

**Project #21-015
Center Block Public Plaza
Located at approx. 41-67 North Main Street**

REPORT SUMMARY...

<i>Project Name:</i>	Center Block Public Plaza
<i>Proponent / Owner:</i>	Mike Desimone / Logan City
<i>Project Address:</i>	41-67 North Main Street
<i>Request:</i>	Design Review Permit
<i>Current Zoning:</i>	Town Center One (TC-1)
<i>Type of Action:</i>	Quasi-Judicial
<i>Hearing Date</i>	April 8, 2021
<i>Submitted By:</i>	Russ Holley, Senior Planner

RECOMMENDATION

Staff recommends that the Planning Commission **conditionally approve** a Design Review Permit for Project PC#21-015 Center Block Public Plaza, located at 41-67 North Main Street, TIN# 06-025-0009; -0011, 06-028-0015; -0016.

Land use adjoining the subject property

<i>North:</i>	TC-1 HD: Commercial Uses	<i>East:</i>	NR-6 HD Religious Uses
<i>South:</i>	TC-1 HD: Commercial Uses	<i>West:</i>	TC-1 HD: Commercial Uses

Project Proposal

The proposal includes the demolition of buildings 47, 55 and 67 North Main, the renovation of buildings 41 and 45 North Main and the construction of a new 17,000 SF Public Plaza with a performance stage, portable ice rink (winter) and a water feature/splash pad (summer). New restrooms, fire pits, shade structures, raised flower planters, landscaping, brick gateway features, metal archway elements and new lighting are all also proposed. The remodeled buildings (41 & 45 N. Main) are intended to contain restaurant/retail uses on the main floor and office uses on the upper floors. New ground floor restaurants/retail would include unique plaza access and outdoor dining opportunities. With the exception of a proposed small drop-off/rear west entrance area proposed on two parking stalls, the entire project area remains within the existing footprint of the five buildings and leaves the interior surface parking lot untouched.



Figure 1 Shows the proposed east entrance to the Center Block Public Plaza adjacent to Main Street.

Zoning

The proposed project is located in the Town Center 1 (TC-1) zoning district. The property is also within the Historic District (HD) overlay zone. The TC-1 zone is Logan City's core downtown zone permitting a wide range of commercial, institutional and civic uses along with high-density residential housing. Historically considered the center of town, this block adjacent to Center Street and Main Street holds a significant core-position in the city. As per the Logan City Land Development Code (LDC) 17.11.030, the TC-1 zone permits restaurant, retail, office, parks, public squares, and recreation facilities.

Planning Commission Scope

The LDC 17.43 requires a Design Review Permit for new public development. The Planning Commission is the review authority for Design Review Permits based on the project's compliance with the LDC standards and regulations. For projects located inside the Historic District (HD), the LDC 17.20 requires a Certificate of Appropriateness for demolitions, exterior façade remodels and compatibility for new construction. The Logan City Historic Preservation Committee is the review authority for a Certificate of Appropriateness. In this case, where a project proposal requires both permits/certificates, the applicant will present the project to both hearing bodies for review. The Planning Commission will review and assure compliance with proposed land use, maximum heights, setbacks, lot coverages, site design, open space and vehicular parking requirements. When this project is presented to the Historic Preservation Committee, they will consider the demolition of contributing structures and the exterior façade designs.

Setbacks

The LDC requirements for building setbacks in the TC-1 zone are as follows (as measured from property lines):

Front:	0-5'
Side:	0'
Rear:	5'

The 41 & 45 N. Buildings proposed to remain and are entitled to their setbacks as is. The primary land use proposed is a public plaza which primarily consists of open space and would not be subject to setback requirements. The project is proposed with a number of accessory structures that include 27 foot tall east/west brick gateway features, steel trellis structures, stage and a 17 foot tall bathroom/storage buildings. Evaluated individually and considering this site as a double fronted site (east gateway/west gateway), each accessory structure meets the requirements of the LDC as submitted.

Lot Coverage & Frontage

The LDC allows a 100% lot coverage and requires a minimum 75% building frontage in the TC-1 zone. The goal of this code requirement is to allow for dense urban development within the downtown area. This proposal includes the removal of three (3) buildings to create open space for the new public plaza. The proposal includes the stage, storage and bathroom structure, which would be considered an accessory structure to overall plaza. Because the proposed plaza does not contain primary building mass like, for example a new office building, the lot coverage and frontage requirement are not applicable for this proposal.

Parking

The LDC 17.31.090 specifically exempts new parking stall requirements for existing buildings within the TC zone that are remodeled and do not expand beyond the current walls or rooftop of the structure. For example, a downtown building that is remodeled within the walls of the structure from an office to a restaurant is not required to obtain additional parking stalls for the new restaurant. When new construction and development are proposed in areas outside of existing buildings or redevelopment results in demolishing buildings, the LDC parking requirements are applicable. For this project, 41 & 45 North Main buildings are exempt from

LDC standards because they are proposed to remain and the new plaza development, positioned where buildings 47, 55 & 67 North Main are planned for demolition would be required to meet the LDC parking requirements. LDC 17.31.040 does not specifically prescribe a standard or number of parking stalls parking for public parks/plazas but instead requires an alternative parking plan be submitted for review and approval by the Planning Commission. The attached parking plan and analysis states that this block contains over 300 private and public parking stalls. Based on the space analysis of the new plaza, the projected peak activity parking demand is 112 parking stalls. Standard plaza programming of ice skating in the winter and splash pad in the summer will have fewer participants and require a lower number of parking stalls. Plaza activities will also often be outside peak parking demand times of the day/week as other businesses located in the block. The Planning Commission will need to review and either approve or deny the proposed alternative parking plan.



Figure 2 shows the plaza in the summer with the adjacent west interior block parking lot.

STAFF SUMMARY

The proposed plaza and renovated building space will provide Logan City with unique and memorable gathering space for generations to come. Ice skating in the winter amongst the iconic downtown landmarks offers a special experience for users. Downtown has long lacked a public plaza space programmed for outdoor destination-based activities. This plaza will provide a draw to downtown that not only provides citizens with distinct outdoor opportunities but will greatly benefit surrounding commercial businesses as crowds assemble. The design and materials are durable and compatible with the character of downtown. The gateway and arching metal features along with the unique stage structure and landscaping will intimately enclose the space, buffer out noises and ultimately become landmarks themselves.

Land and space are precious commodities downtown and dense clusters of buildings with lively business activity has historically defined Logan's downtown character. If the code required new parking for every business conversion downtown resulting in the new businesses purchasing next door buildings to simply demolish them and build new parking lots, then before too long downtown would cease to be downtown. The numerous parking lots and spread-out buildings would sprawl development further out into suburb areas resulting in unnecessary infrastructure, more vehicle miles traveled daily, decreased walkability downtown and negative impacts to the

current synergy of densely clustered buildings and businesses downtown. For these reasons, staff argues that inefficiently used and vacant parking lots are more detrimental to the city than the occasional large event that requires distant parking. There are five public parking lots within a one-block radius on this block and numerous on-street parking opportunities for the occasional large event that may be programmed here. Parking a distance away from the event in downtown is a good thing for adjacent businesses. As event goers walk to their designation, they will pass numerous shops and restaurants enticing them to patronize before or after the event. For typical plaza programming (ice skating, splash pad), the adjacent rear parking lot will satisfy the parking demand. The attached 2011 downtown parking analysis shows approximately 2,900 off-street and 900 on-street parking stalls throughout the larger downtown area with peak demands at around 50% of capacity. Staff recommends that the Planning Commission approve the alternative parking plan for the Center Block Public Plaza as proposed.

AGENCY AND CITY DEPARTMENT COMMENTS

Comments were solicited from City departments or reviewing agencies and are included in the Conditions of Approval.

PUBLIC COMMENTS

Notices were mailed to property owners within 300 feet of the subject property. No comments have been received.

PUBLIC NOTIFICATION

Legal notices were published in the Herald Journal on 3/27/21 posted on the City's website and the Utah Public Meeting website on 4/2/21 and mailed to property owners within 300 feet on 3/22/21.

RECOMMENDED CONDITIONS OF APPROVAL

This project is subject to the proponent or property owner agreeing to comply with the following conditions as written, or as may be amended by the Planning Commission.

1. All standard conditions of approval will be recorded with the Design Review Permit and are available in the Community Development Department.
2. The Planning Commission approves the setbacks, bulk, height and layout of the plaza.
3. The Historic Preservation Committee will review the demolition of contributing structures, building designs and materials for historic compatibility and appropriateness.
4. The Planning Commission exempts parking requirements for the two buildings proposed to remain within their current walls and rooftop areas.
5. The Planning Commission accepts and approves the alternative parking plan for the proposed plaza based on existing parking in the vicinity and analysis provided.
6. If the proposed project is changed in design, layout or intensity prior to the submittal of a building permit, the applicant shall return to the Planning Commission for subsequent review and approvals.
7. Prior to recording of a Final Plat or issuance of a Building Permit, the Director of Community Development shall receive a written memorandum from the following departments indicating that their requirement has been satisfied:

a. Engineering:

- Provide water shares or in-leu fee for any increased demand to City system
- Provide storm water detention/retention for redeveloped area of the City Block per Logan City Storm Water design standards
- All construction/modifications within UDOT right of way shall be to UDOT standards
- Cap any existing water and sewer services that will not be used with the new development at the City main.

b. Water/Cross Connection:

- The commercial buildings water mains and rest rooms (containment after each meter) need to have their own RP (ASSE1013) installed and tested on the water mains as they enter the buildings before any branch offs or connections. Properly sized drain required to serve the dump port of RP's. If further review shows buildings 41-45 are a low hazard threat to the water system, then a DC (ASSE1015) may be used in place of the RP for containment (cities) protection.
- The landscape irrigation system's fed from Logan City water must have a RP (ASSE 1013) installed and tested. All backflow assemblies must be tested within 10 days of turning in water to them and annually thereafter.
- Fire suppression systems that are connected to Logan City water (with no added chemicals) must have a minimum DC (ASSE1015) installed and tested. Fire risers and B/F assemblies must be installed as per Logan City standards.
- All points of use of water must comply with the 2018 IPC and State of Utah Amendments, during and after construction. Splash pad must be fed threw an air gap or RP (ASSE1013). Suggest the Ice Skate Rink (if water feed is to be permanently connected to city water) and rest rooms B/F assemblies be installed indoors where its heated for wintertime use.
- Project shall comply with all current plumbing codes, Utah State Amendments, Utah Division of Drinking Water rules and regulations including, but not limited to, those pertaining to backflow protection and cross connection prevention.

c. Environmental (Waste Management):

- Location will use the refuse compactor in the middle of the block.

RECOMMENDED FINDINGS FOR APPROVAL FOR THE DESIGN REVIEW PERMIT

The Planning Commission bases its decisions on the following findings supported in the administrative record for this project:

1. The proposed project is compatible with surrounding land uses and will not interfere with the use and enjoyment of adjacent properties as it meets the minimum requirements of the LDC.
2. The Design Review Permit conforms to the requirements of Title 17.47 concerning hearings, procedures, application requirements and plat preparations.
3. The project meets the goals and objectives of the Logan General Plan and the Downtown Specific Plan.
4. The project meets the site and building design requirements of the LDC as conditioned.

This staff report is an analysis of the application based on adopted city documents, standard city development practices, and available information. The report is to be used to review and consider the merits of the application prior to and during the course of the Planning Commission meeting. Additional information may be revealed by participants at the Planning Commission meeting which may modify the staff report and become the Certificate of Decision. The Director of Community Development reserves the right to supplement the material in the report with additional information at the Planning Commission meeting.



APPLICATION FOR PROJECT REVIEW

☒ **Planning Commission** ☐ **Land Use Appeal Board** ☐ **Administrative Review**

Date Received 3-8-21	Received By	Scheduled Meeting Date Apr. 8, 2021	Zone TC-1	Application Number PG 21-015
Type of Application (Check all that apply): <input checked="" type="checkbox"/> Design Review <input type="checkbox"/> Conditional Use <input type="checkbox"/> Subdivision <input type="checkbox"/> Zone Change <input type="checkbox"/> Code Amendment <input type="checkbox"/> Appeal <input type="checkbox"/> Other <input type="checkbox"/> Administrative Design Review				
PROJECT NAME LOGAN CITY - CENTER BLOCK PUBLIC PLAZA PROJECT				
PROJECT ADDRESS 41N – 67N MAIN STREET			COUNTY PLAT TAX ID # 06-025-0009, 0011, 06-028-0015, 0016	
AUTHORIZED AGENT FOR PROPERTY OWNER (Must be accurate and complete) Mike DeSimone			MAIN PHONE # 435.716.9021	
MAILING ADDRESS 290 NORTH 100 WEST		CITY LOGAN	STATE UT	ZIP 84321
EMAIL ADDRESS mike.desimone@loganutah.org				
PROPERTY OWNER OF RECORD (Must be listed) LOGAN CITY – HOLLY H. DAINES, MAYOR			MAIN PHONE # 435.716.9002	
MAILING ADDRESS 290 NORTH 100 WEST		CITY LOGAN	STATE UT	ZIP 84321
EMAIL ADDRESS holly.daines@loganutah.org				
DESCRIBE THE PROPOSED PROJECT AS IT SHOULD BE PRESENTED (Include as much detail as possible - attach a separate sheet if needed) Construct new 17,000 square foot Public Plaza containing portable ice rink, covered stage, restrooms, storage, splash pad, fire pits, entry features (east & west sides), shade structures, landscaping, lighting, street trees; demolish 47 – 67 N Main Street buildings; interior/exterior renovation of 41 – 45 N Main Street buildings; and upgrade utility services where necessary. - NO SITE ACTIVITY MAY OCCUR UNTIL AFTER APPROPRIATE COMMITTEE APPROVAL -			Total Lot Size (acres) 0.59 Acres (25,700 sf)	
			Size of Proposed New Building (square feet)	
			Number of Proposed New Units/Lots 0	
I certify that the information contained in this application and all supporting plans are correct and accurate. I also certify that I am authorized to sign all further legal documents and permit on behalf of the property owner.			Signature of Property Owner's Authorized Agent	
I certify that I am the property owner on record of the subject property and that I consent to the submittal of this project. I understand that all further legal documents and permits will be sent to my authorized agent listed above.			Signature of Property Owner	

Center Block Public Plaza Project Description

Logan City is constructing a new public plaza on the Center Block to provide year-round gathering and entertainment space for residents and visitors alike. The Plaza, which will be in the footprint of the Emporium and Poco Loco Buildings, fronts onto Main Street with significant views of the Tabernacle to the east while opening a new visual connection towards the west.

This public plaza includes a portable ice rink and fire pits for winter time enjoyment and a small splash pad for summer time use. A stage area along the western side of the plaza will provide a variety of entertainment opportunities throughout the year. There is ample space for outdoor seating and dining from adjacent restaurants. The plaza will be programmed with a variety of uses and activities throughout the entire year to draw people downtown, and will be continuously updated with fresh landscaping and decorations to coincide with the seasons.

The Cardon Building (41) and the Plaza 45 Building (45) will both be beautifully restored to house future restaurant, retail and office space fronting out onto the new space. The purposeful design of the plaza builds upon the renovation of Center Street in stimulating new private downtown investment with a common goal of revitalization and placemaking.

The plaza will be enhanced with generous seating and landscaped areas along with shade structures. The seating and planters along the northern portion of the plaza will be movable to accommodate future renovations of the Levin's Building including windows and doorways out onto the plaza. The east side of the plaza includes two entrance gateways along with iron trellis work, bricked columns, and bricked landscaped walls, all of which help to frame the plaza and tie it in with the Center Street archway and the historic character of downtown. From a practical standpoint, these design features will provide some sound attenuate along Main Street while also adding a safety element separating the plaza from the busy street. The west side of plaza contains the stage and utility areas along with another entrance gateway. The large blank wall area on the Plaza 45 building facing the plaza will be used for vertical banners and/or rotating public wall art.

The public plaza is funded and we are anticipating demolition and construction to be initiated later in 2021.

Project Specifications

Overall 17,000 square foot Public Plaza containing the following elements:

- 14,000 square foot Plaza Space
- 5,600 square foot Portable Ice Rink
- 2,472 square foot covered stage, restrooms & Zamboni storage
- Splash Pad
- Fire Pits
- Outdoor Dining & Seating
- Bricked columns and planters along Main Street & parking area
- Distinct plaza entrance gateways
- Approximately 23,000 square feet of renovated building area for new restaurant, retail, and office space

Buildings

- 41 N. Main (Cardon Building) – Grade B (contributory) building on the 2011 Reconnaissance Level Survey (RLS) will have both facades restored to include a restoration of existing materials and/or replacement of materials with new similar materials dependent upon building condition. Interior to be completely remodeled.
- 45 N. Main (Plaza 45) – Grade C (non-contributory) building on the 2011 RLS will have the east and west facades restored to include a restoration of existing materials and/or replacement of materials with new similar materials dependent upon building condition. The north façade fronting the plaza will be modified to include new doors, windows, brick, stucco, and trim details. Both the front and rear facades will be restored and/or upgraded to match. Interior to be completely remodeled.
- 47 N. Main – Grade C (non-contributory) building on the 2011 RLS will be removed.
- 55 N. Main (Emporium) – Grade D (non-contributory) building(s) on the 2011 RLS will be removed.
- 67 N. Main (City Drug/Poco Loco) – Grade B (contributory) building on the 2011 RLS will be removed.
- A total building footprint of 17,000 square feet will be removed (47,55,67) and replaced with the plaza.

