. HEATING, VENTILATING, AND AIR-CONDITIONING: IS NOT PART OF THIS DRAWING AND SHALL BE INSTALLED IN ACCORDANCE WITH ALL STATE AND LOCAL CODES.
6. ELECTRICAL: IS NOT PART OF THIS DRAWING AND SHALL BE INSTALLED IN ACCORDANCE WITH ALL STATE AND LOCAL CODES.
7. DEVIATION FROM PLANS: THERE SHALL BE NO DEVIATION FROM THESE PLANS WITHOUT THE WRITTEN CONSENT OF FOREST CONSTRUCTION CO., INC.
8. FIRE EXTINGUISHERS: FIRE EXTINGUISHERS TO BE PROVIDED PER IFC906 AND MAINTAINED PER NFPA 10.
9. FOREST CONSTRUCTION CO., INC.: TO PROVIDE BUILDERS RISK INSURANCE.

DRAWN BY:	AMC	CLIENT NAME:	RICH BIERMANN
SCALE:		PROJECT NAME:	BUILDING # 1 40' X 144' X 11'6" STORAGE
ISSUE DATE:	6/21/18	PROJECT LOCATION:	STURGEON BAY, WI

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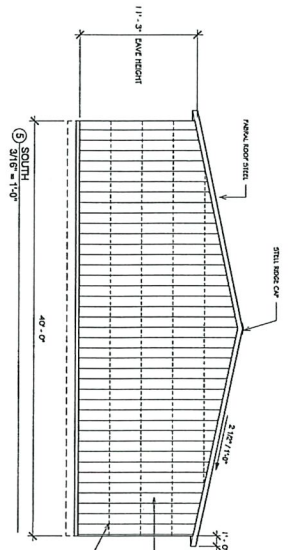
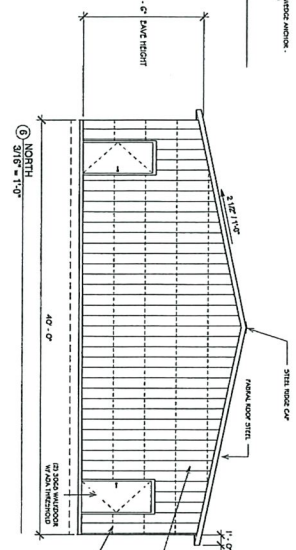
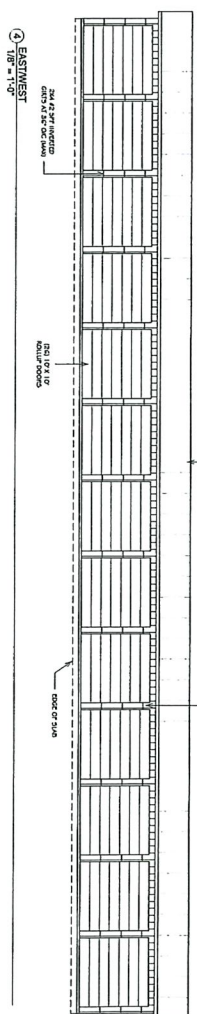
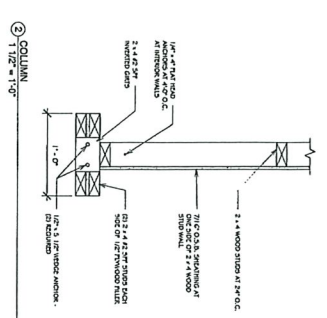
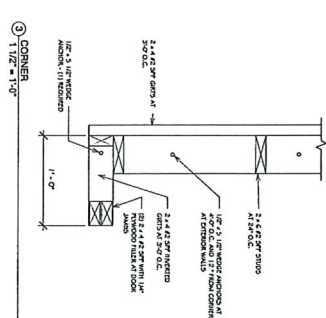
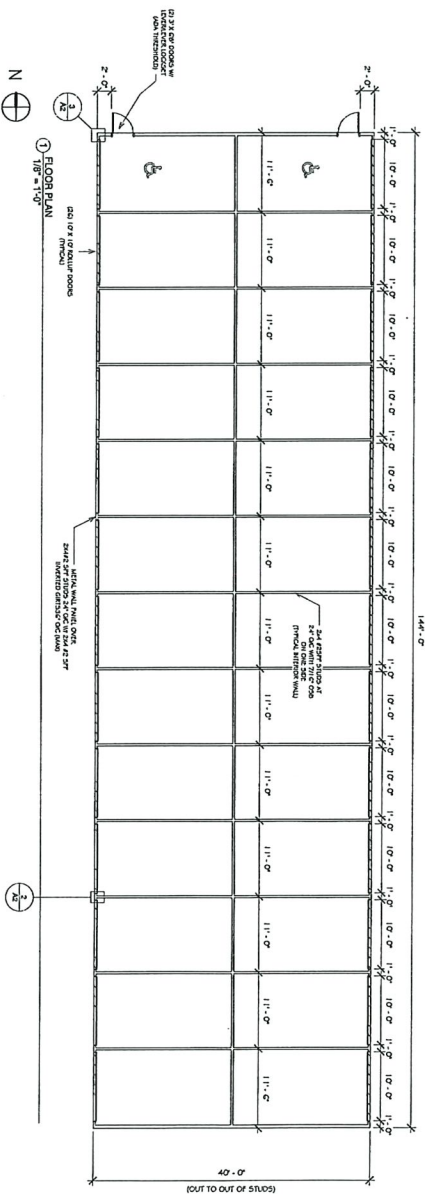
FOREST CONSTRUCTION CO., INC.  
106 PRAMING LANE  
P.O. BOX 250  
LUDWIGSBURG, WI 54173  
OFFICE: 920-446-2082  
FAX: 920-446-2082  
WEB: WWW.FORESTBUILDINGS.COM

SHEET NUMBER:  
**A1**  
LATEST REVISION:



ACCESSIBLE SELF-SERVICE STORAGE FACILITIES		
TOTAL SQUARE FOOTAGE	MINIMUM NUMBER OF ACCESSIBLE STORAGE SPACES	
1 TO 500	5% BUT NOT LESS THAN 1	
OVER 500	10% BUT NOT LESS THAN 2	