Parking

- Center Block contains approximately 300 public & private stalls
- Plaza use and activities will generally coincide with times when parking demand is typically at its lowest (evenings)
- Parking demands of new uses are consistent with uses being replaced (retail, restaurant, office)
- Plaza and building renovations are occurring within the footprint of existing buildings
- Land Development Code 17.31.090 governs parking requirements by permitting the expansion of uses within the footprint of existing buildings in the TC zone
- Peak Parking Demand is approximately 112 stalls based on the use type and if all uses are being utilized at one time – office space, concert, ice rink, plaza

Funding

- Logan City Redevelopment Agency approved \$4,347,905 for building demolition and Center Block Plaza construction on March 2, 2021
 - Demolition - \$1,370,000
 - Plaza - \$2,129,525
 - Ice Rink - \$397,840
 - Stage Building - \$440,540
- Logan City Redevelopment Agency approved \$1,575,157 for building improvements at 41 & 45 North Main Street on March 2, 2021

Timing

- Project to start fall 2021 with construction completed fall of 2022

Process

- RDA Funding Approval March 2, 2021
- Planning Commission Design Review April 8, 2021
- Historic Preservation Commission Certificate of Appropriateness April 19, 2021



MEMORANDUM TO FILE

DATE: March 8, 2021
FROM: Mike DeSimone, Director
SUBJECT: Parking Summary – Center Block Public Plaza Project

Background

This information summarizes the parking analysis that has been conducted in downtown Logan, first with two “point in time” parking studies conducted in 2019 & 2020, and second Appendix B from the Downtown Specific Plan. The purpose of this information is to demonstrate that there is not a parking problem in downtown; rather, there is a problem with convenient parking in downtown. The first set of information were a use survey conducted in 2019 by two college students tracking seven days of parking activity on the Center Block, and a second brief study conducted in 2020 by Kirk Jensen. These two use studies were in response to the proposed Cowboy Partner residential project on Center Block. The second set of information is from the Downtown Specific Plan in 2011 and was prepared by Wilbur Smith, an engineering consultant firm retained by Logan City. All three demonstrate that there is an abundance of available parking either on the Center Block or surrounding areas available for this project or other new proposals in the downtown area.

2010/2011 Parking Study Summary

- **Larger Downtown Area**
- Total Available Parking Stalls: 3,897
- Available Off-Street Parking Stalls: 2,980
- Available On-Street Parking Stalls: 917
- Average Demand: 39% (9 am - 5 pm)
- Peak Demand: 48% (lunchtime)
- Peak Demand: 1,887 Full Stalls & 2,010 Vacant Stalls
- **Conclusion: Adequate Parking Exists & is Available in the Downtown Through 2050**

Downtown Specific Plan Parking Analysis



Downtown Specific Plan Parking Analysis



Table 1: Total Parking Demand										
Demand by Facility	Hour of Observation ¹									
	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	Average ²
Total (3,897 spaces)	1,341	1,412	1,534	1,887	1,038	1,627	1,649	1,825	1,369	1,520
	34%	36%	39%	48%	27%	42%	42%	47%	35%	39%
On-Street (917 spaces)	162	217	248	270	196	252	261	281	257	238
	18%	24%	27%	29%	21%	27%	28%	31%	28%	26%
Off-Street (2,980 spaces)	1,179	1,195	1,286	1,617	842	1,375	1,388	1,544	1,112	1,282
	40%	40%	43%	54%	28%	46%	47%	52%	37%	43%

Notes:

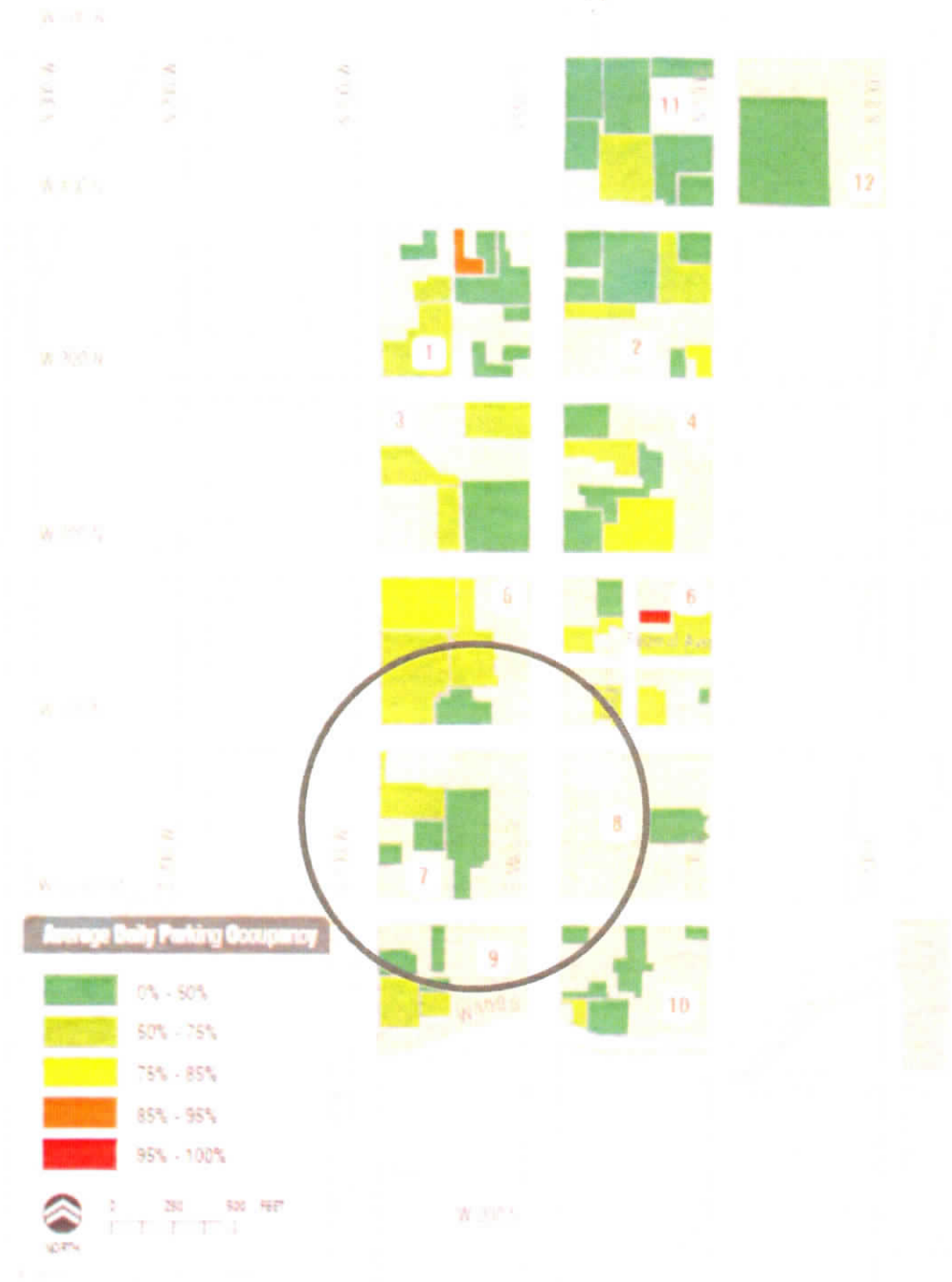
Source: Wilbur Smith Associates (March 2010)

Appendix B & C in the Downtown Logan Specific Plan

Downtown Logan Specific Plan
On Street Parking Demand
Figure 3.4



Downtown Logan Specific Plan Off Street Parking Demand Figure 3.5



2019 Center Block Parking Analysis

Center Block Parking Study 306 Total Parking Stalls								
Time	10/8/2019 Tuesday	10/9/2019 Wednesday	10/10/2019 Thursday	10/11/2019 Friday	10/12/2019 Saturday	10/13/2019 Sunday	10/14/2019 Monday	10/22/2019 Tuesday
7:15 AM	29	21	21	27	25	16	16	21
8:15 AM	64	52	48	54	33	15	33	47
9:15 AM	76	80	94	86	71	27	74	80
10:15 AM	114	106	121	110	105	36	99	122
11:15 AM	129	120	137	152	140	43	103	142
12:15 PM	173	143	164	171	161	46	118	163
1:15 PM	157	168	173	188	167	41	128	157
2:15 PM	138	131	170	171	149	34	118	140
3:15 PM	132	124	155	154	156	38	117	151
4:15 PM	126	123	149	143	144	31	118	143
5:15 PM	101	103	148	121	126	22	131	126
6:15 PM	92	91	116	123	130	23	106	122
7:15 PM	70	86	99	108	142	21	103	115
8:00 PM	58	78	113	102	111	18	98	112

2019 Center Block Truck Traffic Study Summary

- **Average Daily Truck Traffic: 14 Trucks per day (7 am – 8 pm)**
- **Busiest Day: Friday – 24 Trucks**
- **Proposed Site Plan Accommodates Truck Traffic**

2019 Center Block Parking Summary

- Available Off-Street Parking Stalls: 306
- Average Demand: 40% (7 am – 8 pm)
- Average Demand: 122 Full Stalls & 183 Vacant Stalls
- Peak Demand: 61% (Friday - Lunchtime) 188 Full Stalls & 118 Vacant Stalls
- On-Street Parking Adjacent to Block: 87 (not included in count)
- Total Stalls Available: 393
- Project Ensures Internal Truck Traffic is not Impeded
- **Study Conclusion: Adequate Off-Street Parking Available on Center Block**

2/21/2020 (Friday 7 pm) Snapshot of Parking

County Block
Capacity: 348
Utilization: 71 (20%)

LCSD Lot
Capacity: 64
Utilization: 5 (8%)

Center Block
Capacity: 306
Utilization: 106 (35%)

City Lot 1st West
Capacity: 52
Utilization: 1 (2%)

Theater Block
Capacity: 280
Utilization: 110 (39%)



2/24/2020 (Monday 3 pm) Snapshot of Parking

County Block
Capacity: 348
Utilization: 236 (68%)

100 North (On Street)
Capacity: 53
Utilization: 29 (55%)

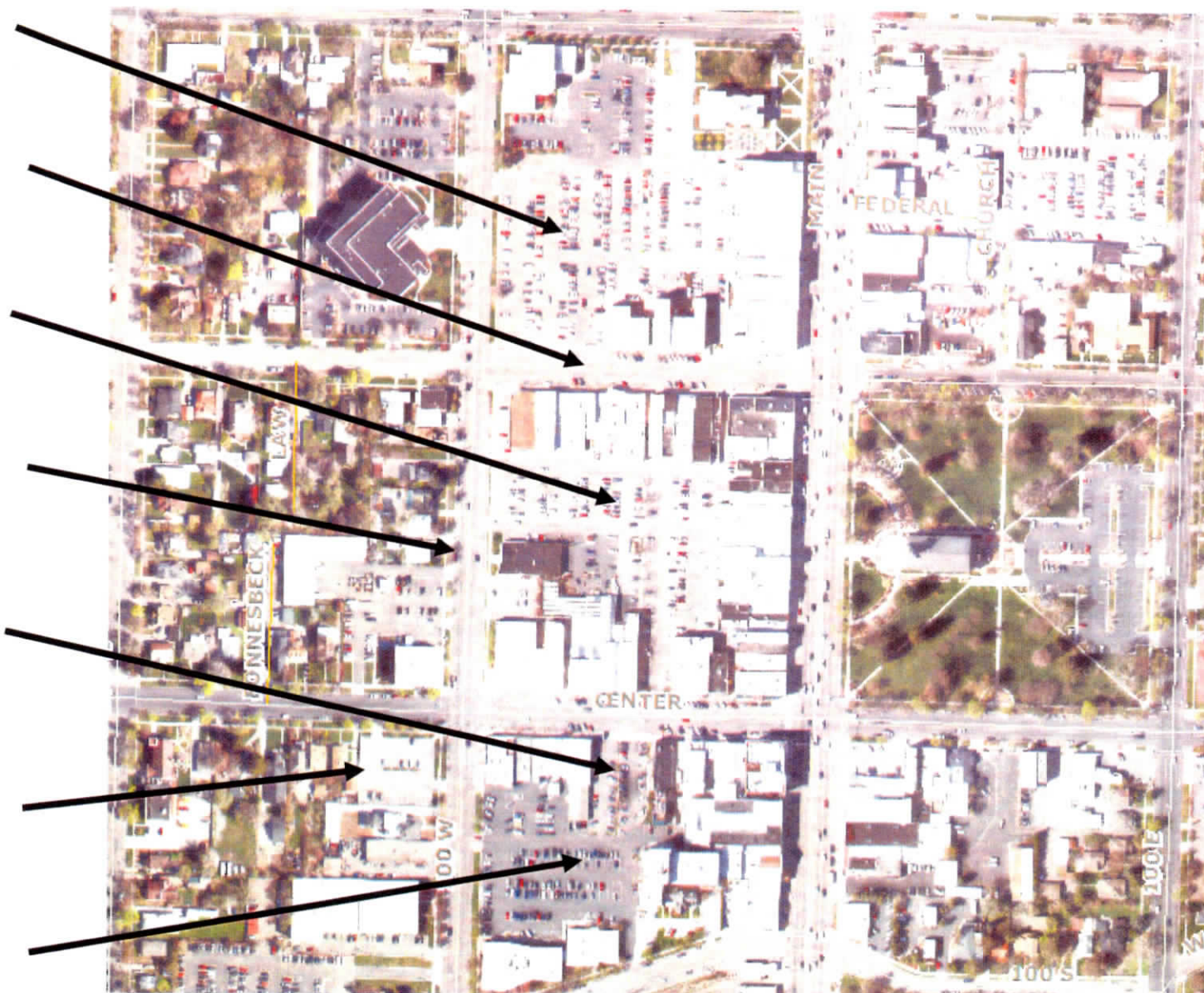
Center Block
Capacity: 306
Utilization: 177 (58%)

100 West (On Street)
Capacity: 30
Utilization: 3 (10%)

Center St. Lot
Capacity: 35
Utilization: 18 (51%)

City Lot 1st West
Capacity: 52
Utilization: 5 (2%)

Theater Block (public)
Capacity: 30
Utilization: 17 (57%)



General Parking Conclusions

- Weekends & Evenings – More than 70% of the existing parking stalls are available both on Center Block and in broader Downtown Area for large events
- Parking Utilization generally correlates with 11 am – 4 pm
- Peak Demands generally correlate with a lunchtime demand
- Average Parking Demand: 40% (7 am – 8 pm)
- Peak Parking Demand: 50% - 60% (Friday - Lunchtime)
- Center Block Peak Demand: 188 Full Stalls & 118 Vacant Stalls
- Downtown Peak Demand: 1,887 Full Stalls & 2,010 Vacant Stalls

APPENDIX B – PARKING ANALYSIS: EXISTING CONDITIONS

MEMO

Date: April 28, 2010

To: Bill Young
Logan City
290 North, 100 West
Logan, UT 84321

From: Terri O'Connor, AICP
Peter Costa

Subject: **Technical Memorandum: Logan City Downtown Specific Plan – Task 1
Parking Analysis**

This technical memorandum includes a detailed parking supply and demand analysis for the on- and off-street parking environs encompassing the downtown region of Logan, Utah. Current parking inventory and supply in the downtown area, which includes the existing number of parking spaces located along the downtown streets (on-street facilities) and parking spaces located in public and private parking lots (off-street facilities) were examined. Parking occupancies (number of observed parked vehicles) were recorded during a typical weekday to understand current parking behavior and patterns throughout the downtown area. Additional parking information, including current parking regulations and time restrictions were accounted in the study.

The following sections include a detailed description of the study area, the methodology performed to collect parking utilization data, the existing parking behaviors throughout the study area, and study findings.

1.0 Study Area

The extents of the parking study area include 500 North (to the north), 100 South (to the south), 100 East (to the east), and 100 West (to the west). This area is comprised of several land uses ranging from governmental offices, shopping centers, theaters, commercial businesses, grocers, hotels, banks, and residences (including single-family and multifamily homes). Major generators include City Hall, Logan City Public Library, U.S. Postal Office, Cache County Courthouse, Cache County Administration Building, Emporium Shopping Center, Ellen Eccles Theater, and major grocers including Albertson's and Smith's Food and Drug Center. It should be noted that the parking analysis performed did not include the entire area of the Logan City Downtown Specific Plan Area. After discussions with Logan City Engineering Staff and evaluating current parking locations and land use applications, the determination was made to focus the parking analysis on the central business core of downtown. As mentioned, the parking analysis area included 100 West, Main Street, and 100 East from 100 South to 400 North. Also included in the parking analysis were the two blocks along the north side of 400 North, which include Albertsons and Smiths grocery stores. To the east and west of parking study area is mostly residential and does not figure into the parking analysis of the Downtown Specific Plan. In addition, based on the available resources, this area was determined the most valuable source of information to use for the parking analysis. If needed for future evaluation, predictions and forecasts can be made based on the

information collected within the parking study limits. The analysis area and information collected is well within the parameters needed to produce a credible parking analysis and evaluation for this area.

The transportation system in the downtown area is built in a traditional, grid-like network, with all of the roadways operating in a north-south and east-west orientation. In general, the major retail, commercial and entertainment corridor is located along Main Street (the major north-south route), with several businesses, civic, and institutional buildings located to the east and west of Main Street. Structural and building design along Main Street consists of storefront and building façades lining the corridor with off-street parking located in the rear of the buildings. Larger surface lot parking is evident along the periphery of the downtown area, typically located in areas with shopping centers, and big box grocery stores. On-street parking spaces (parallel, diagonal, or perpendicular) are located throughout the entire study area and the majority of these on-street parking facilities are located along each downtown block.

1.1 Downtown Block Area

The study area is comprised of 12 downtown city blocks. Each block contains several distinct land uses with unique parking facilities.¹ The existing land uses and parking operations per study block are described below:

- **Block 1** is located in the northwest quadrant of the study area. General land use types include office, and retail uses as well as single-family residences along the western portion. Parking is generally located off-street in the rear of the buildings.
- **Block 2** is a developed area consisting of primarily retail uses. The Four North Shopping Center and the Wilson Motor automotive dealership are the major land uses within the block. Few residential homes are located in the southeast quadrant. The majority of parking is located off-street, in surface lots with striped parking stalls for employees and customers.
- **Block 3** contains mostly governmental and public land uses. Logan City Hall, Logan Public Library, and the U.S. Post Office are located within the block. The block contains interconnected surface parking lots and sidewalks; allowing convenient access to these buildings for employees and customers.
- **Block 4** is comprised of mostly service-oriented land uses, including a hotel, bank, fast-food restaurant and offices are located in the north, south, and western portions of the block. Each building contains a surface parking lot to accommodate employees and customers. Single-family and multifamily homes are located in the eastern portion of the block.
- **Block 5** includes the Cache County Courthouse, and Cache County Administration Building as well as several retail uses along the south and eastern portions of the block. Since most of the building façades abut Main Street, off-street parking is located in the rear of these buildings for employees and customers.
- **Block 6** contains a wide array of land uses, including mixed-use development (primarily retail on the bottom floor and offices on the top floors) along the western portion of the block, restaurants, clinical services and religious institutions. Off-street parking near each use is rather sparse throughout most of the block; however a sizeable surface lot is located in the northeast quadrant of the block.
- **Block 7** is primarily occupied by the Emporium Shopping Center. The general layout of the block contains several mixed-use building with retail and restaurants along the ground floors of each building and office uses located on the top floors. A substantial off-street surface parking lot is located in the middle portion of the block, (in the rear of each building).