NOTE: THESE ARE MINIMUM REQUIREMENTS. THE NUMBER OF ACCESSIBLE STORAGE SPACES SHOULD BE DETERMINED BY THE LOCAL BUILDING DEPARTMENT. THE FOLLOWING ARE THE MINIMUM REQUIREMENTS FOR ACCESSIBLE STORAGE SPACES:



DRAWN BY: **AMC**  
 SCALE: **As indicated**  
 ISSUE DATE: **6/21/18**

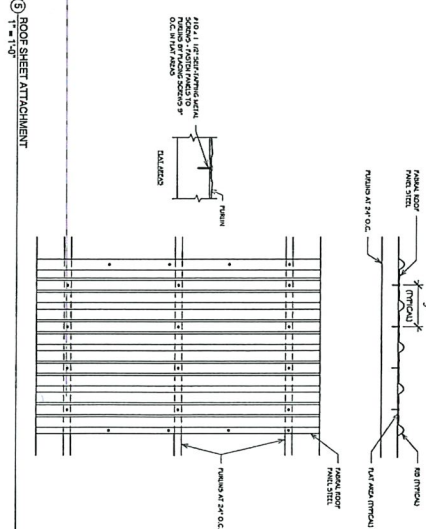
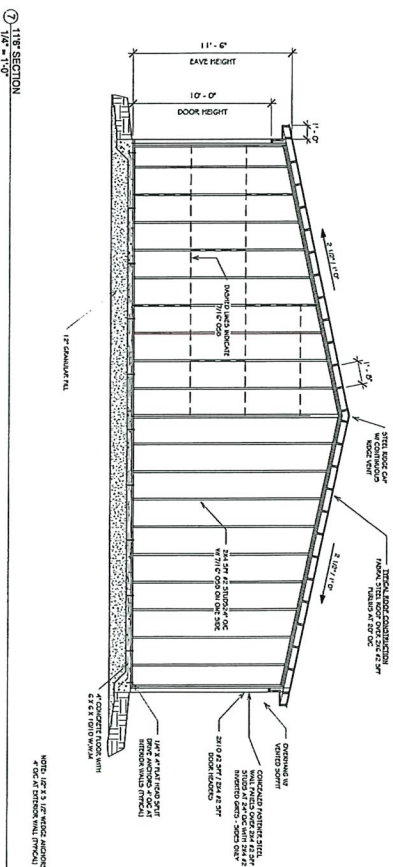
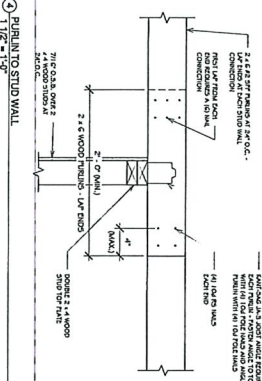
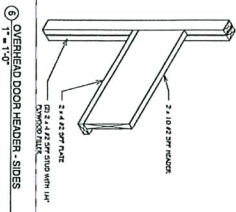
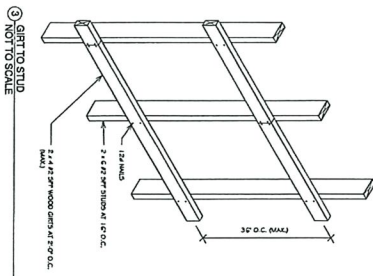
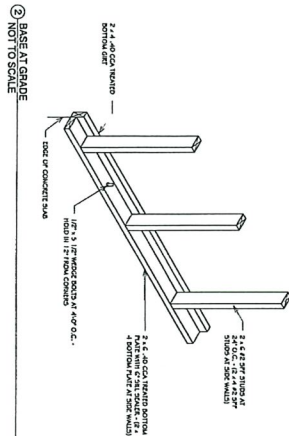
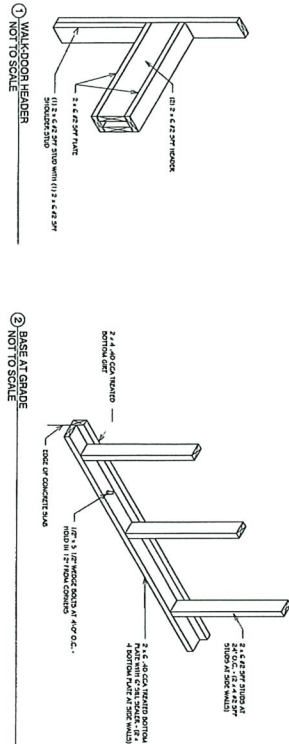
CLIENT NAME: **RICH BIERMAN**  
 PROJECT NAME: **BUILDING # 1 40' X 144' X 11'6\"/>**

PROJECT LOCATION: **STURGEON BAY, WI**

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FOREST CONSTRUCTION CO., INC.  
 166 PRAIRIE LANE  
 P.O. BOX 550  
 LUDWIG, WI 53071  
 OFFICE: 262-446-2302  
 FAX: 262-446-2302  
 WWW.FORESTBUILDINGS.COM

SHEET NUMBER:  
**A2**



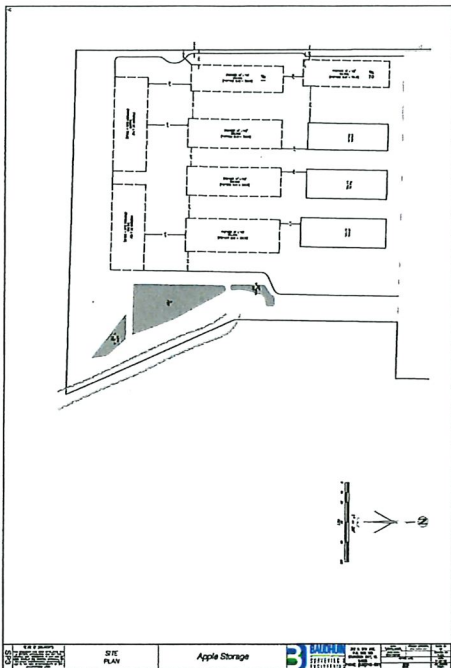
DRAWN BY:	AMC	CLIENT NAME:	RICH BIERMANN
SCALE:	As indicated	PROJECT NAME:	BUILDING # 1 40' X 144' X 11'6\"/>
ISSUE DATE:	6/21/18	PROJECT LOCATION:	STURGEON BAY, WI