¹ Study blocks were designated and assigned by Logan City Staff (March 2010).

- **Block 8** is solely inhabited by the Latter-day Saints Tabernacle, a religious institution that is located in the middle of the block surrounded by greenery and open space. An off-street, surface lot is located adjacent to the church to accommodate visitors.
- **Block 9** is located in the southwest quadrant of the study area. General uses located within the block include a bank and retail stores in the northern section, the Ellen Eccles Theatre in the eastern section, and offices along the southern section of the study block. Similar to other study blocks, several off-street surface parking lots are located in the rear of these establishments.
- **Block 10** contains general retail and service-oriented uses along with residences along the southeast section of the block. There is one primary surface parking lot located in the middle of the block to service these uses; and there are few ancillary lots available for customers and employees along the periphery of the study block.
- **Block 11** is located in the northernmost quadrant of the study area; comprising of a large surface parking lot to accommodate the Albertson's Grocery Store, and ancillary buildings on the edges of the study block, including a bank and restaurants.
- **Block 12** contains a vast off-street surface parking lot to accommodate the Smith's Food and Drug Center (grocery store), as well as the Cache Valley Transit District (CVTD) Transit Center located in the northern section of the study block.

The study area, including each study block is presented in Figure 1.

2.0 Data Collection Methodology

Parking data was collected and recorded by the Logan City Staff. Current parking conditions in and around the study area, including parking inventory, supply, and demand for all on- and off-street parking facilities were observed. These observations include the location of parking spaces, the total number of existing parking spaces, and categorization of parking spaces, current parking restrictions, and the total amount of observed parked vehicles throughout the entire study area.

In order to determine typical, weekday parking conditions, parking data was collected during several midweek days. Hourly parking occupancy data (the amount of observed parked vehicles per hour) was collected on Wednesday, March 3rd, 2010 and on Thursday, March 4th, 2010 during the hours of 9:00 AM and 5:00 PM. Parking inventory (the type of parking spaces), and parking supply (the amount of existing parking spaces) data was collected during the week of March 22nd, 2010.

All available parking facilities were accounted for during the field reconnaissance. These include all on- and off-street parking facilities that are public or privately-owned and are available to the public and/or only employees and customers. Approximately 62 off-street, surface parking lots and 67 on-street (blockfaces) were analyzed during the data collection effort.

In order to determine the appropriate amount of the available parking throughout the study area; field analysts counted and recorded the number of striped, marked, and/or designated parking spaces within each observed off-street facility. For on-street parking, the amount of spaces that were clearly painted or marked was included in the total on-street supply. In addition, for on-street facilities along roadways without painted, striped parking spaces, the field analyst would apply a measurement of every 20 feet to identify an available on-street parking space; and include these spaces in the total supply count.²

² This measurement is an industry standard and is commonly applied when on-street parking spaces are not clearly defined. This measurement was approved by Logan City Staff (March 2010).

LOGAN CITY DOWNTOWN SPECIFIC PLAN PARKING ANALYSIS



STUDY AREA AND STUDY BLOCKS
Figure 1

3.0 Existing Parking Capacity

The following includes a detailed description of existing parking inventory and supply throughout the study area. Existing parking restrictions and the location of these restricted areas are also discussed in the following sections.

3.1 Existing Parking Inventory and Supply

As discussed in Section 1.1, each study block contains a significant amount of on-street parking (typically located along all four curbsides (or "blockfaces") of each block) and substantial off-street parking, typically located in the rear of most buildings. The study area is comprised of approximately 3,897 parking spaces; 917 spaces are located on-street, along the local roadways and 2,980 spaces are located off-street, in surface parking lots. Table 1 presents the parking inventory and supply per study block area.

Table 1: Existing Parking Inventory and Supply						
Study Block	Supply		Parking Typology			
	Off-Street	On-Street	Regular ¹	Disabled	Permit	Time-Limit ²
1	283	63	338			8
2	186	49	227			8
3	304	63	353			14
4	245	66	297			14
5	378	80	412			46
6	262	133	160	3		232
7	275	104	270	7		102
8	84	100	114			70
9	210	97	141	4	9	153
10	163	71	212			22
11	313	48	361			0
12	277	43	320			0
Total	2,980 (76%)	917 (24%)	3,205 (82%)	14 (<1%)	9 (<1%)	669 (17%)

Notes:

Source: Logan City Staff, Wilbur Smith Associates (March 2010).

1. Represents parking supply that is not subject to current parking restrictions (time limits, disabled, and permit).
2. Time-limited spaces include current 20-minute, 30-minute, 2-hour, 4-hour, and 8-hour parking restrictions.

Of the total parking supply, approximately 3,205 spaces are regular, unrestricted parking spaces; 669 spaces are time-restricted; 14 spaces are dedicated for disabled parking; and nine (9) are permit-only spaces. The following section includes a detailed description of current parking restrictions throughout the study area.

3.2 Existing Parking Restrictions

Parking restrictions are often enforced to manage parking demand and to encourage parking turnover in areas of relative high demand. These procedures attempt to assure parking availability and convenience for short-term users (i.e. customers, visitors) while encouraging long-term users (i.e. employees) to park their vehicles in other appropriate locations. There are a variety of parking management techniques that can be applied, such as pricing (i.e. metering), time limits, enforcement (i.e. monitoring), and policy management (i.e. Residential Parking Permits "RPPs"). These strategies may vary by demand and adjacent land uses.

Currently, there are established time limits for several parking facilities in the study area. In addition, there are no metered parking spaces or RPP zones within the study area. Field observations indicated that there are approximately 692 parking spaces that are time limited spaces or restricted based on user type (i.e. disabled, permit parking). Of the total restricted spaces, 462 spaces are located on-street and the remaining 230 spaces are located within specific off-street parking lots. Furthermore, field observations

indicated that 516 spaces are designated, 2-hour time limited spaces (75 percent of total restricted spaces); 32 spaces are 30-minute parking; 104 spaces are 8-hour parking; 14 spaces are disabled; 13 spaces are 4-hour parking; nine (9) spaces are permit-only parking; and eight (8) spaces are 20-minute parking. In addition, there are few areas where parking is prohibited; these areas are outlined below:

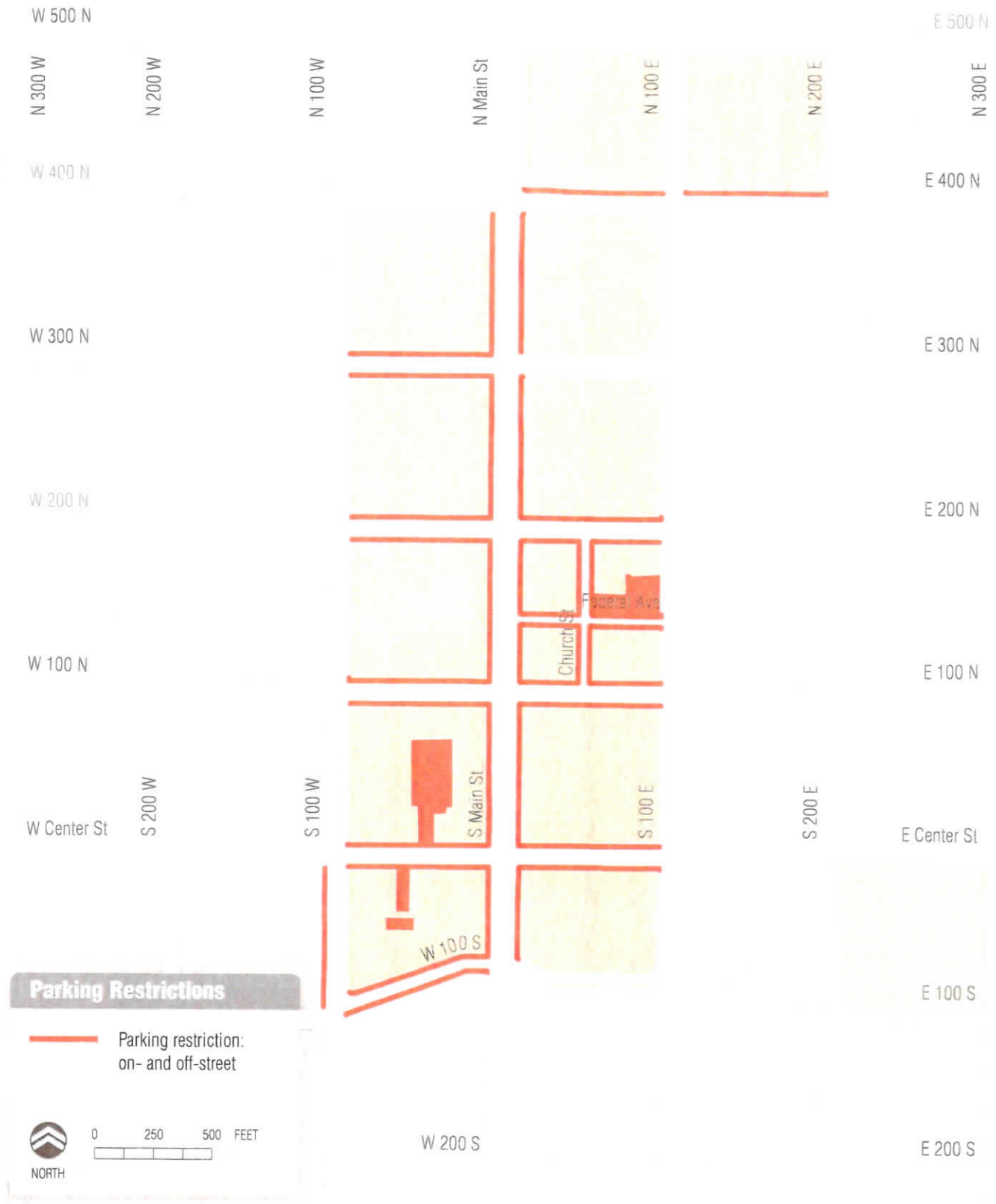
- Both sides of West 200 North Street between North 100 West and North Main streets;
- Both sides of West 300 North Street between North 100 West and North Main streets;
- North side of East 400 North Street between North Main and North 200 East streets.

Table 2 presents existing parking restrictions, including the location, type of restriction, and amount of spaces subject to these restrictions. Figure 2 illustrates the parking facilities that currently have a parking restriction.

Table 2: Existing Parking Restrictions			
Restricted Area	To/From	Restriction Type	Duration/ Number of Spaces
100 South	West of Main St. to 100 West (both sides)	Time Limited	4-Hour Parking/36 spaces (24 spaces 4-hour, 12 no limit)
Main Street	100 South to Center St. West (west side)	Time Limited	20-Minute Parking, 2-Hour Parking/12 spaces (4 20-minute parking, 8 regular 2hr parking)
	100 South to Center St. East (east side)	Time Limited	2-Hour Parking/8 spaces
	400 North to Center St. (both sides)	Time Limited	2-Hour Parking/102 spaces
Center Street	West of Main St. to 100 West (both sides)	Time Limited	2-Hour Parking/42 spaces
	East of Main St. to 100 East (both sides)	Time Limited	2-Hour Parking/52 spaces
Wells Fargo Lot	W Center St, between 100 W and Main St.	Time Limited	2-Hour Parking, Permit-only, Disabled/37 spaces (9 permit, 2 disabled, 26 regular 2hr limit)
Eccles Theatre Lot	Between W Center St./100 S/ S Main St./100W	Time Limited	2-Hour Parking, Disabled/26 spaces (2 disabled, 24 regular 2hr limit)
100 North	West of Main St. (south side)	Time Limited	2-Hour Parking/32 spaces
	West of Main St. to 100 West (north side)	Time Limited	30-Minute Parking, 2-Hour Parking/28 spaces (4 30-minute, 24 regular 2hr limit)
	East of Main St. to 100 East (both sides)	Time Limited	2-Hour Parking/34 spaces
200 North	West of Main St. to 100 West (both sides)	No Parking	No parking allowed
	East of Main St. to 100 East (both sides)	Time Limited	2-Hour Parking/18 spaces
300 North	West of Main St. to 100 West (both sides)	No Parking	No parking allowed
400 North	East of Main St. to 200 East (north side)	No Parking	No parking allowed
100 West	Center St. to 100 South (west side)	Time Limited	20-Minute, 30-Minute Parking/ 29 spaces (4 20-minute, 4 30-minute parking, 21 no limit)
	Center St. to 100 North (east side)	Time Limited	2-Hour Parking/26 spaces
Emporium Lot	100 North/Center St – West of Main St.	Time Limited	30-Minute Parking, Disabled Parking/ 153 spaces (24 30-minute parking, 7 disabled, 122 no limit)
Church/ Federal Lot	Church St./Federal St. – East of Main St.	Time Limited	2-Hour Parking, 8-Hour Parking, Disabled/ 136 spaces (29 regular 2hr parking, 104 8hr parking, 3 disabled)
Church Street	200 North to 100 North	Time Limited	2-Hour Parking/30 spaces
Federal Street	Main St. to 100 East	Time Limited	2-Hour Parking/37 spaces

Source: Logan City Staff (March 2010).

LOGAN CITY DOWNTOWN SPECIFIC PLAN PARKING ANALYSIS



EXISTING PARKING RESTRICTIONS BY FACILITY
Figure 2

4.0 Existing Parking Utilization Analysis

Parking occupancy (demand) refers to the accumulation of parking over the course of the day. Occupancy during peak periods is the primary measure of parking usage and can identify the potential need for additional parking. A parking occupancy rate of 90 percent for off-street parking facilities is typically defined as “practical capacity” meaning that it has reached a balance point between supply and demand where there are sufficient empty spaces to assure parking availability. Practical capacity for on-street parking facilities is typically 85 percent. As occupancy rates climb towards 100 percent, drivers will resort to “cruising” for parking or may be tempted to park illegally- activities that can create severe traffic impacts.³ The following sections include a detailed evaluation of the existing weekday parking behaviors throughout the study area. Detailed parking utilization data is located in the Appendices.

4.1 Existing Parking Demand

The study area occupancy rates represent a ‘bell-shaped’ curve, with weekday on- and off-street demand observed to be relatively low during the morning period, increasing by midday, and decreasing incrementally during the course of the afternoon and evening hours. Field observations indicated that the total average occupancy rate for all observed on- and off-street parking facilities throughout the study area was 39 percent, and peak hour occupancy rate of 48 percent between the hours of 12:00 PM and 1:00 PM. This parking trend is likely attributed to the concentration of land uses within the study area, consisting mostly of business, government, service, retail, and restaurant-oriented uses. These uses normally generate a higher parking demand in the morning and afternoon hours rather than the late afternoon and evening hours, primarily due to typical operating (working) hours for employees and visitors. Table 3 presents the total hourly and average on- and off-street parking demand throughout the entire study area.

Table 3: Total Parking Demand										
Demand by Facility	Hour of Observation ¹									
	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	Average ²
Total (3,897 spaces)	1,341 34%	1,412 36%	1,534 39%	1,887 48%	1,038 27%	1,627 42%	1,649 42%	1,825 47%	1,369 35%	1,520 39%
On-Street (917 spaces)	162 18%	217 24%	248 27%	270 29%	196 21%	252 27%	261 28%	281 31%	257 28%	238 26%
Off-Street (2,980 spaces)	1,179 40%	1,195 40%	1,286 43%	1,617 54%	842 28%	1,375 46%	1,388 47%	1,544 52%	1,112 37%	1,282 43%

Notes:

Source: Wilbur Smith Associates (March 2010).

Bold represents observed peak hour occupancy rate per facility type.

1. Occupancy counts for Study Blocks 11 and 12 were observed only at 9:00 AM, 12:00 PM, and 4:00 PM.

2. Represents the weighted average parking occupancy rate per facility

4.2 Existing Off-Street Parking Demand

Field observations indicated that the off-street peak hour parking demand occurred between the hours of 12:00 PM and 1:00 PM. The overall occupancy rate during this hour was recorded at 54 percent. The average parking occupancy rate for all off-street facilities was 42 percent. Data findings identified two off-street parking lots that experienced an average occupancy rate above practical capacity; notably, the off-street lot located adjacent to the Logan City Fire Department Building (in study block #6), and the

³ Donald Shoup, *The High Cost of Free Parking*, Chapter 11: Cruising, p 290.

parking lot in the rear of the KSM Guitars building (in study block #1). However, the remaining 60 off-street facilities experienced an average parking demand rate below practical capacity. Table 4 presents the existing off-street parking demand per study block.

Table 4: Existing Off-Street Parking Demand											
Block	Supply	Hour of Observation ¹									Average ²
		9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	
1	283	96	125	136	135	129	147	147	143	130	132
		34%	44%	48%	48%	46%	52%	52%	51%	46%	47%
2	186	74	90	95	112		111	115	94	87	86
		40%	48%	51%	60%		60%	62%	51%	47%	46%
3	304	130	146	172	130	153	145	157	159	103	143
		43%	48%	57%	43%	50%	48%	52%	52%	34%	47%
4	245	84	96	103	114	102	117	109	92	80	100
		34%	39%	42%	47%	42%	48%	44%	38%	33%	41%
5	378	252	284	284	245		303	314	311	318	257
		67%	75%	75%	65%		80%	83%	82%	84%	68%
6	262	159	169	177	177	175	175	212	181	153	175
		61%	65%	68%	68%	67%	67%	81%	69%	58%	67%
7	275	67	100	121	195	211	184	133	99	97	134
		24%	36%	44%	71%	77%	67%	48%	36%	35%	49%
8	84	16	34	38	31	26	27	31	18	17	26
		19%	40%	45%	37%	31%	32%	37%	21%	20%	31%
9	210	93	114	112	105		125	130	129	83	99
		44%	54%	53%	50%		60%	62%	61%	40%	47%
10	163	30	37	48	41	46	41	40	41	44	41
		18%	23%	29%	25%	28%	25%	25%	25%	27%	25%
11	313	95			187				156		146
		30%			60%				50%		47%
12	277	83			145				121		116
		30%			52%				44%		42%
Total	2,980	1,179	1,195	1,286	1,617	842	1,375	1,388	1,544	1,112	1,282
		40%	40%	43%	54%	28%	46%	47%	52%	37%	43%

Notes:

Source: Logan City Staff, Wilbur Smith Associates (April 2010).