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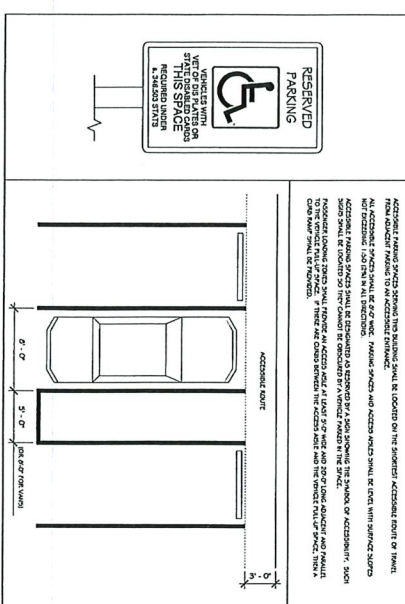


FOREST CONSTRUCTION CO., INC.  
144 BURNING LANE  
P.O. BOX 150  
LUZERN, WI 54501  
TEL: 715-255-4444  
FAX: 715-255-4444  
WWW.FORESTCONSTRUCTION.COM

SHEET NUMBER:  
**A3**  
LATEST REVISION:



## PARKING AND PASSENGER LOADING ZONES



DRAWN BY:	CL. ENT. NAME:
AMC	RICH BIERMAN
SCALE:	PROJECT NAME:
As indicated	BUILDING # 1 40' X 144' X 11'6" STORAGE
ISSUE DATE:	PROJECT LOCATION:
6/21/18	STURGEON BAY, WI

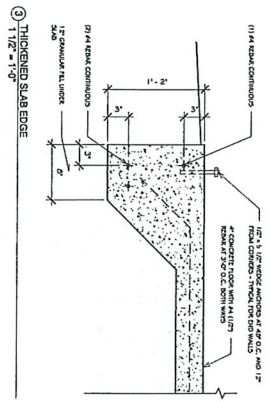
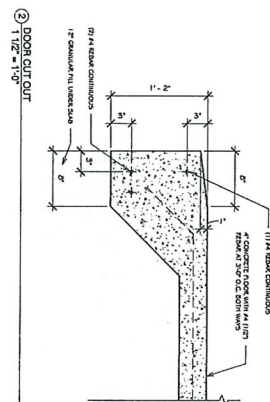
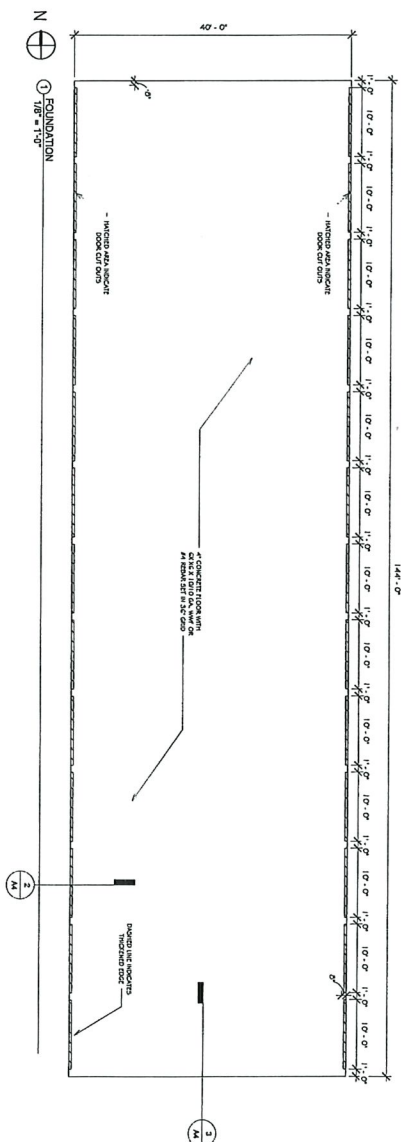
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**FOREST CONSTRUCTION**

FOREST CONSTRUCTION CO., INC.  
100 PARKING LANE  
P.O. BOX 100  
LUDWIGSBURG, WI 5417  
TOLL FREE: 800-242-4885  
TEL: 920-446-5002  
FAX: 920-446-5002  
EMAIL: INFO@FORESTBUILDINGS.COM  
WEB: WWW.FORESTBUILDINGS.COM

SHEET NUMBER:  
**A4**







# MEMO

To: Aesthetic Design and Site Plan Review Board  
From: Christopher Sullivan-Robinson  
Date: October 13, 2020  
Subject: Addition to 422 Building for Fincantieri Bay Shipbuilding

---

Fincantieri is requesting approval of a 14'W x 155'L x 13'H building addition to the 422 building. The addition is being placed as shown due to the existing easements that extend around the remainder of the building. Per the drawings, the design appears to match materials of the existing building. There is also 2 windows being installed on the west elevation and an overhead door on the north elevation of the addition. There is a 12 x 12 overhead door being added to the west side of the building and a 40 x 49 megadoor being added to the south side of the building. It is important to note that there is a 3-foot elevation drop between this property and the properties to the east. So, a portion of the building will not be as visible to the public.

Below are the City's Aesthetic Guidelines and Review Criteria:

(1) Site layout.