Bold represents observed peak hour occupancy rate per study block.

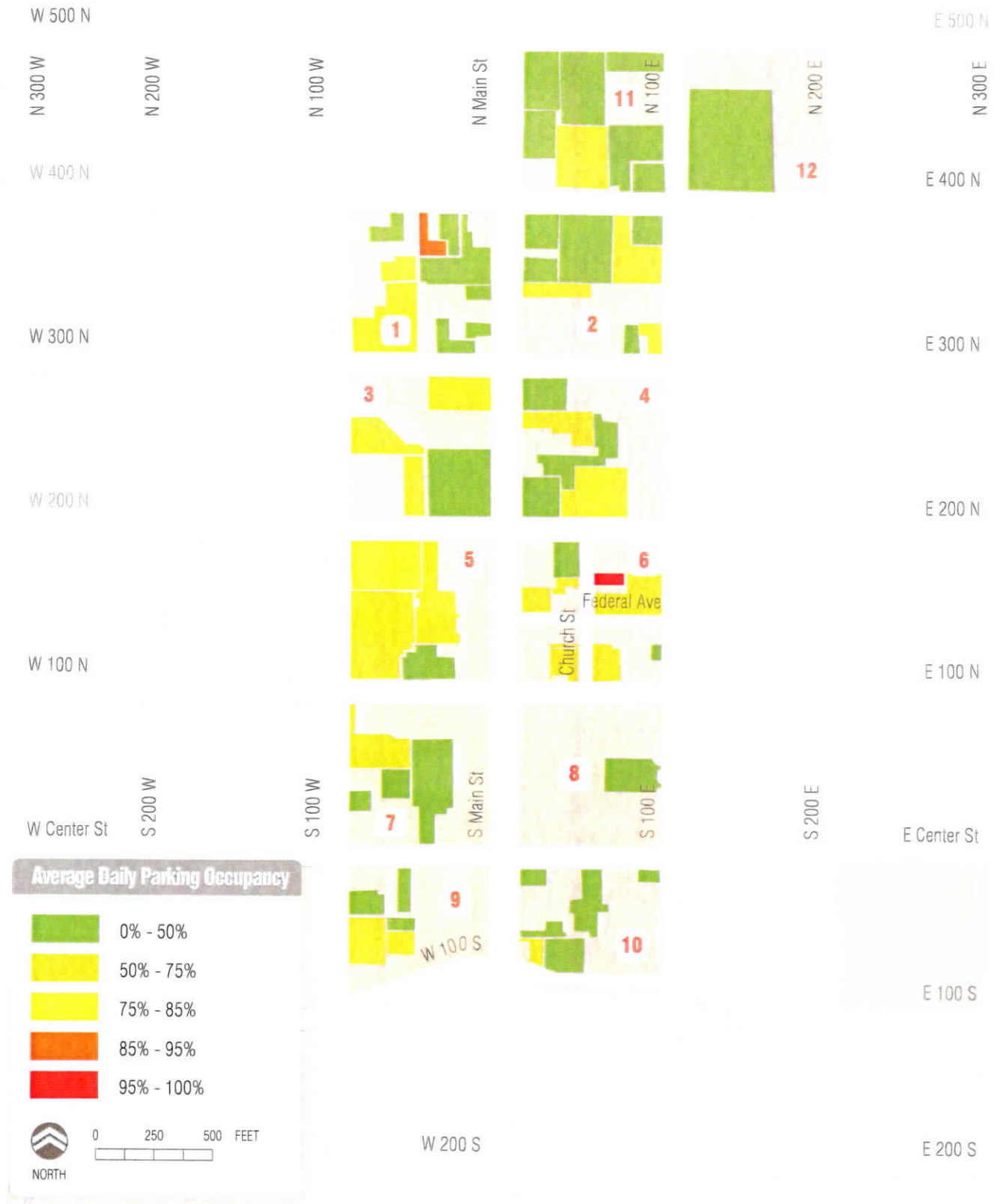
1. "Blank" brackets indicate parking occupancy data was not collected during a specific time.

2. Represents the weighted average parking occupancy rate per study block.

As Table 4 suggests, few study blocks throughout the course of the day experienced any significant amount of parking demand. Study blocks #5 and #6 reached parking occupancy rates above 80 percent between the observed hour of 3:00 PM and 4:00 PM; and study block #7 peaked at 77 percent during the hour of 1:00 PM and 2:00 PM. The remaining nine study blocks experienced a peak occupancy rate between 29 percent and 62 percent, respectively. Detailed off-street parking utilization data is located in Appendix A.

Existing off-street parking demand is illustrated in Figure 3.

LOGAN CITY DOWNTOWN SPECIFIC PLAN PARKING ANALYSIS



EXISTING OFF-STREET PARKING DEMAND
Figure 3

4.3 Existing On-Street Parking Demand

Field observations indicated that the on-street peak hour parking demand occurred between the hours of 4:00 PM and 5:00 PM. The overall occupancy rate during this hour was recorded at 31 percent. The average parking occupancy rate for all on-street facilities was 26 percent. Similar to the observed off-street parking trend; no on-street parking facilities experienced an occupancy rate at or above practical capacity (85 percent), and the throughout the course of the day, the majority of these facilities were underutilized. Table 5 presents the existing on-street parking demand per study block.

Table 5: Existing On-Street Parking Demand											
Block	Supply	Hour of Observation ¹									Average ²
		9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	
1	63	6	5	7	9	9	10	9	13	7	8
		10%	8%	11%	14%	14%	16%	14%	21%	11%	13%
2	49	9	16	22	13		17	18	14	13	14
		18%	25%	35%	21%		27%	29%	22%	21%	28%
3	63	23	33	29	26	34	36	40	41	35	33
		37%	52%	46%	41%	54%	57%	63%	65%	56%	52%
4	66	2	10	4	5	5	5	9	5	9	6
		3%	15%	6%	8%	8%	8%	14%	8%	14%	9%
5	80	8	17	17	13		16	17	25	19	15
		10%	21%	21%	16%		20%	21%	31%	24%	18%
6	133	49	61	65	68	64	63	62	57	63	61
		37%	46%	49%	51%	48%	47%	47%	43%	47%	46%
7	104	18	28	35	59	59	39	36	43	42	40
		17%	27%	34%	57%	57%	38%	35%	41%	40%	38%
8	100	9	13	20	19	19	21	18	17	14	17
		9%	13%	20%	19%	19%	21%	18%	17%	14%	17%
9	97	28	28	38	44		34	44	49	45	34
		29%	29%	39%	45%		35%	45%	51%	46%	36%
10	71	1	6	11	9	6	11	8	11	10	8
		1%	8%	15%	13%	8%	15%	11%	15%	14%	11%
11	48	7			5				5		6
		15%			10%				10%		12%
12	43	2			0				1		1
		5%			0%				2%		2%
Total	917	162	217	248	270	196	252	261	281	257	238
		18%	24%	27%	29%	21%	27%	28%	31%	28%	26%

Notes:

Source: Logan City Staff, Wilbur Smith Associates (April 2010).

Bold represents observed peak hour occupancy rate per study block.

1. "Blank" brackets indicate parking occupancy data was not collected during specific time.

2. Represents the weighted average parking occupancy rate per study block.

As presented in Table 5, all of the study blocks experienced occupancies well below practical capacity. The data indicated that Block #3 experienced the highest observed on-street parking occupancy rate throughout the course of the day, at 65 percent occupied between 4:00 PM and 5:00 PM, respectively. Blocks #6, #7, and #9 experienced peak hour occupancy rates between 51 percent and 57 percent; whereas the remaining eight study blocks experienced peak hour occupancy rates ranging from five (5) to 35 percent, respectively. Detailed on-street parking utilization data is located in Appendix B. Existing on-street parking demand is illustrated in Figure 4.

LOGAN CITY DOWNTOWN SPECIFIC PLAN PARKING ANALYSIS



EXISTING ON-STREET PARKING DEMAND
Figure 4

5.0 Study Findings

Study findings have shown that off-street and on-street parking demand patterns throughout the study area represent a common trend. As presented in the previous sections, both off- and on-street parking demand increases throughout the day, notably decreases by the mid-afternoon hours and incrementally increases by the early evening hours. This current parking trend reflects the typical parking behavior associated with existing land uses within the study area. These uses normally generate a higher parking demand in the morning and afternoon hours, primarily due to typical operating (working) hours for employees and visitors, and the substantial presence of retail and restaurants uses within the downtown area contribute to the incremental increase in demand during the evening hours.

Specific study blocks, including a concentration of retail and service-oriented businesses in the downtown area experienced relatively high on- and off-street parking demand in comparison to other study blocks. These areas include study blocks #6, and #7, which are occupied by the Emporium Shopping Center, and a multitude of local retail stores and office uses. Additional areas of notable parking demand include study blocks #3 and #5, which are comprised of mostly governmental buildings (i.e. Logan City Hall, Logan City Public Library, U.S. Post Office, Cache County Courthouse, and Cache County Administration Building); which typically generate higher parking demand during the midday hours, due to the significant presence of employees and patrons during this time period. It should be noted that these study blocks are located along Main Street and in the center core of the downtown area. Study blocks along the periphery of the study area (study blocks #1, #2, #8, #9, and #10) experienced low-to-moderate demand, which may be attributed to the amount of specialized services, special generators (i.e. LDS Tabernacle, Ellen Eccles Theatre), and current building vacancies located within these blocks. Other notable areas of low demand were study blocks #11 and #12, which contain two large, big box retail (grocery) stores; which experienced an average off-street occupancy rate of 42 and 47 percent and an on-street average occupancy rate of two (2) and 12 percent, respectively.

Overall, the study findings indicated that on- and off-street parking throughout the entire downtown area is underutilized; with less than five percent of all observed parking facilities experiencing demand at or above practical capacity. As a result, there is an abundant amount of available parking throughout the downtown area during a typical weekday. There are two key factors that may attribute to these findings: an evident oversupply in off-street, surface lot parking and current parking restrictions. As discussed in Section 1.1, the majority of study blocks in the downtown contain multiple surface parking lots. Field observations have indicated that within each study block, there is an even distribution in demand for off-street facilities, with no apparent parking constraints or periods of significant high parking demand throughout the day. As such, off-street parking demand is suppressed by the excess in parking supply, which contributes to the observed low-to-moderate occupancy rates. Current parking restrictions, which primarily include two-hour time limitations and a variation of other established time limits throughout the study area, may also support these findings. As stated in Section 4.3, the average on-street parking demand rate was observed at 26 percent, and a peak hour demand rate of 31 percent; which are well below practical capacity. These findings may be a direct result of visitors and other short-term users parking in unrestricted areas, primarily in off-street parking lots near their destination. More so, the majority of restricted parking spaces are located on-street, therefore these users may be avoiding these restrictions and opting to park in off-street facilities with no designated time limit.

The parking behavior and travel patterns discussed in the report may lead to positive changes to the downtown area. In knowing that weekday parking demand is relatively low and there is an evident oversupply of off-street parking; potential redevelopment and infill opportunities may become more practical. Further investigation into the relationship between future parking needs and future downtown development will be required to comprehensively understand and maximize these opportunities. The following section summarizes the appropriate measures required to identify the future parking needs and goals of the downtown area.

6.0 Next Steps

The “next steps” in the parking analysis will further investigate the relationship between existing land uses within the study area and current demand. Demand-based parking rates per land use type will be developed based on existing land use and demand data. These rates will be incorporated into a parking model that will calibrate demand-based rates against the current parking conditions within the study area. This will further determine future parking needs for the downtown area. In addition, the parking model will be used to assess parking needs under future conditions. These future scenarios will include planned land uses set forth by Logan City Staff. The parking model will identify future demand-based parking rates associated with these planned uses and identify the short-term and long-term parking needs and goals. Ultimately, the results of the parking demand analysis will assist in developing a range of strategies and recommendations that support alternative modes, and regulate both parking supply and demand throughout the study area.

APPENDIX

APPENDIX A

OFF-STREET PARKING UTILIZATION ANALYSIS

APPENDIX A - OFF-STREET PARKING UTILIZATION

Logan Parking Utilization Analysis

Location	Supply	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM
Block 1										
Interior (off street)										
P1	57	32	40	43	42	26	36	41	45	42
P2	14	4	6	5	6	11	10	13	9	11
P3	20	5	8	8	7	11	10	9	9	9
P4	23	12	18	17	17	19	23	28	23	19
P5	15	0	1	2	9	8	6	2	3	5
P6	93	37	47	54	46	43	50	45	44	31
P7	5	2	2	2	1	1	1	1	1	1
P8	14	0	0	0	1	1	0	0	1	0
P9	42	4	3	5	6	9	11	8	8	12
Total Supply/Demand	283	96	125	136	135	129	147	147	143	130
Average Occupancy	132	34%	44%	48%	48%	46%	52%	52%	51%	46%
Avg Occupancy Rate	47%									

Block 2										
Interior (off street)										
P1	54	36	35	36	35		28	33	28	28
P2	12	4	5	4	2		3	2	2	2
P3	4	0	2	2	2		2	2	2	3
P4	56	14	15	16	27		31	26	31	21
P5	30	11	21	22	23		27	21	22	21
P6	19	2	5	8	15		13	24	4	7
P7	5	5	5	5	5		5	5	4	4
P8	6	2	2	2	3		2	2	1	1
Total Supply/Demand	186	74	90	95	112	0	111	115	94	87
Average Occupancy	86	40%	48%	51%	50%	0%	60%	62%	51%	47%
Avg Occupancy Rate	46%									

Block 3										
Interior (off street)										
Post Office (ES)	44	29	28	30	26	31	24	31	23	14
City Hall (SS)	57	36	48	50	34	41	39	45	55	24
Library (NS)	52	33	34	37	29	28	41	30	41	39
Library (SS)	151	32	36	55	41	53	41	42	40	26
Total Supply/Demand	304	130	146	172	130	153	145	157	159	103
Average Occupancy	144	43%	48%	57%	43%	50%	48%	52%	52%	34%
Avg Occupancy Rate	47%									

Block 4										
Interior (off street)										
P1	46	31	41	43	34	36	51	51	34	28
P2	42	11	8	8	20	14	11	4	5	4
P3	83	17	12	12	11	11	14	9	9	13
P4	47	19	31	34	44	33	35	36	38	30
P5	27	6	4	6	5	8	6	9	6	5
Total Supply/Demand	245	84	96	103	114	102	117	109	92	80
Average Occupancy	100	34%	39%	42%	47%	42%	48%	44%	38%	33%
Avg Occupancy Rate	41%									

Block 5										
Interior (off street)										
State	102	95	88	84	73		92	103	93	90
County (WS)	178	102	140	142	125		152	149	153	158
County (Middle)	86	52	51	52	42		53	56	59	64
Wilson	12	3	5	8	5		6	6	6	6
Total Supply/Demand	378	252	284	284	245	0	303	314	311	318
Average Occupancy	257	67%	75%	75%	65%	0%	80%	83%	82%	84%
Avg Occupancy Rate	68%									

Block 6										
Interior (off street)										
P1	17	5	7	8	7	9	10	11	14	11
P2	28	18	18	16	14	17	18	23	19	22
P3	15	4	7	5	4	2	8	17	21	22
P4	10	3	4	2	3	4	3	31	3	2
P5	39	20	18	24	22	17	18	14	13	11
P6	15	16	15	14	22	19	17	17	14	11
P7	136	92	98	107	104	106	101	99	97	73
P8	4	1	2	1	1	1	0	0	0	1
Total Supply/Demand	262	159	169	177	177	175	175	212	181	153
Average Occupancy	175	61%	65%	68%	67%	67%	67%	81%	69%	58%
Avg Occupancy Rate	67%									

Parking Utilization rates

9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	weighted avg
56%	70%	75%	74%	46%	63%	72%	79%	74%	68%
29%	43%	36%	43%	79%	71%	93%	64%	79%	60%
25%	40%	40%	35%	55%	50%	45%	45%	45%	42%
52%	78%	74%	74%	83%	100%	122%	100%	83%	85%
0%	7%	13%	60%	53%	40%	13%	20%	33%	27%
40%	51%	58%	49%	46%	54%	48%	47%	33%	47%
40%	40%	40%	20%	20%	20%	20%	20%	20%	27%
0%	0%	0%	7%	7%	0%	0%	7%	0%	2%
10%	7%	12%	14%	21%	26%	19%	19%	29%	17%

67%	65%	67%	65%	0%	52%	61%	52%	52%	53%
33%	42%	33%	17%	0%	25%	17%	17%	17%	22%
0%	50%	50%	50%	0%	50%	50%	50%	75%	42%
25%	27%	29%	48%	0%	55%	46%	55%	38%	36%
37%	70%	73%	77%	0%	90%	70%	73%	70%	62%
11%	26%	42%	79%	0%	68%	126%	21%	37%	46%
100%	100%	100%	100%	0%	100%	100%	80%	80%	84%
33%	33%	33%	50%	0%	33%	33%	17%	17%	28%

66%	64%	68%	59%	70%	55%	70%	52%	32%	60%
63%	64%	86%	60%	72%	68%	79%	96%	42%	73%
63%	65%	71%	56%	79%	75%	79%	75%	75%	69%
21%	24%	36%	27%	35%	27%	28%	26%	17%	27%

67%	89%	93%	74%	78%	111%	111%	74%	61%	84%
26%	19%	19%	48%	33%	26%	10%	12%	10%	22%
20%	14%	14%	13%	13%	17%	11%	11%	14%	14%
40%	66%	72%	94%	70%	74%	77%	81%	64%	71%
22%	15%	22%	19%	30%	22%	33%	22%	19%	23%

93%	86%	82%	72%	0%	90%	101%	91%	88%	78%
57%	79%	80%	70%	0%	85%	84%	86%	89%	70%
60%	59%	60%	49%	0%	82%	65%	69%	74%	55%
25%	42%	50%	42%	0%	50%	50%	50%	50%	40%

29%	41%	47%	41%	53%	59%	65%	82%	65%	54%
69%	69%	62%	54%	65%	69%	88%	73%	85%	71%
27%	47%	33%	27%	13%	53%	113%	140%	147%	67%
30%	40%	20%	30%	40%	30%	310%	30%	20%	61%
51%	46%	62%	56%	44%	46%	36%	33%	28%	45%
107%	100%	93%	147%	127%	113%	113%	93%	73%	107%
68%	72%	79%	76%	78%	74%	73%	71%	54%	72%
25%	50%	25%	25%	25%	0%	0%	0%	25%	19%

APPENDIX A - OFF-STREET PARKING UTILIZATION

Logan Parking Utilization Analysis

Location	Supply	8:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM
Block 7										
Interior (off street)										
Logan City 1	15	2	2	3	7	6	6	5	6	3
Logan City 2	153	17	39	46	120	122	89	72	53	48
Logan City 3	81	45	53	70	82	75	86	52	36	40
Anderson	28	3	6	2	6	8	4	4	4	6
Total Supply/Demand	275	67	100	121	195	211	184	133	99	97
Utilization		24%	36%	44%	71%	77%	67%	48%	36%	35%
Average Occupancy	134									
Avg Occupancy Rate	49%									
Block 8										
Interior (off street)										
P1	64	16	34	36	31	26	27	31	18	17
Total Supply/Demand	64	16%	40%	45%	37%	31%	32%	37%	21%	20%
Utilization										
Average Occupancy	26									
Avg Occupancy Rate	31%									
Block 9										
Interior (off street)										
P1	42	12	15	16	15		14	20	22	15
P2	79	45	56	80	58		87	60	53	29
P3	26	24	23	21	17		23	21	17	8
P4	26	2	3	5	5		6	9	19	15
Wells Fargo	37	10	14	10	10		15	20	18	14
Total Supply/Demand	210	93	114	112	105	0	125	130	129	83
Utilization		44%	54%	53%	50%	0%	60%	62%	61%	40%
Average Occupancy	99									
Avg Occupancy Rate	47%									
Block 10										
Interior (off street)										
P1	18	9	9	9	9	7	7	6	7	12
P2	19	9	10	14	13	10	12	15	13	16
P3	20	3	5	8	5	11	4	5	11	9
P4	18	0	2	7	5	8	7	4	2	2
P5	70	5	4	7	6	8	8	3	4	2
P6	9	4	7	3	3	2	3	5	4	3
Total Supply/Demand	163	30	37	48	41	46	41	40	41	44
Utilization		18%	23%	29%	25%	28%	25%	25%	25%	27%
Average Occupancy	41									
Avg Occupancy Rate	25%									
Block 11										
Interior (off street)										
P1	13	0			5					1
P2	88	16			25					42
P3	85	40			87					62
P4	38	4			31					18
P5	41	17			17					17
P6	30	6			21					15
P7	7	3			1					1
Total Supply/Demand	313	85	0	0	187	0	0	0	0	156
Utilization		30%	0%	0%	60%	0%	0%	0%	0%	50%
Average Occupancy	146									
Avg Occupancy Rate	47%									
Block 12										
Interior (off street)										
P1	277	83			145					121
Total Supply/Demand	277	30%	0%	0%	52%	0%	0%	0%	44%	
Utilization										
Average Occupancy	116									
Avg Occupancy Rate	42%									

Parking Utilization rates

	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	weighted avg
Block 7	15%	13%	20%	47%	40%	40%	33%	40%	20%	30%
Block 8	11%	25%	30%	78%	80%	58%	47%	35%	31%	44%
Block 9	56%	65%	89%	77%	93%	100%	64%	44%	49%	71%
Block 10	12%	23%	8%	23%	31%	15%	15%	15%	23%	18%
Block 11	19%	40%	45%	37%	31%	32%	37%	21%	20%	31%
Block 12	29%	36%	36%	36%	0%	33%	48%	52%	36%	34%
	57%	75%	78%	73%	0%	85%	76%	67%	37%	81%
	82%	88%	81%	85%	0%	88%	81%	85%	35%	88%
	8%	12%	19%	19%	0%	23%	35%	73%	62%	28%
	27%	36%	27%	27%	0%	41%	54%	49%	38%	33%
	50%	50%	50%	50%	36%	38%	44%	39%	67%	48%
	47%	53%	74%	68%	53%	63%	79%	68%	84%	65%
	15%	25%	40%	25%	55%	20%	25%	55%	45%	34%
	0%	11%	36%	28%	44%	36%	22%	11%	11%	23%
	6%	5%	5%	8%	10%	10%	4%	5%	3%	7%
	44%	78%	33%	33%	22%	33%	56%	44%	33%	42%
	0%		36%					8%		15%
	18%		28%					47%		31%
	52%		92%					65%		68%
	11%		82%					47%		46%
	41%		41%					41%		41%
	20%		70%					50%		47%
	43%		14%					14%		24%
	30%			52%				44%		42%

APPENDIX B

ON-STREET PARKING UTILIZATION ANALYSIS

APPENDIX B - EXISTING ON-STREET PARKING UTILIZATION

Logan Parking Utilization Analysis

Location	Supply	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM
Block 1										
Main St (NS)	8	0	0	0	0	0	0	0	0	0
400 North (NS)	8	0	0	0	0	0	0	0	0	0
400 North (SS)	7	0	1	0	0	0	0	0	0	0
300 North (NS)	15	2	1	4	3	4	5	2	7	3
300 North (SS)	12	2	1	1	2	2	3	3	2	3
100 W (NS)	13	2	2	2	4	3	3	4	4	1
100 W (SS)	8	5	7	9	9	10	9	13	7	7
Total Supply/Demand	63	10%	8%	11%	14%	14%	16%	14%	21%	11%
Average Occupancy	8									
Avg Occupancy Rate	13%									
Block 2										
Main St (ES)	8	0	1	3	1	0	0	0	0	0
400 North (NS)	0	0	0	0	0	0	0	0	0	0
400 North (SS)	0	0	0	0	0	0	0	0	0	0
300 North (NS)	19	7	6	8	7	8	8	8	8	4
300 North (SS)	14	0	2	4	2	4	5	5	5	5
100 E (NS)	8	2	7	7	3	5	4	3	4	4
100 E (SS)	40	8	18	22	13	0	17	18	14	13
Total Supply/Demand	83	18%	25%	30%	21%	0%	27%	28%	22%	21%
Average Occupancy	14									
Avg Occupancy Rate	28%									
Block 3										
300 N (SS)	24	16	17	17	17	21	20	21	22	18
Main St (NS)	14	0	3	6	3	3	6	6	5	4
200 N (NS)	6	1	1	0	0	0	0	0	0	0
100 W (ES)	7	1	5	2	3	4	4	1	3	4
100 W (NS)	12	5	7	4	3	8	6	12	11	8
Total Supply/Demand	63	23	32	29	26	34	36	40	41	35
Average Occupancy	33									
Avg Occupancy Rate	52%									
Block 4										
Main St (ES)	8	1	6	0	3	1	1	1	0	2
200 N (NS)	8	0	1	2	1	2	3	0	0	2
300 N (SS)	13	0	2	1	1	1	1	1	2	3
100 E (NS)	15	0	0	0	0	0	0	2	0	0
100 E (SS)	24	1	1	1	0	1	0	5	3	2
100 W (NS)	2	10	4	5	5	5	8	5	5	9
Total Supply/Demand	66	3%	15%	6%	8%	8%	8%	14%	8%	14%
Average Occupancy	8									
Avg Occupancy Rate	8%									
Block 5										
200 N (SS)	9	1	1	0	0	0	0	0	0	0
Main St (NS)	18	2	10	6	7	5	5	9	7	7
100 N (NS)	28	4	5	8	5	9	10	14	10	10
100 W (ES)	12	1	1	1	1	2	2	2	2	2
100 W (NS)	13	0	0	2	0	0	0	0	0	0
Total Supply/Demand	80	8	17	17	13	0	16	17	25	19
Average Occupancy	15									
Avg Occupancy Rate	18%									
Block 6										
Main St (ES)	15	5	9	8	8	9	10	9	11	8
200 N (SS)	10	6	6	5	2	4	4	2	1	6
100 E (NS)	26	4	4	4	6	1	2	3	3	8
100 E (SS)	8	3	5	1	3	5	3	4	5	3
100 N (NS)	7	2	2	2	2	3	2	1	0	4
Federal Ave (NS)	0	0	0	0	0	0	0	0	0	0
Federal Ave (SS)	37	17	16	29	29	24	26	23	21	21
Church St (ES)	24	8	15	15	13	13	11	16	11	11
Church St (NS)	6	4	4	3	5	5	5	4	5	4
Total Supply/Demand	133	48	81	85	88	84	83	82	57	63
Average Occupancy	61									
Avg Occupancy Rate	46%									

Parking Utilization rates

9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	weighted avg
0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
0%	14%	0%	0%	0%	0%	0%	0%	0%	2%
13%	7%	27%	20%	27%	33%	13%	47%	20%	23%
17%	8%	8%	17%	17%	25%	25%	17%	25%	18%
15%	15%	15%	31%	23%	15%	31%	31%	8%	21%
0%	13%	36%	13%	0%	0%	0%	0%	0%	7%
0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
37%	32%	42%	37%	0%	42%	47%	42%	21%	33%
0%	14%	26%	14%	0%	25%	36%	21%	36%	20%
25%	88%	88%	36%	0%	63%	50%	38%	50%	46%
67%	71%	71%	71%	68%	83%	88%	82%	76%	76%
0%	21%	43%	21%	21%	43%	43%	36%	26%	26%
17%	17%	0%	0%	0%	0%	0%	0%	0%	4%
14%	71%	39%	43%	57%	57%	14%	43%	57%	43%
42%	58%	33%	25%	50%	50%	100%	92%	67%	57%
17%	100%	0%	50%	17%	17%	17%	0%	33%	28%
0%	13%	25%	13%	25%	39%	0%	0%	25%	15%
0%	15%	8%	8%	8%	8%	8%	15%	23%	10%
0%	0%	0%	0%	0%	0%	13%	0%	0%	1%
4%	4%	4%	0%	4%	0%	21%	13%	8%	6%
11%	11%	0%	0%	0%	0%	0%	0%	0%	2%
11%	58%	33%	39%	0%	28%	28%	80%	36%	31%
14%	18%	29%	18%	0%	32%	36%	50%	36%	26%
0%	8%	8%	8%	0%	17%	17%	17%	17%	11%
0%	0%	15%	0%	0%	0%	0%	0%	0%	2%
33%	80%	40%	63%	80%	67%	80%	73%	53%	56%
80%	80%	50%	20%	40%	40%	25%	10%	80%	40%
10%	15%	15%	23%	4%	8%	12%	12%	22%	14%
36%	63%	19%	36%	63%	36%	50%	63%	36%	44%
29%	29%	29%	29%	43%	29%	14%	0%	57%	29%
0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
46%	43%	78%	78%	65%	70%	82%	57%	57%	82%
33%	63%	63%	54%	54%	46%	67%	46%	46%	52%
67%	67%	50%	63%	63%	63%	67%	63%	67%	72%

APPENDIX B - EXISTING ON-STREET PARKING UTILIZATION

Logan Parking Utilization Analysis

Location	Supply	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM
Block 7										
Main St (WS)	18	2	5	5	13	20	13	7	12	6
Center St (NS)	28	9	16	20	24	18	13	18	9	19
100 N (SS)	32	5	5	8	14	14	9	9	20	13
100 W (ES)	10	0	0	2	4	3	0	0	0	0
100 W (WS)	16	2	2	2	4	4	4	2	2	4
Total Supply/Demand	104	18	28	35	59	59	39	36	43	42
Utilization		17%	27%	34%	57%	57%	39%	35%	41%	40%
Average Occupancy	40									
Avg Occupancy Rate	38%									

Block 8										
Main St (ES)	15	8	9	9	5	11	10	12	13	9
100 N (SS)	27	1	2	6	9	4	3	1	1	3
Center St (NS)	28	1	0	0	1	3	2	2	1	0
100 E (ES)	13	0	1	1	0	0	3	1	1	1
100 E (WS)	17	1	1	2	4	1	3	2	1	1
Total Supply/Demand	100	9	13	20	19	19	21	18	17	14
Utilization		9%	13%	20%	19%	19%	21%	18%	17%	14%
Average Occupancy	17									
Avg Occupancy Rate	17%									

Block 9										
Main St (WS)	12	3	4	2	5	4	6	5	9	
Center St (SS)	24	10	12	16	16	9	14	15	11	
100 W (ES)	14	0	0	1	0	0	7	7	0	
100 W (WS)	15	1	1	2	3	2	4	4	1	
100 S (NS)	16	10	8	12	12	12	8	9	10	
100 S (SS)	16	4	3	5	8	7	5	9	14	
Total Supply/Demand	97	28	28	38	44	0	34	44	49	43
Utilization		29%	29%	39%	45%	0%	35%	45%	51%	48%
Average Occupancy	34									
Avg Occupancy Rate	36%									

Block 10										
Main St (ES)	8	0	0	3	2	2	4	1	4	2
Center St (NS)	14	0	2	2	3	1	3	3	1	2
100 S (NS)	10	0	1	1	2	2	2	1	2	2
100 S (SS)	14	0	2	3	2	1	1	3	4	4
100 E (ES)	12	0	0	1	0	0	0	0	0	0
100 E (WS)	13	1	1	1	0	0	1	0	0	0
Total Supply/Demand	71	1	6	11	9	6	11	8	11	10
Utilization		1%	8%	15%	13%	8%	15%	11%	15%	14%
Average Occupancy	8									
Avg Occupancy Rate	11%									

Block 11										
Main St (ES)	8	3			3				5	
500 North (SS)	12	2			0				0	
500 North (NS)	15	2			2				0	
100 East (WS)	13	0			0				0	
400 North (NS)	no parking									
Total Supply/Demand	46	7			5				5	
Utilization		15%			10%				10%	
Average Occupancy	6									
Avg Occupancy Rate	12%									

Block 12										
100 East (ES)	13	0			0				0	
500 North (SS)	3	0			0				0	
500 North (NS)	13	2			0				1	
200 East (WS)	14	0			0				0	
400 North (NS)	no parking									
Total Supply/Demand	43	2			0				1	
Utilization		5%			0%				2%	
Average Occupancy	1									
Avg Occupancy Rate	2%									

Parking Utilization rates

	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	weighted avg
11%	28%	28%	28%	72%	111%	72%	36%	87%	33%	51%
32%	57%	71%	86%	84%	48%	84%	32%	88%	58%	
10%	16%	16%	44%	44%	28%	28%	63%	41%	33%	
0%	0%	20%	40%	30%	0%	0%	0%	0%	10%	
13%	13%	13%	25%	25%	25%	13%	13%	25%	16%	

40%	60%	60%	33%	75%	57%	80%	87%	80%	62%	
4%	7%	30%	33%	15%	11%	4%	4%	11%	13%	
4%	0%	0%	4%	11%	7%	7%	4%	0%	4%	
0%	8%	8%	0%	0%	23%	8%	8%	8%	7%	
6%	6%	12%	24%	8%	18%	12%	6%	6%	10%	

25%	33%	17%	42%	0%	33%	50%	42%	75%	35%	
42%	50%	67%	67%	0%	36%	58%	63%	46%	46%	
0%	0%	7%	0%	0%	0%	50%	50%	0%	12%	
7%	7%	13%	20%	0%	13%	27%	27%	7%	13%	
83%	50%	75%	75%	0%	75%	50%	56%	83%	56%	
25%	19%	31%	50%	0%	44%	31%	56%	88%	38%	

0%	0%	38%	25%	25%	50%	13%	50%	25%	25%	
0%	14%	14%	21%	7%	21%	21%	7%	14%	13%	
0%	10%	10%	20%	20%	20%	10%	20%	20%	14%	
0%	14%	21%	14%	7%	7%	21%	29%	29%	18%	
0%	0%	8%	0%	0%	0%	0%	0%	0%	1%	
8%	8%	8%	0%	0%	8%	0%	0%	0%	3%	

38%		38%		63%	46%					
17%		0%		0%	8%					
13%		13%		0%	9%					
0%		0%		0%	0%					
0%		0%		0%	0%					

0%		0%		0%	0%					
15%		0%		0%	0%					
0%		0%		8%	8%					
0%		0%		0%	0%					
0%		0%		0%	0%					



PROJECT:

LOGAN MAIN STREET ICE SKATE AND PLAZA

ADDRESS: 55 NORTH MAIN, LOGAN, UTAH



Architecture
Architecture
Interior Design
Landscape Architecture
Land Planning
Construction Management

7727 So. Highway Parkway, Suite 303
 Sandy, Utah 84224
 ph 801 269 0355
 fax 801 269 1143
www.briarac.com

It describes the visual and descriptive layout, a step in the technical drawing, graphic representation of a model. Therefore, the architectural drawing is a visual representation of a model, a model which is a simplified version of a complex object. The architectural drawing is a visual representation of a model, a model which is a simplified version of a complex object. The architectural drawing is a visual representation of a model, a model which is a simplified version of a complex object.