- (a) The existing natural topographic and landscape features of a site should be incorporated into a development plan. Such plan shall include all prudent and necessary steps required to protect the natural environment of the site and surrounding areas during and after construction. *There is no natural landscape to protect, however there is a 3 foot elevation difference between this property and the property to the east. Currently there is a retaining wall which will need to be modified to accommodate the addition.*
- (b) The site shall be designed to accomplish a desirable transition from the public streetscape, and between commercial, industrial, and residential land use areas. *This property and the abutting properties on 3<sup>rd</sup> Avenue belong to Fincantieri, so the present impact is minimal. However, the committee could require some kind of buffer between the subject building and adjacent properties. Keep in mind, Fincantieri is required to provide a public improvement plan within the next 5.5 months. There are also existing evergreen trees along the eastern property edge.*
- (c) Where grading is necessary for the construction of structures and paved areas, the grading should blend into adjacent property. Abrupt changes in grade are discouraged. *There is no impact to public view.*
- (d) Where possible, buildings should be situated on the site to promote and protect public views to and along shorelines from public roads and other public lands. *This is met.*

- (e) In areas where sidewalks exist or are planned, buildings containing retail and other commercial uses should have entrances that provide convenient pedestrian access from the street. [N/A](#)
  - (f) Where a pattern of small-scale commercial development exists or is planned, narrower buildings or multiple storefronts within larger buildings are encouraged. [N/A](#)
  - (g) Where a pattern of relatively consistent building setbacks exists on a street, new buildings should be situated to closely match such setback pattern. [N/A](#)
  - (g) On corner parcels where the proposed building is located close to both street lines, the corner of the building shall be recessed from the street lines in order to create pedestrian areas; prominent building entrances; and more architecturally interesting buildings. [N/A](#)
  - (h) Stormwater drainage shall be designed so as not to alter the natural drainage systems or cause flooding or erosion on neighboring properties. [The overall expansion of the south yard will be reviewed by the DNR for stormwater management requirements.](#)
- (2) Access, circulation and parking. [N/A – No changes proposed](#)
- (a) Vehicular driveways into the site shall be located in a manner to minimize traffic congestion and difficult turning movements and shall be coordinated with existing and proposed access points on adjoining or nearby properties. Individual developments having more than one access points per street are discouraged and shared access driveways with adjoining properties is encouraged. [N/A](#)
  - (b) The interior circulation of the site shall be designed to provide for the convenient and safe flow of pedestrians and non-pedestrian traffic through the site and to/from public streets or sidewalks. [N/A](#)
  - (c) Off-street parking areas for motor vehicles and bicycles that are shared with neighboring businesses and parcels through the use of common driveways or internal access driveways shall be encouraged, if feasible, instead of stand-alone single use parking areas. [N/A](#)
  - (d) Off-street parking located to the rear or side of buildings is preferred over parking between the building and the street, particularly if the amount of off-street parking supplied is greater than required under the zoning code. For stand-alone buildings under 15,000 square feet, it is preferable that not more than one tier (single row or double row) of parking be located between the building and the street it fronts. [N/A](#)
  - (e) For large developments over 40,000 square feet, the site design shall allow for present or future alternative transportation modes, such as bike routes or transit stops. [N/A](#)

- (f) Permeable surfaces, bioswales, rain gardens, and other forms of stormwater runoff prevention for parking and on-site traffic areas are encouraged. [N/A](#)



(3) Building design.

- (a) Buildings shall not be limited to a preferred type of architecture or building materials. However, architectural styles, which are generally not common to Sturgeon Bay or Northeastern Wisconsin, are discouraged. [This addition uses the same materials as the existing structure.](#)
- (b) Buildings should be sited and designed to be aesthetically pleasing as viewed from adjoining public streets. Excessively long, unbroken building facades shall be avoided. Building materials and design features shall be consistent with the general design theme and/or proposed use of the development. [This is in the public's view, so the committee could require some additional features on the view from 3<sup>rd</sup> Avenue.](#)
- (c) The architectural character of historic structures shall be maintained or enhanced whenever remodeling, expansions, or other changes are proposed. [N/A](#)
- (d) Buildings on in-fill sites shall be compatible with surrounding buildings in terms of scale, massing, height, entrances, and windows. [The building as well as the addition matches other newer constructions within the Fincantieri's property.](#)
- (e) Rhythm/re-occurring patterns in windows and storefronts are encouraged. [It could be required to match the features of the existing building.](#)
- (f) Building components, such as windows, doors, eaves, and parapet, should be in proportionate scale in relationship to one another. [N/A](#)
- (g) The use of door and window canopies and awnings is encouraged. [N/A](#)
- (h) The use of special architectural features, including projecting windows, towers, turrets, arches and cupolas are encouraged, particularly on corner buildings. [This addition could be considered an architectural element which removes the "box-like" look of the building.](#)
- (i) Rooftop mechanical equipment shall be screened with parapets or the roof form. [N/A](#)
- (j) Designs seeking Leadership in Energy and Environmental Design (LEED) certification are encouraged. [The committee can encourage environmentally sustainable construction practices.](#)
- (k) Buildings containing retail commercial uses that are located within 15 feet of the street should have transparent windows on the ground floor along at least 50% of the length of the building facade facing the street. [N/A](#)
- (l) Garages should be designed to not dominate the site. Whenever possible,

detached garages shall be located to the rear of the principal building or shall be screened or have overhead doors not facing the street. Attached garages that extend beyond the front façade of the building are discouraged. Garages with three or more parking stalls shall be designed to limit the visual impact of overhead doors through features such as offsets or side-loading. [N/A](#)

- (m) Blank walls viewable from the street are undesirable. [The west elevation of the addition shows two windows, otherwise it is fairly blank. However, the property sits lower than other properties along 3<sup>rd</sup> Avenue.](#)

(4) Materials and colors.

- (a) Opaque or reflective window tints and glazes are discouraged. [N/A](#)
- (b) The use of identical building materials on all sides of a building that are visible from public streets is encouraged. [This is met.](#)
- (c) For developments with multiple buildings, a palette of options for exterior building colors for use throughout the site should be established. The range should be wide enough to allow for variety, yet narrow enough to unify all the buildings on a site. [The color scheme and materials match the existing building.](#)
- (d) Metal siding is strongly discouraged except for industrial buildings or for facades not facing public areas such as streets or parking areas. [This building is industrial.](#)

(5) Landscaping.