LOGAN CITY MAIN STREET ICE SKATE AND PLAZA

55 North Main
Logan, Utah

PROJECT NO. 20-028

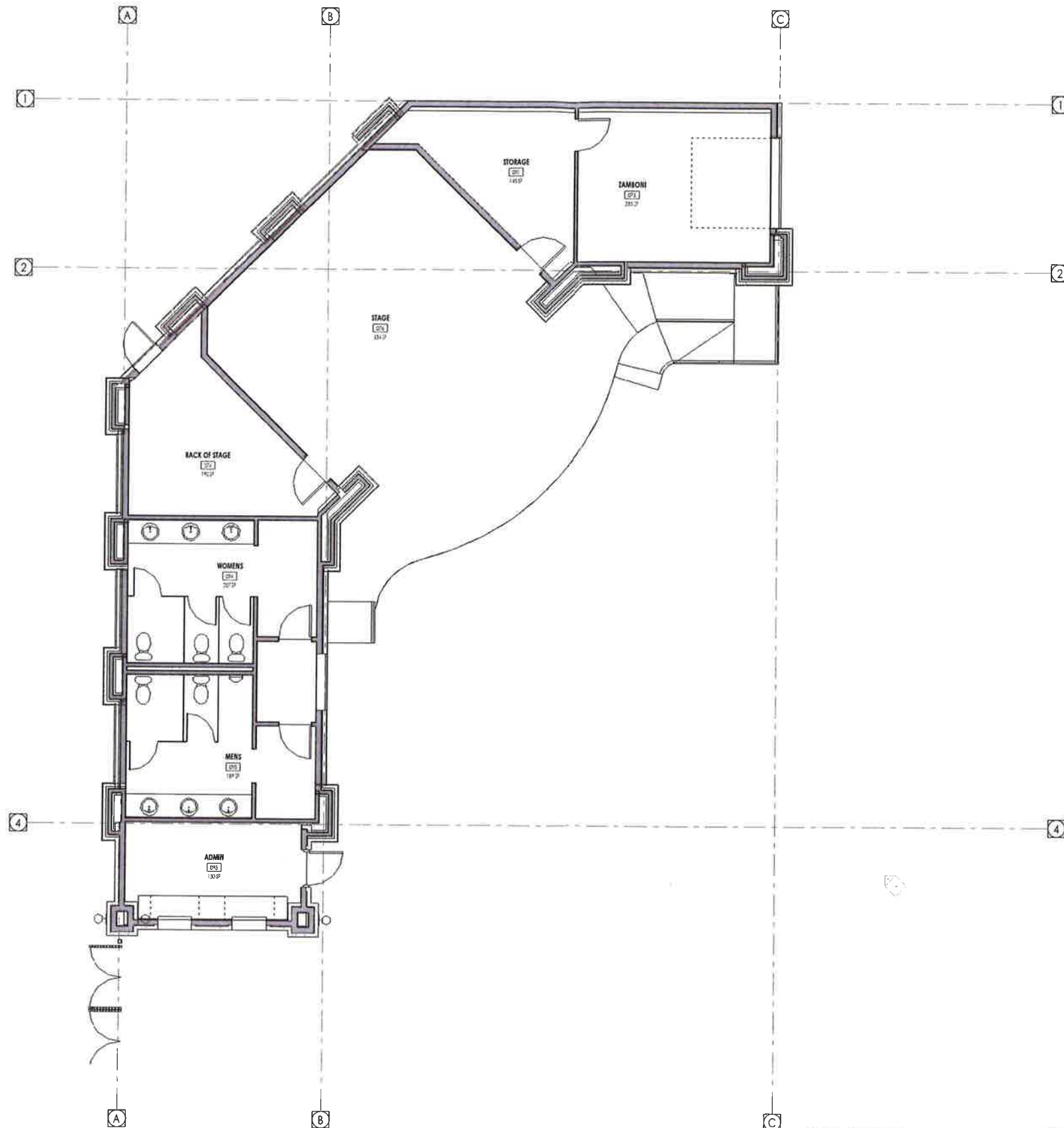
DATE: 3/5/2021

RESULTS

SHEET TITLE
COVER SHEET

DESIGN REVIEW

[illegible]



LEVEL 1 - FLOOR PLAN

FLOOR PLAN LEGEND	
HATCH PATTERN	DESCRIPTION
	ROUNDED FACE OF DOOR
	WOOD STUD WALL
	CLAY WALL
	STONE MASONRY

FLOOR PLAN MATERIAL LEGEND	
HATCH PATTERN	DESCRIPTION
	CARPET FINISH
	TILE FINISH
	EXTERIOR CONCRETE SLAB
	ASPHALT ROOFING

FLOOR PLAN GENERAL NOTES	
<p>1. ALL DIMENSIONS ARE TO INTERIOR FACE UNLESS OTHERWISE NOTED OTHERWISE.</p> <p>2. CEILING HEIGHTS ARE ASSUMED FROM PLUMB LINE OR CONCRETE - SEE SECTIONS.</p> <p>3. REFER TO ENLARGED PLANS FOR ALL UNFINISHED DIMENSIONS. INDICATE TYPES OF DOORS AND WALLS.</p> <p>4. REFER TO ENLARGED PLANS FOR ALL DETAILS.</p> <p>5. COORDINATE WITH ALL ENLARGED PLANS FOR ADDITIONAL INFORMATION AND DETAILS.</p> <p>6. ALL TOPPING SLABS MUST BE FORMED AFTER ROOF IS COMPLETE AND BUILDING IS DRYED IN.</p> <p>7. SEE SHEET "NOTES FOR PROJECT GENERAL NOTES AND SHEET" FOR PROJECT KEY NOTES. PERMIT ALL NOTES TO BE COORDINATED.</p> <p>8. COORDINATE WITH STRUCTURAL FRAMING PLAN AND SHEET WALL PLANS FOR LOCATIONS OF COLUMN, BEAM, AND WALLS ETC.</p> <p>9. COORDINATE WITH BUILDING OVERLAY FOR ALL BUILDING REQUIREMENTS.</p> <p>10. COORDINATE WITH ELECTRICAL DRAWINGS FOR ALL LIGHTING, POWER AND DATA REQUIREMENTS.</p> <p>11. ALL EXTERIOR WALLS ARE ASSUMED TO BE 24" STUD WALLS UNLESS SHOWN OTHERWISE.</p> <p>12. ALL INTERIOR WALLS ARE ASSUMED TO BE 24" STUD WALLS UNLESS SHOWN OTHERWISE.</p> <p>13. ALL ROOF THICKNESS SHALL BE 4" MINIMUM. SEE SECTION FOR ALLOW. FOR FALL DOWN SLOPE SHALL BE OVER 1:12. ALL ROOF THICKNESS SHALL BE 4" MINIMUM. SEE SECTION FOR ALLOW. FOR FALL DOWN SLOPE SHALL BE OVER 1:12.</p>	

FLOOR PLAN KEY NOTES	
	Key Note Symbol
	Key Note Symbol

KEYNOTE INTERSECTION	

SYMBOL LEGEND	
	BUILDING ELEVATION
	BUILDING GROUND TAG
	BUILDING SECTION 1
	WALL SECTION
	SPECIFICATION REFERENCE
	FUNCTIONAL REFERENCE
	DOOR PARAMETER TAG
	WINDOW PARAMETER TAG
	WALL TYPE
	DETAIL HEAD
	DETAIL TAIL
	FINISHED CEILING HEIGHT
	FINISHED FLOOR HEIGHT
	BUILDING ELEVATION
	BUILDING GROUND TAG
	BUILDING SECTION 1
	WALL SECTION
	SPECIFICATION REFERENCE
	FUNCTIONAL REFERENCE
	DOOR PARAMETER TAG
	WINDOW PARAMETER TAG
	WALL TYPE
	DETAIL HEAD
	DETAIL TAIL
	FINISHED CEILING HEIGHT
	FINISHED FLOOR HEIGHT



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7937 So. Highpoint Parkway, Suite 300
Sandy, Utah 84095
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The project is a major, nationwide effort to help children and young people understand and respond to the pressures and temptations of the world of advertising. It is a project that will help young people to become more confident and more assertive in the face of the pressures and temptations of the world of advertising. It is a project that will help young people to become more confident and more assertive in the face of the pressures and temptations of the world of advertising.

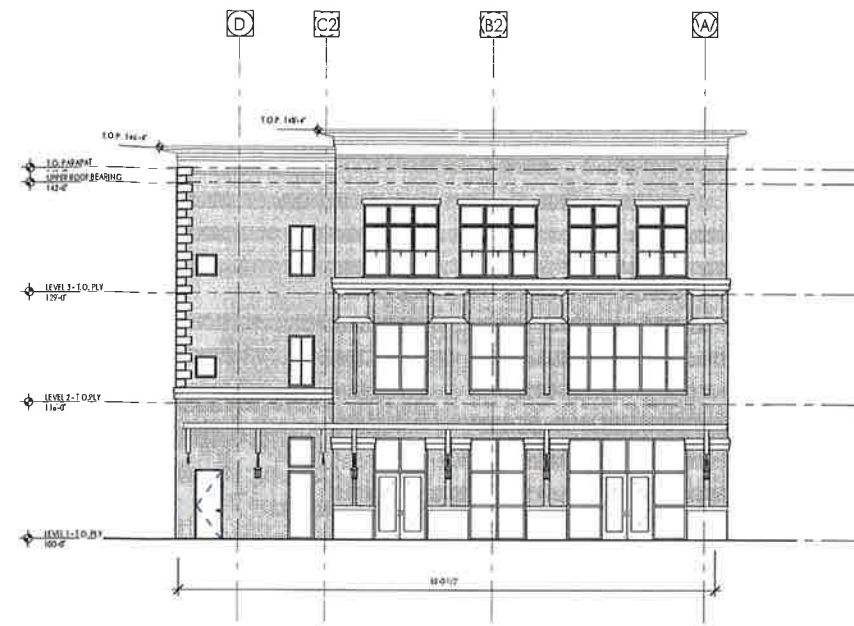
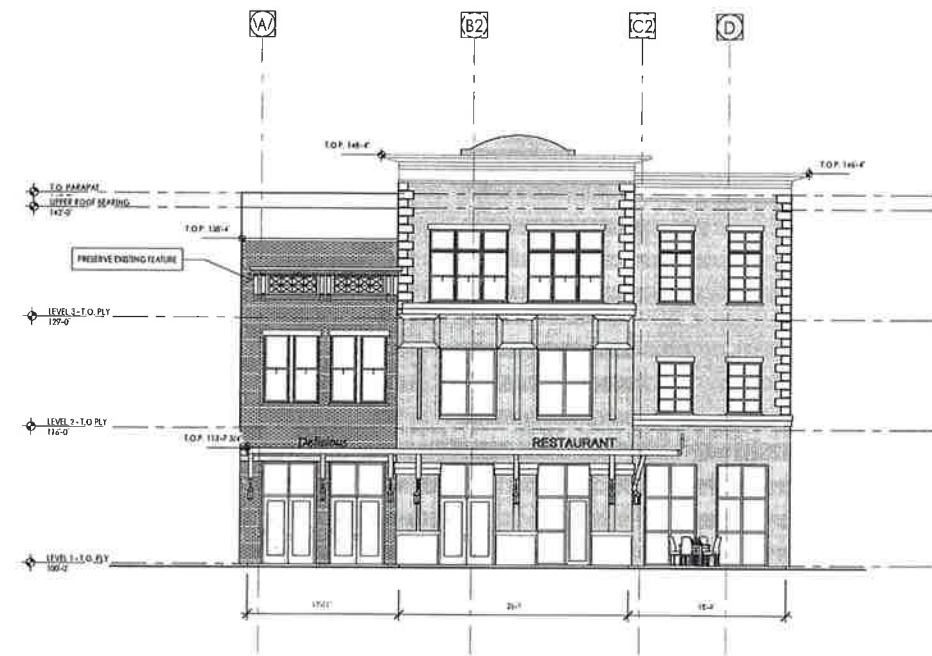
LOGAN CITY MAIN STREET ICE SKATE AND PLAZA
STAGE BUILDING
55 North Main
Logan, Utah






PROJECT NO. 20-028
DATE: 3/5/2021

85-10040

STAGE BUILDING FLOOR PLAN

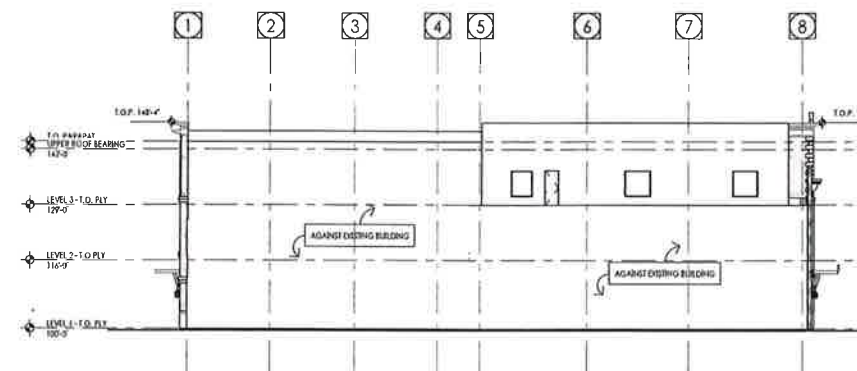
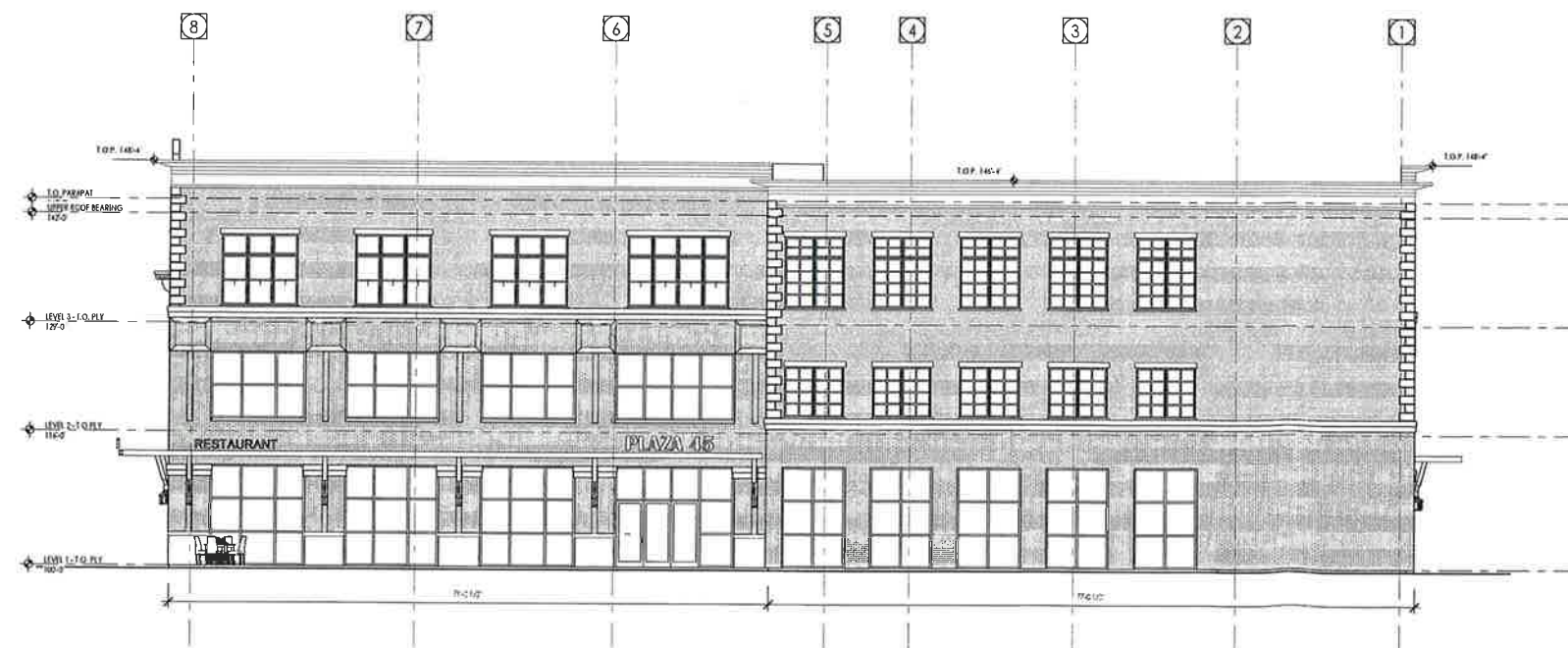
A103



ELEVATION / SECTION MATERIAL LEGEND	
MATCH PATTERNS	DESCRIPTION
	PLAIN AS-BUILT STEEL SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BY YOU TO MATCH EXISTING BUILDING
	GALVANIZED STEEL SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BY YOU TO MATCH EXISTING BUILDING
	HOT-DIPPED GALVANIZED STEEL SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BY YOU TO MATCH EXISTING BUILDING
	CAST IRON SEE SPECIFICATIONS FOR TYPE AND FINISH
	WOOD SEE SPECIFICATIONS FOR TYPE, MATERIAL, AND COLOR

NOTE: REFER TO MATERIAL SPECIFICATION DOCUMENT FOR DETAILED INFORMATION REGARDING EACH REPRESENTATION.

KEYNOTE INSTRUCTIONS



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The design of the model is consistent with the idea of a 'best practice' model, which is a model that is based on the best available evidence, and is used to guide the design of a new model. The model is based on the best available evidence, and is used to guide the design of a new model.