- (a) The appearance of paved areas should be enhanced through landscaping. Large parking areas shall comply with the interior landscaping requirements of the Sturgeon Bay Zoning Code. Required landscape islands shall be dispersed throughout the parking area to avoid large expanses of pavement. [N/A](#)
- (b) Storage areas, dumpsters and other places that tend to be unsightly shall be screened by walls, fences, berms, vegetation, or combinations of these. The screening should be equally effective in winter and summer. [N/A](#)
- (c) To reduce erosion and surface runoff, trees and other vegetative land cover shall be removed only where necessary for the construction of structures or paved areas. [N/A](#)
- (d) Vegetative landscaping shall be used to soften the appearance of blank walls. [This could be a requirement if desired. As previously noted, there is an existing line of evergreens which extend along the eastern property line this could be required to be maintained on the property.](#)



- (e) Fences and retaining walls shall be surfaced, painted, landscaped or otherwise treated to blend with their surroundings. There is an existing fence on the north side of the building which is not indicated on the plan. It will likely need to be modified to accommodate the addition. There is also an existing retaining wall to the east, which will need to be modified to accommodate the addition.
  - (f) Utility lines should be placed underground where appropriate and economically feasible to reduce the visual impact on open and scenic areas. Utility pedestals should be screened. N/A
  - (g) The use of native and indigenous plant species is encouraged over exotic species. The use of invasive species, as defined by the Wisconsin Department of Natural Resources, is prohibited. This could be a requirement if vegetation is also required by committee.
- (6) Lighting. No lighting is shown on plan, but all added lighting should be required to be downward directed, LED, and contained within the property.
- (a) Exterior light fixtures shall be shielded such that direct beams of light are not cast skyward or onto adjoining property. Exterior light fixtures for signage, building entrances, accents, parking lots, and landscaped areas are strongly encouraged to be downward directed. Auto-dimmers and timers are encouraged for all exterior lights to limit energy waste during non-operational hours.
  - (b) Lighting of an intensity beyond which is reasonably required to conduct operations or maintain security is discouraged.
  - (c) Lighting fixtures and devices promoting energy efficiency are encouraged.
- (7) Signs. N/A
- (a) The location, size, design, materials, and colors of signs should be compatible with the building style.
  - (b) The location and design of freestanding signs shall be coordinated with the landscape treatment.
  - (c) Neon signs, portable arrow signs, and light box signs are discouraged. Interior lit cabinet signs, when approved, should be designed with opaque materials such that only the message/logo is lit rather than the entire cabinet.
  - (d) Signs that are raised on a pole or pylon should have landscaping such as shrubs or flower beds surrounding the base of the sign.
  - (e) Lighted signage is strongly encouraged to be halo-lit such that there is no

visible light source. Auto-dimmers and timers are strongly encouraged to limit energy waste during non-operational hours.

CITY OF STURGEON BAY  
AESTHETIC DESIGN & SITE PLAN REVIEW BOARD  
APPLICATION FOR ***CERTIFICATE OF APPROPRIATENESS***

Name: FINCANTIERI BAY SHIPBUILDING

Owner of Premises: FINCANTIERI BAY SHIPBUILDING

Address or Legal Description of Premises:


605 N 3RD AVENUE, STURGEON BAY, WI 54235

**Statement of Specific Item Requested for Approval:**

Fincantieri Bay Shipbuilding is requesting Aesthetic Design and Site Plan approval necessary for  
the construction of an addition to Building B422 (Formerly 324 N 1st Avenue) along with  
related site work. Fincantieri Marine Group was recently awarded a Navy Contract and there  
is a need to add production capabilities at the Sturgeon Bay Facility. Fincantieri plans to  
incorporate the current B420 Building into a module blasting and painting center to support  
their Navy contract production.

09/18/2020

Date

  
Applicant

Date Received: 9.18.20

Staff Signature: 

Date Approved/Denied: \_\_\_\_\_

**TO:** Christopher Sullivan-Robinson  
City of Sturgeon Bay

**FROM:** Peter Glassen

**DATE:** 9/18/2020

**SUBJECT:** Site Plan Review Application Narrative

Project

Fincantieri Bay Shipbuilding (FBS)  
Addition to B422 (Former 324 N 1<sup>st</sup>)  
605 N. Third Ave., Sturgeon Bay, WI  
Zoning: I-2 Heavy Industrial

The intent of the narrative below is to provide further detail and information regarding the noted items that may not be clear or entirely included within the submitted drawings.

Roadways and Parking

No parking will be added as part of these projects. No impacts to the adjacent interior access roadway is anticipated other than driveway adjustments. Interior vehicle access will be slightly modified for equipment movements.