LOGAN CITY MAIN STREET ICE SKATE AND PLAZA
EXISTING BUILDINGS 45/41

515 North Main

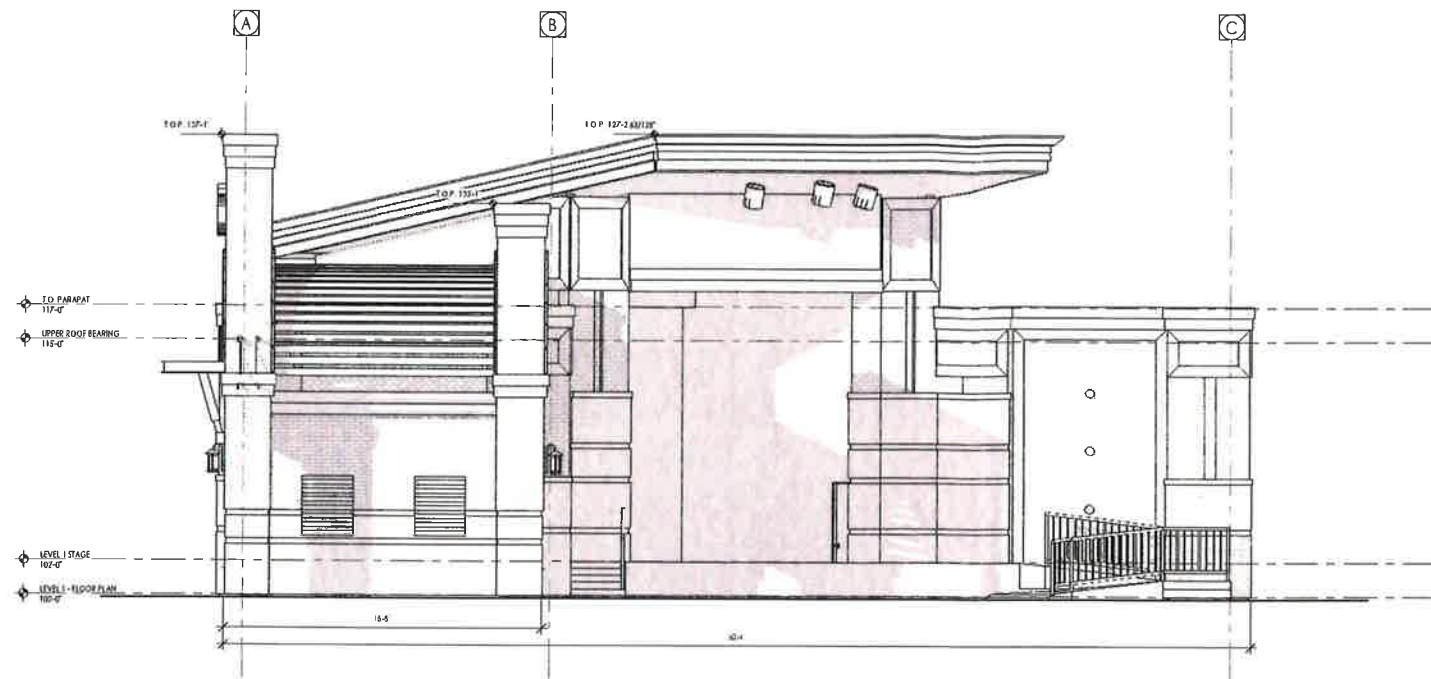
PROJECT NO. 20-01
DATE: 3/5/201

ANALYST

EXTERIOR ELEVATION

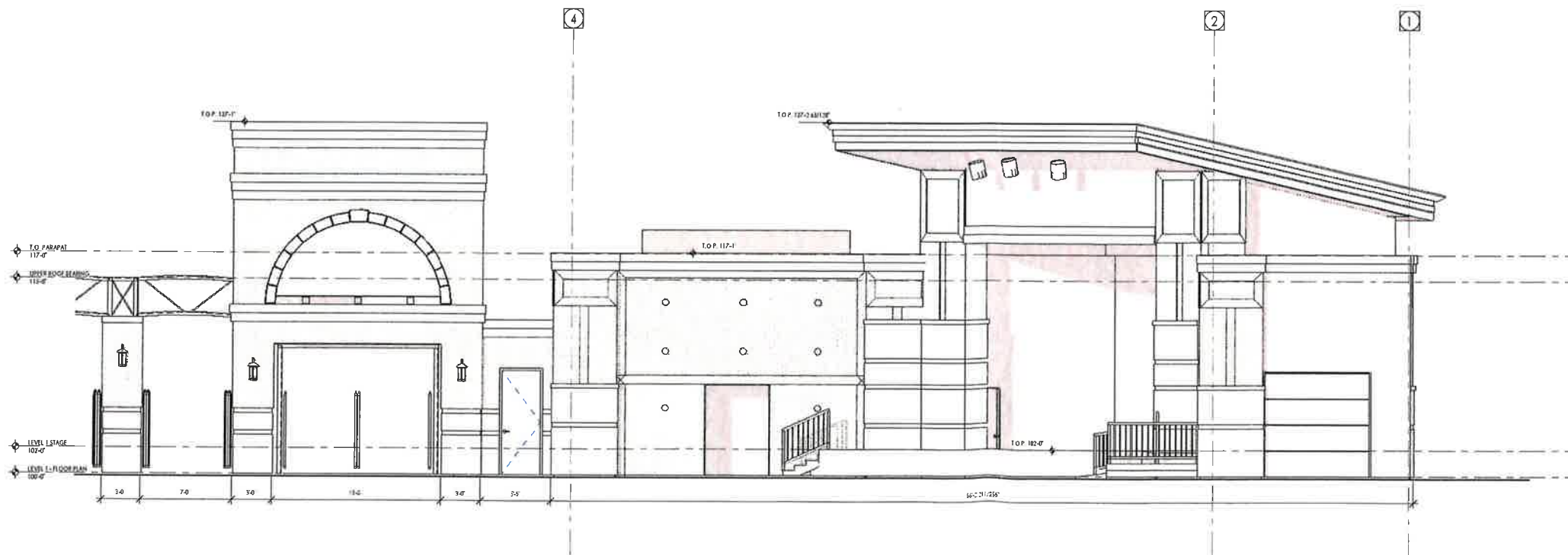
IN REVIEW SHEET NUMBER A201

DESIGN REVIEW



SOUTH ELEVATION
1/4" = 1'-0"

1
A201



EAST ELEVATION
1/4" = 1'-0"

2
A202

ELEVATION / SECTION MATERIAL LEGEND	
SYMBOL	DESCRIPTION
[Symbol]	PLATE 45 BRCK V8 REP SEE SPECIFIC MATERIAL FOR TYPE L.V. AND OTHER SELECTED BRCK TO MATCH EXISTING BUILDING
[Symbol]	MO-45 BRCK V8 REP SEE SPECIFIC MATERIAL FOR TYPE L.V. AND OTHER SELECTED BRCK TO MATCH EXISTING BUILDING
[Symbol]	HE-45 BRCK V8 REP SEE SPECIFIC MATERIAL FOR TYPE L.V. AND OTHER SELECTED BRCK TO MATCH EXISTING BUILDING
[Symbol]	CASI BRCK SEE SPECIFIC MATERIAL FOR TYPE AND COLOR
[Symbol]	ROOF TPO SEE SPECIFIC MATERIAL FOR TYPE AND COLOR
NOTE: REFER TO MATERIAL SPECIFICATIONS DOCUMENT FOR DETAILED INFORMATION ON MATERIALS AND FINISHES.	
KEYNOTE INSTRUCTIONS	



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Cottonwood, UT 84304
PH: 801.297.0265
FAX: 801.297.0265
WWW.THINKARCHITECT.COM

The design and construction of this project is the result of a collaborative effort between the client and the design team. The design team has worked closely with the client to ensure that the project meets their needs and expectations. The design team has also worked closely with the construction team to ensure that the project is built to the highest quality standards.

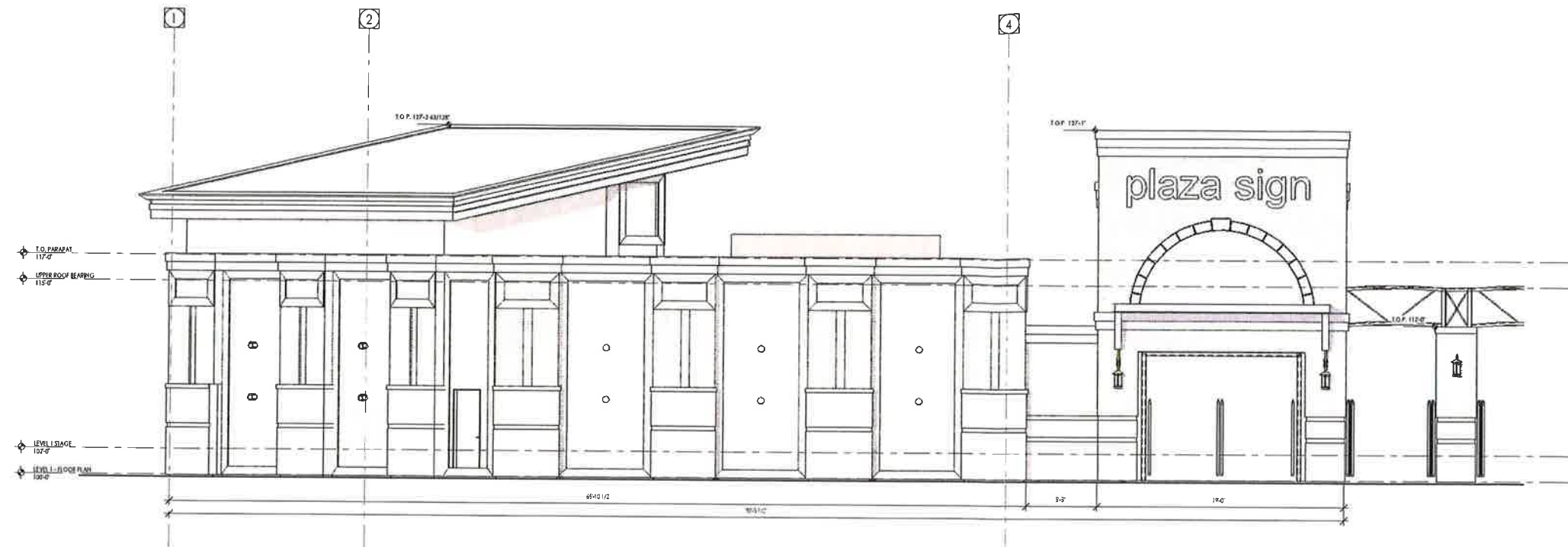
LOGAN CITY MAIN STREET ICE SKATE AND PLAZA STAGE BUILDING

55 North Main
Logan, Utah

PROJECT NO. 20-028
DATE: 3/5/2021
REVISIONS

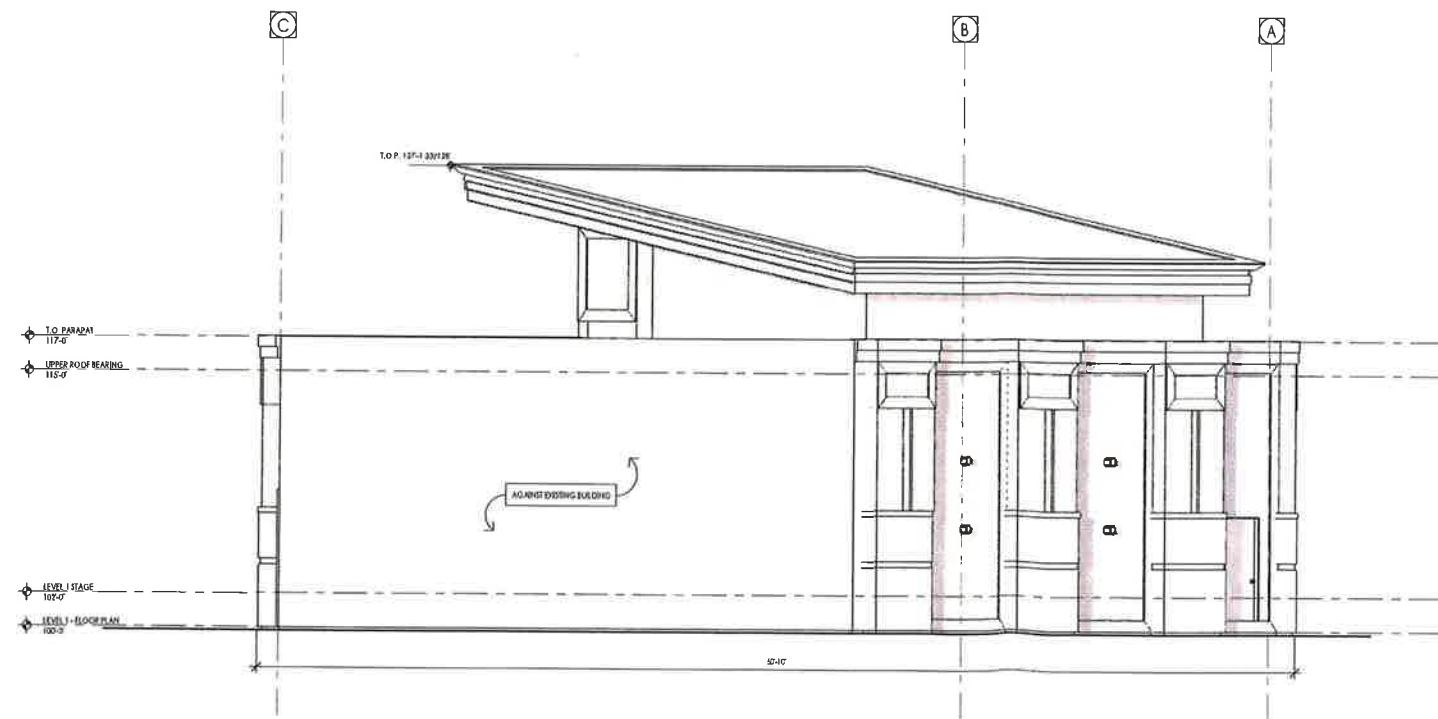
SHEET TITLE
EXTERIOR ELEVATIONS

DESIGN REVIEW
SHEET 1 OF 1
A202



WEST ELEVATION
1/8" = 1'-0"

1
A201



NORTH ELEVATION
3/4" = 1'-0"

2
A201

ELEVATION / SECTION MATERIAL LEGEND	
SWATCH FACTORY	DESCRIPTION
	PLAIN AS BRICK - VENEER SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	DARK BRICK - VENEER SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	NEW BRICK - ACCENT VENEER SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	CAST STONE SEE SPECIFICATIONS FOR TYPE AND FINISH
	ROOF TILE SEE SPECIFICATIONS FOR TYPE, PATTERN, AND COLOR
NOTE: REFER TO MATERIAL SPECIFICATION DOCUMENT IF NOT DEFINED IN PRELIMINARY REFERENCE TO EXISTING MATERIAL	
KEYNOTE INSTRUCTIONS	

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Land Planning
Construction Management

7727 So. Highways Parkway, Suite 202
Sandy, Utah 84074
PH: 801.389.0355
FX: 801.389.1423
WWW.PKTHINK.COM

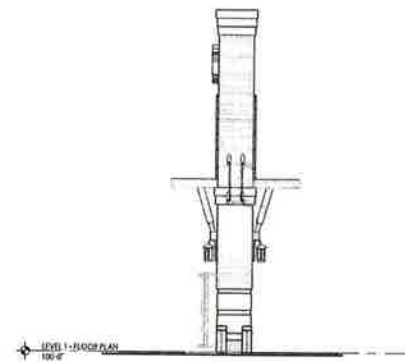
Our designs are based on the client's vision, and we work closely with the client to ensure that the design is both functional and aesthetically pleasing. We are committed to providing the highest quality of service and to ensuring that our clients are satisfied with the results of our work.

LOGAN CITY MAIN STREET ICE SKATE AND PLAZA
STAGE BUILDING
55 North Main
Logan, Utah

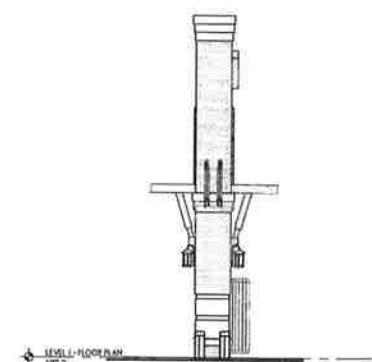
PROJECT NO. 20-028
DATE 3/5/2021
REVISED

SHEET TITLE
EXTERIOR ELEVATIONS

DESIGN REVIEW
SHEET NUMBER
A203

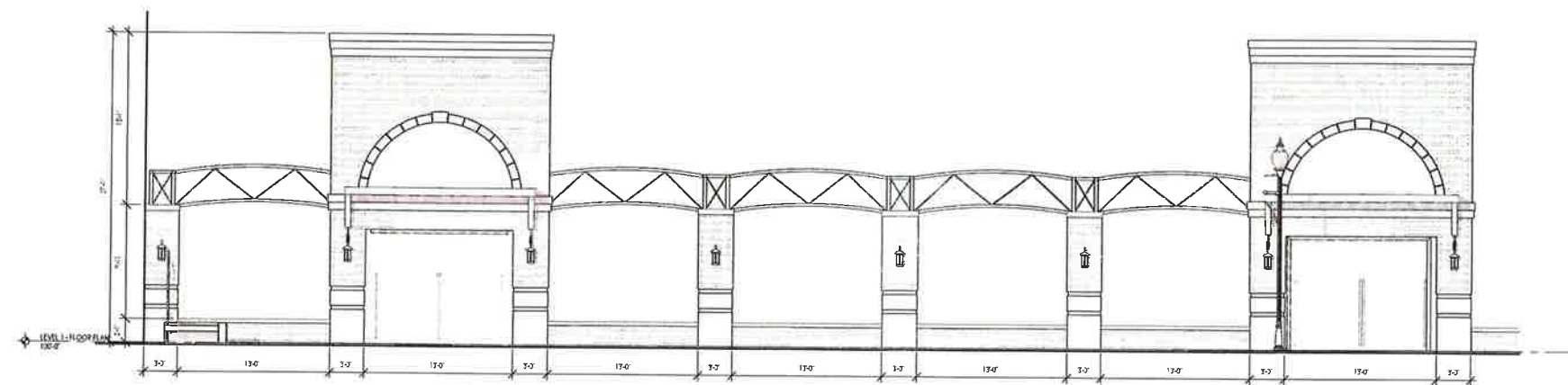


NORTH ELEVATION
3/16" = 1'-0"

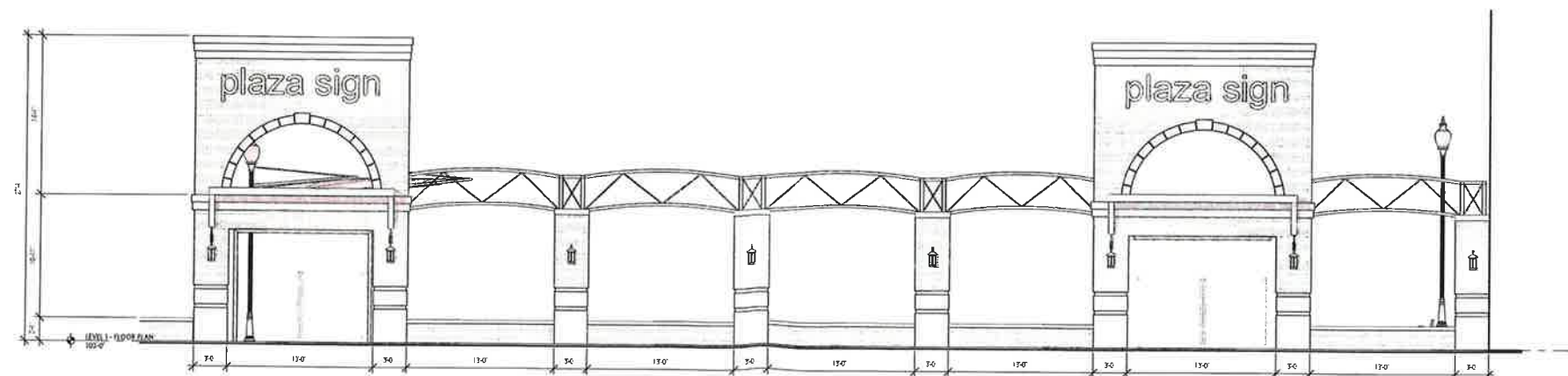


SOUTH ELEVATION
3/16" = 1'-0"

ELEVATION / SECTION MATERIAL LEGEND	
MATCH PATTERNS	DESCRIPTION
	BRICK - VENEER SEE SPECIFICATIONS FOR TYPE, LAI, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	BRICK - SOLID SEE SPECIFICATIONS FOR TYPE, LAI, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	BRICK - SOLID SEE SPECIFICATIONS FOR TYPE, LAI, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	CAST STONE SEE SPECIFICATIONS FOR TYPE AND FINISH
	ROOF PROFILE SEE SPECIFICATIONS FOR TYPE, PATTERN AND COLOR
NOTE: PREPARE MATERIAL SPECIFICATIONS DOCUMENT FOR DEVELOPER AND CONTRACTOR TO MATCH EACH MATERIAL.	
KEYNOTE INSTRUCTIONS	



EAST GATE WEST ELEVATION
3/16" = 1'-0"



EAST GATE EAST ELEVATION
3/16" = 1'-0"



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7000 So. Highland Parkway, Suite 300
Sandy, Utah 84070
ph: 801.380.0000
fax: 801.380.1111
www.thinkpk.com

The design, construction and operation of this project shall be in accordance with the applicable laws, codes, regulations, standards, and specifications of the State of Utah, the City of Logan, and the Logan City Main Street Ice Skate and Plaza Main Street Plaza Monuments. The design and construction of this project shall be in accordance with the applicable laws, codes, regulations, standards, and specifications of the State of Utah, the City of Logan, and the Logan City Main Street Ice Skate and Plaza Main Street Plaza Monuments.