Storm Water Management Approach

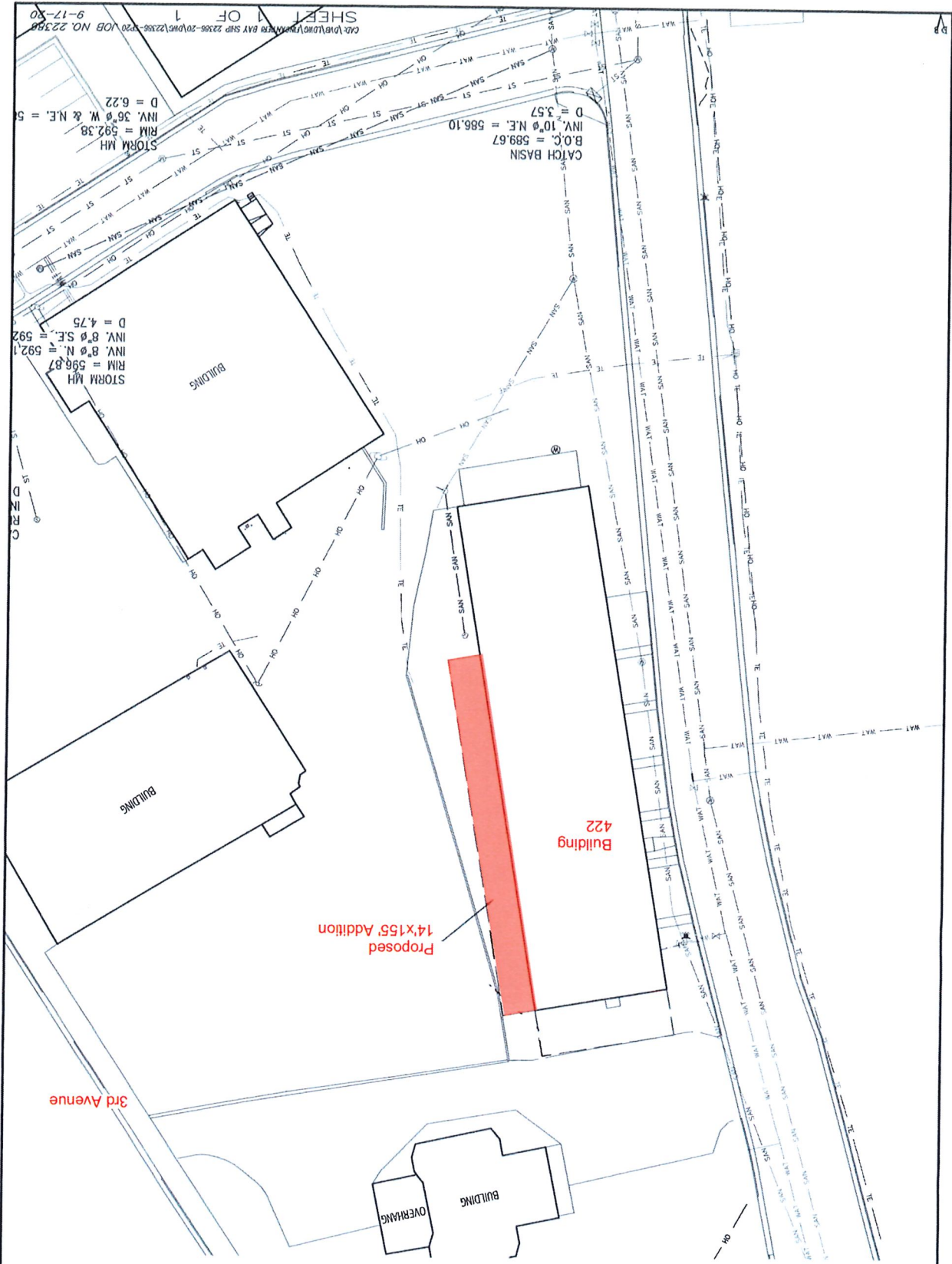
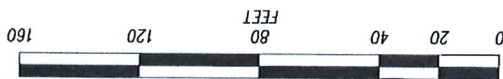
Fincantieri Bay Shipbuilding is planning to construct an addition to an existing structure on the facility in Sturgeon Bay, WI. The site is located along within existing parcels owned by FBS, in the City of Sturgeon Bay, Door County. Construction work associated with this project will include: The addition to Building 422 and associated utility adjustment and access to the building. The storm sewer system will direct drainage to the existing storm sewer. Minor utility relocations will be done within the parcel.

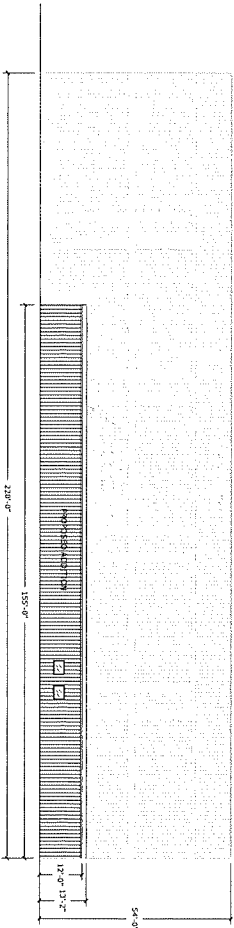
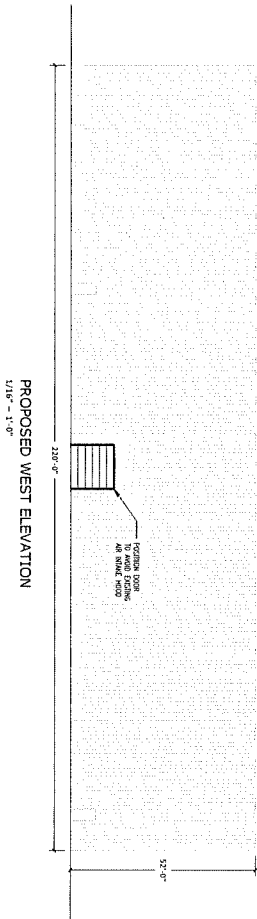
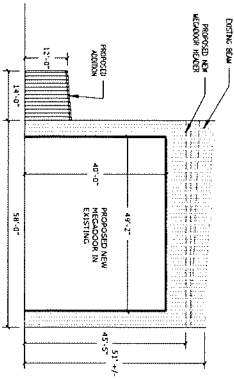
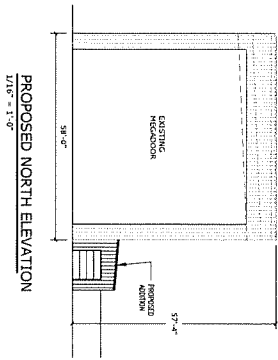
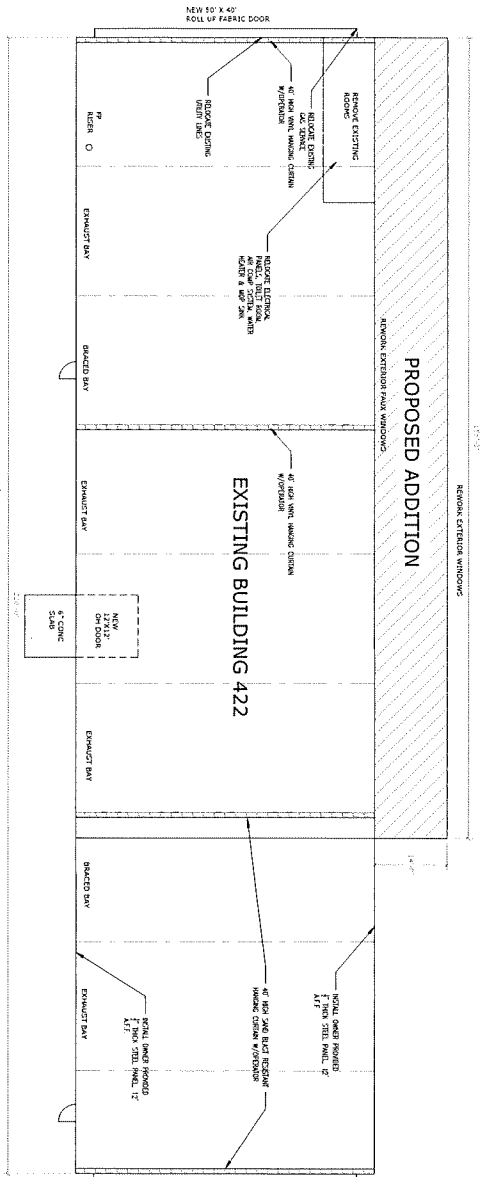
Drainage for the site is under the jurisdiction of the City of Sturgeon Bay (municipal code and WDNR (Chapter NR151). Notice of Intent for stormwater discharge permit will be submitted to WDNR and the Certificate of Coverage provide to the City Engineering Department upon receipt. This site is classified as redevelopment by definition of the regulating agencies. Post-construction stormwater management requirements are as follows:

- The site is exempt from runoff peak discharge reduction being a redevelopment.
- Best management practices (BMPs) are designed to control total suspended solids (TSS) carried by runoff from the post-construction site by 40-percent for

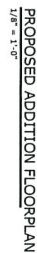


Building 422 Addition @ FBS  
 Project 923/20  
 A.C.E. Building Service  
 Sept 18, 2020







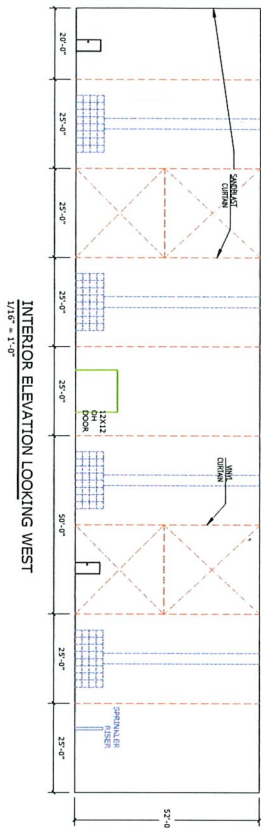


- WALL TYPE KEY**
- |   |   |
|---|---|
| 1 | 1 1/2" CMU W/1" PLYWOOD ON STEEL STUDS, FULL HEIGHT |
| 2 | 3" DW ON 6" STEEL STUDS, BOTH SIDES, 10' A.F.F.     |
| 3 | 3" DW ON 3" STEEL STUDS, ONE SIDE, 10' A.F.F.       |
| 4 | 1" PLYWOOD FULL HEIGHT                              |
| 5 | 1" HR FIRE BARRIER, 6" STEEL STUDS WITH 2 LAYERS OF |
| 6 | 1" HR FIRE BARRIER W/1" TYPE X DW BOTH SIDES        |
| 7 | 1" HR FIRE BARRIER (2) LAYERS OF TYPE X DW AND (2)  |
|   | LAYERS OF 1" SHAFTLINER                             |

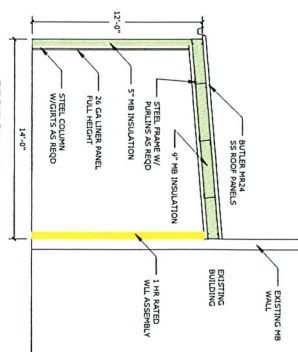
- DOOR/WINDOW KEY**
- |   |                   |
|---|-------------------|
| 1 | 3070 DOOR         |
| 2 | 8080 DOOR         |
| 3 | 6040 FIXED WINDOW |

[illegible]

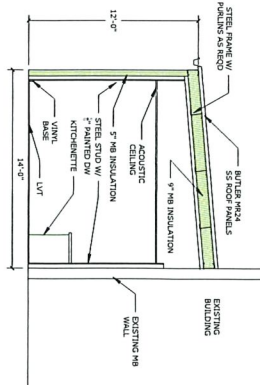
- |            |   |           |   |           |   |
|------------|---|-----------|---|-----------|---|
| <b>1T</b>  | 1,1,1-trichloroethane (TCE)   | <b>4</b>  | poly(2-vinylpyridine) (PVP)                         | <b>15</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>2T</b>  | 1,1,2-trichloroethane (1,1,2TCE)  | <b>5</b>  | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>16</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>3T</b>  | 1,1,2,2-tetrachloroethane (1,1,2,2TCE)  | <b>6</b>  | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>17</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>4T</b>  | 1,1,2,2,3-pentachloroethane (1,1,2,2,3TCE)  | <b>7</b>  | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>18</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>5T</b>  | 1,1,2,2,3,3-hexachloroethane (1,1,2,2,3,3TCE)   | <b>8</b>  | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>19</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>6T</b>  | 1,1,2,2,3,3,4-heptachloroethane (1,1,2,2,3,3,4TCE)  | <b>9</b>  | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>20</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>7T</b>  | 1,1,2,2,3,3,4,4-octachloroethane (1,1,2,2,3,3,4,4TCE)   | <b>10</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>21</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>8T</b>  | 1,1,2,2,3,3,4,4,4-nonachloroethane (1,1,2,2,3,3,4,4,4TCE)   | <b>11</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>22</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>9T</b>  | 1,1,2,2,3,3,4,4,4,4-decachloroethane (1,1,2,2,3,3,4,4,4,4TCE)   | <b>12</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>23</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>10T</b> | 1,1,2,2,3,3,4,4,4,4,4-undecachloroethane (1,1,2,2,3,3,4,4,4,4,4TCE)   | <b>13</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>24</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>11T</b> | 1,1,2,2,3,3,4,4,4,4,4,4-dodecachloroethane (1,1,2,2,3,3,4,4,4,4,4,4TCE)   | <b>14</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>25</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>12T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4-tridecachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4TCE)  | <b>15</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>26</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>13T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4-tetradecachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4TCE)  | <b>16</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>27</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>14T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4-pentadecachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,4TCE)  | <b>17</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>28</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>15T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4-hexadecachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4TCE)                                       | <b>18</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>29</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>16T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4-heptadecachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4TCE)                                  | <b>19</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>30</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>17T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4-octadecachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4TCE)                               | <b>20</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>31</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>18T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4-nonadecachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4TCE)                           | <b>21</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>32</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>19T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4-eicadecachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4TCE)                       | <b>22</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>33</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>20T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4-triacontachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4TCE)                | <b>23</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>34</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>21T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4-hexacontachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4TCE)          | <b>24</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>35</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>22T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4-octacontachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4TCE)      | <b>25</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>36</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>23T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4-tetracontachloroethane (1,1,2,2,3,3,4TCE) | <b>26</b> | poly(2-vinylpyridine-co-vinylidene fluoride) (PVDF) | <b>37</b> | 1,4-bis(4-vinylphenyl)-2,5-bis(4-vinylphenyl)benzene (PVPP) |
| <b>24T</b> | 1,1,2,2,3,3,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4,4-pentaccontachloroethane (1,1,2,2,3,3,4,4,4,4,4,4,4,4,                         |           |   |           |   |



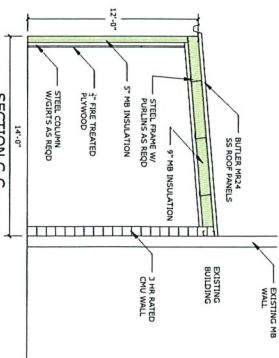
**SECTION A-A**  
**1/4" = 1'-0"**



SECTION B-B  
1/4" = 1'-0"



## SECTION C-C



FINCANTIERI  
605 N. 3RD AVE.  
STURGEON BAY, WI 54225



DESIGN UPDATE	9/2/20	SAJ
DESIGN UPDATE	9/9/20	SAJ
DESIGN UPDATE	9/14/20	SAJ
DESIGN UPDATE	9/16/20	SAJ

# A1.0

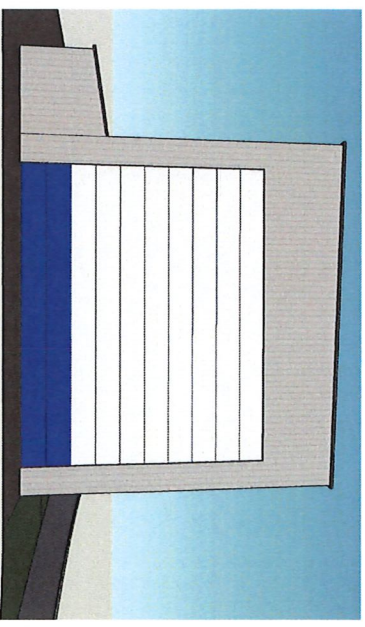
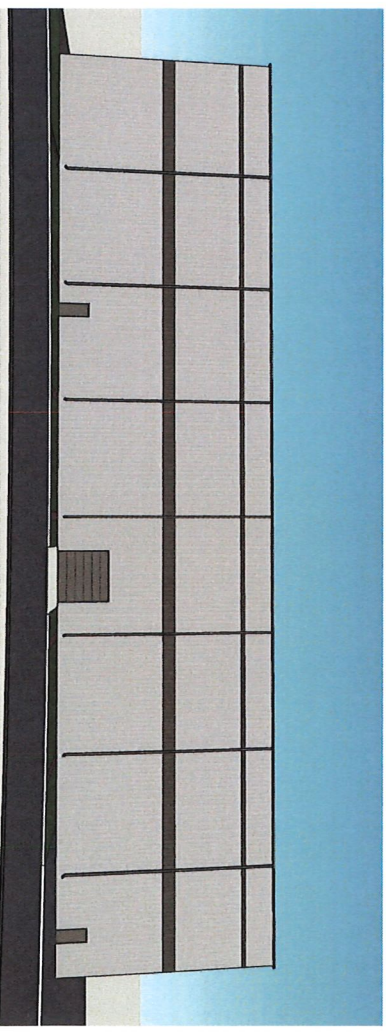
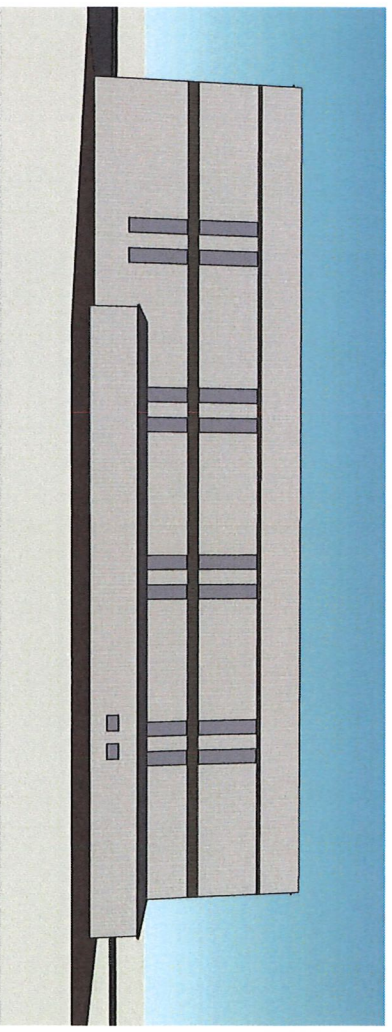
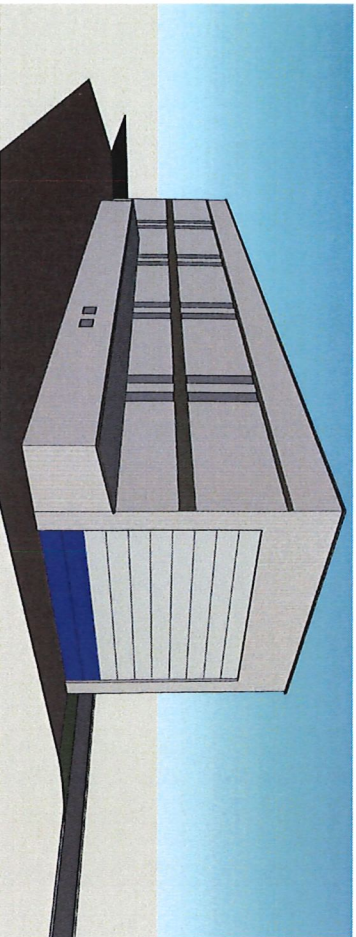
**FINCANTIERI**  
BAY SHIPBUILDING

BLDG 422 EXPANSION

FINCANTIERI BAY SHIPBUILDING

STURGEON BAY, WI

**A.C.E.** BUILDING SERVICE







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