LOGAN CITY MAIN STREET ICE SKATE AND PLAZA MAIN STREET PLAZA MONUMENTS

55 North Main
Logan, Utah

PROJECT NO. 20-028
DATE: 3/5/2021

REVISED

EXTERIOR ELEVATIONS

DESIGN REVIEW
A204



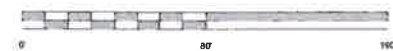
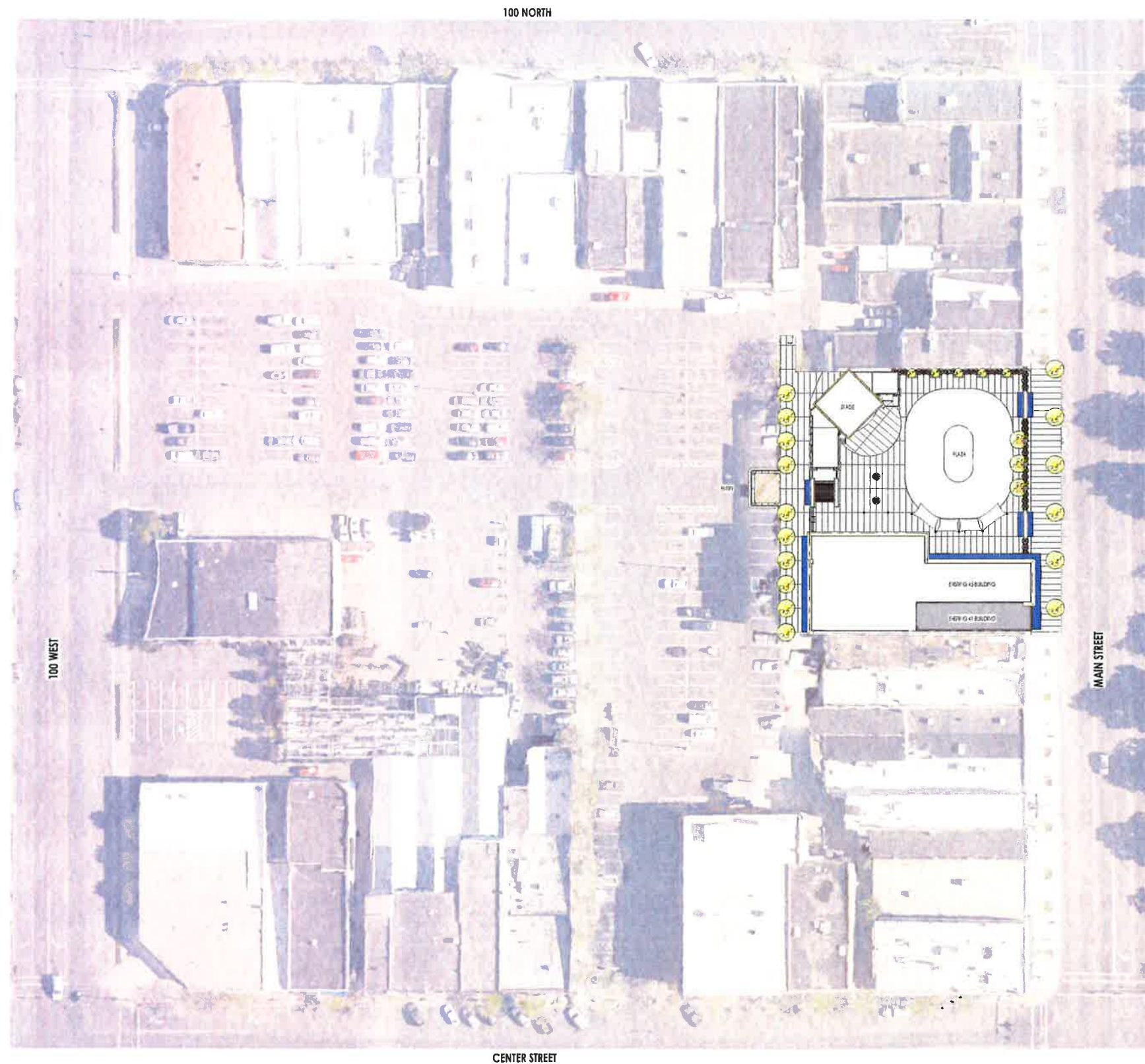
ARIAL VIEW WITH WINTER ICE RINK



ARIAL VIEW WITH SUMMER CONCERT



ARIAL VIEW WITH WINTER ICE RINK



OVERALL SITE
1" = 30'



Architecture
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PH: 801.289.0055
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WWW.THINKARCH.COM

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LOGAN CITY MAIN STREET ICE SKATE AND PLAZA

55 North Main
Logan, Utah

PROJECT NO. 20-028
DATE: 3/5/2021

REVISION:

DESIGN REVIEW	0000
SHEET TITLE	OVERALL SITE
SHEET NUMBER	



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Architecture
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Landscape Architecture
Land Planning
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FAX: 801.339.1425
www.thinkdk.com

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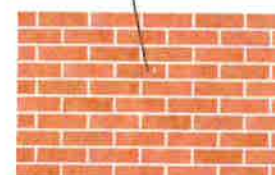
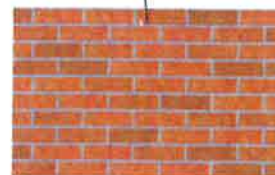
LOGAN CITY MAIN STREET ICE SKATE AND PLAZA

55 North Main
Logan, Utah

PROJECT NO. 20-028
DATE: 3/5/2021
REVISIONS:

SHEET 001 OF 001
EXTERIOR VIEWS

DESIGN REVIEW
SHEET 001 OF 001
D201



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Think Architecture is a full-service architectural firm providing a wide range of services to its clients. Our services include architecture, interior design, landscape architecture, land planning, and construction management. We are committed to providing high-quality, cost-effective solutions for our clients.

LOGAN CITY MAIN STREET ICE SKATE AND PLAZA

55 North Main
Logan, Utah

PROJECT NO. 20-028
DATE: 3/5/2021

REVISIONS

SHEET TITLE
EXTERIOR RENDERINGS
& MATERIALS

DESIGN REVIEW

D202



Architecture
Interior Design
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LOGAN CITY MAIN STREET ICE SKATE AND PLAZA

55 North Main
Logan, Utah

PROJECT NO. 20-028
DATE: 3/5/2021

REVISIONS

SHEET TITLE
EXTERIOR RENDERINGS

DESIGN REVIEW

D204






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LOGAN CITY MAIN STREET ICE SKATE AND PLAZA

55 North Main
Logan, Utah

PROJECT NO. 20-028
DATE: 3/5/2021
REVISION:

SHEET TITLE:
EXTERIOR RENDERINGS

DESIGN REVIEW
SHEET NUMBER
D205

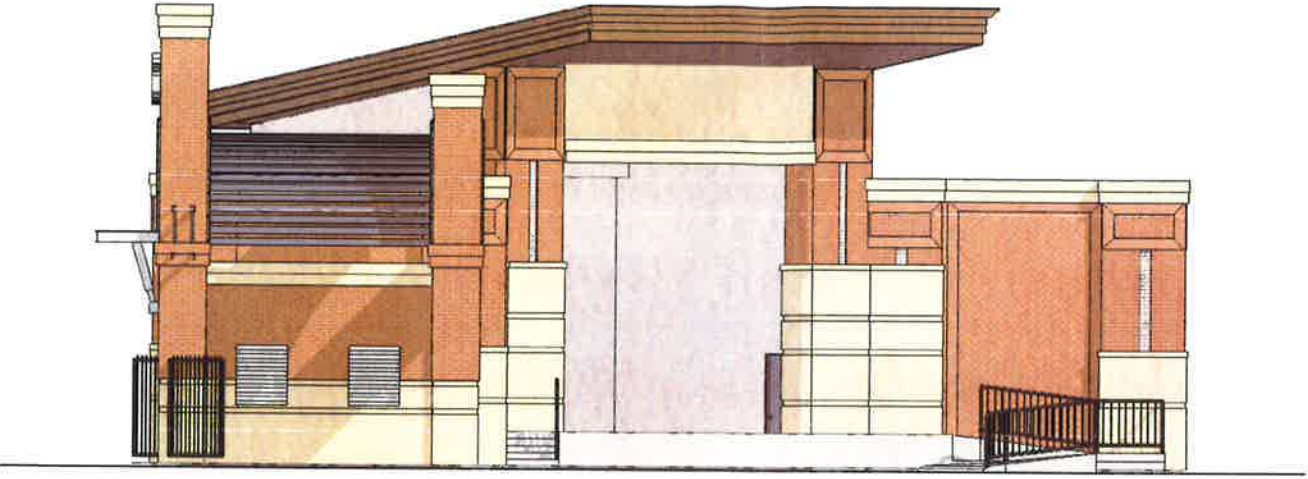
ELEVATION / SECTION MATERIAL LEGEND	
MATCH MATERIAL	DESCRIPTION
	FLYASH BRICK - VENEER SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	KNOWE BRICK - VENEER SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	PINK BRICK - ACCENT VENEER SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	CALPINE SEE SPECIFICATIONS FOR TYPE AND FINISH
NOTE: PREPARE MATERIAL SPECIFICATIONS DOCUMENT FOR THE FIELD FOR FORWARD REGARDING EACH FINISH MATERIAL	



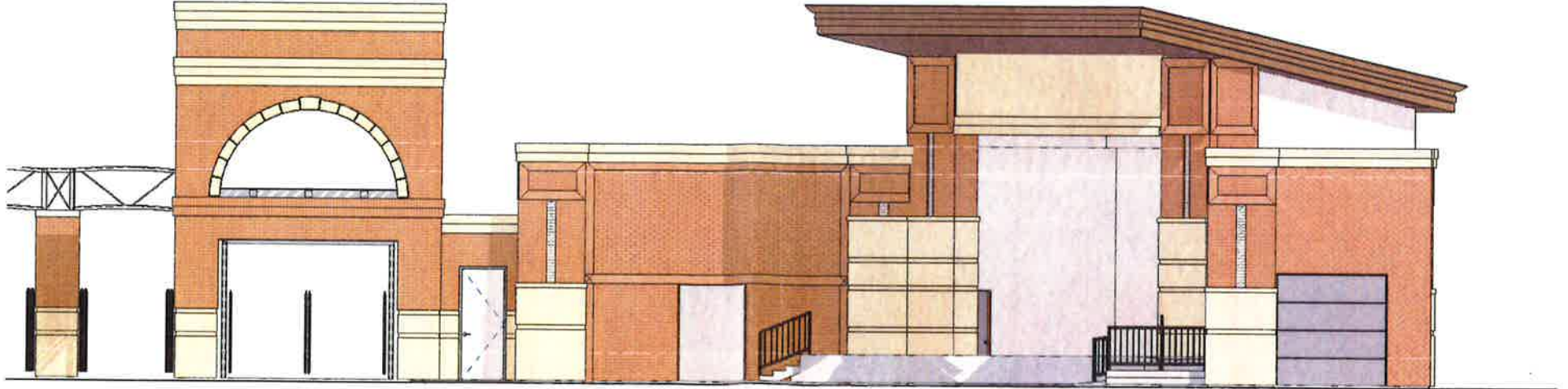
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SOUTH PRESENTATION
 1/4" = 1'-0"







EAST PRESENTATION
 1/4" = 1'-0"

LOGAN CITY MAIN STREET ICE SKATE AND PLAZA
 STAGE BUILDING
 55 North Main
 Logan, Utah

PROJECT NO. 20-028
 DATE 3/5/2021

SHEET NO.
 STAGE BUILDING
 ELEVATIONS
 SHEET NUMBER
 D206

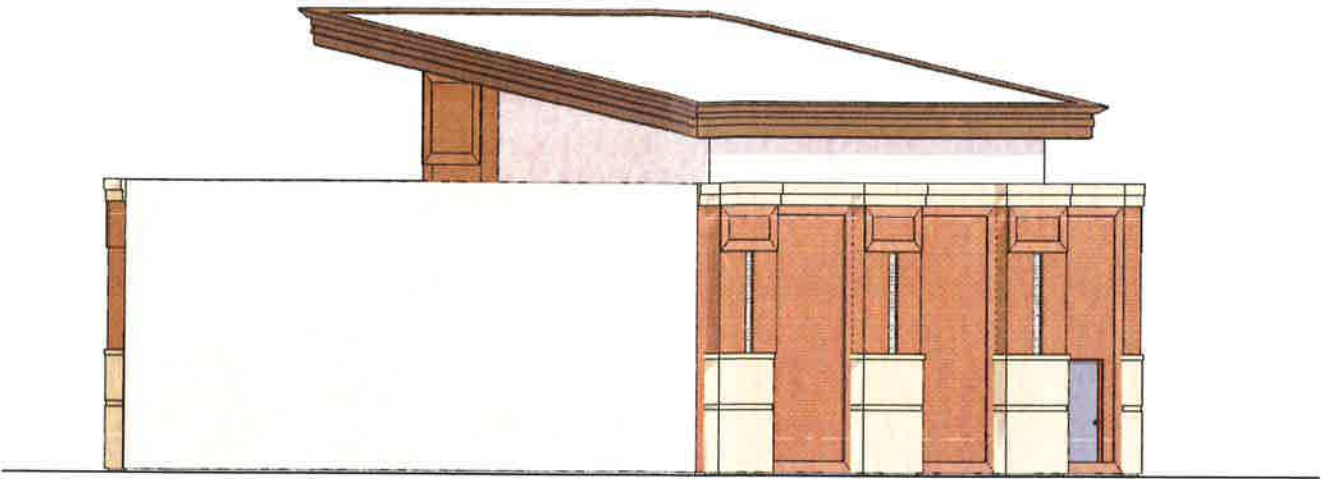
ELEVATION / SECTION MATERIAL LEGEND	
MATCH PATTERN	DESCRIPTION
	PLAZA 44 BRICK V&H SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	4000 BRICK V&H SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	1000 BRICK V&H SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	C&T STONE SEE SPECIFICATIONS FOR TYPE AND FINISH
NOTE: REFER TO MATERIAL SPECIFICATION DOCUMENT FOR COORDINATION REGARDING EACH FINISH MATERIAL	



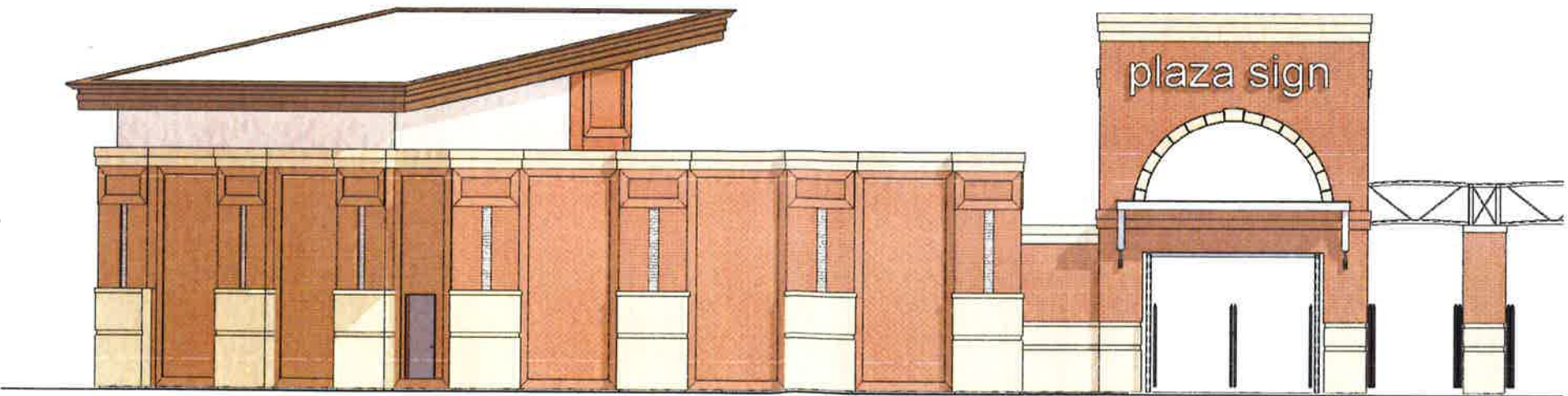
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NORTH PRESENTATION
1/4" = 1'-0"



WEST PRESENTATION
1/4" = 1'-0"



LOGAN CITY MAIN STREET ICE SKATE AND PLAZA
STAGE BUILDING
55 North Main
Logan, Utah

PROJECT NO. 20-028
DATE: 3/5/2021

REVISION:

SHEET TITLE
STAGE BUILDING
ELEVATIONS

DESIGN REVIEW
D207

ELEVATION / SECTION MATERIAL LEGEND	
MATCH MATERIAL	DESCRIPTION
	PLAZA 45 BRICK - 18" DEEP SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	45/41 BRICK - 18" DEEP SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	18" BRICK - 18" DEEP SEE SPECIFICATIONS FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	CAST STONE SEE SPECIFICATIONS FOR TYPE AND FINISH
NOTE: REFER TO MATERIAL SPECIFICATIONS DOCUMENT FOR DETAILED INFORMATION REGARDING EACH FINISH MATERIAL.	



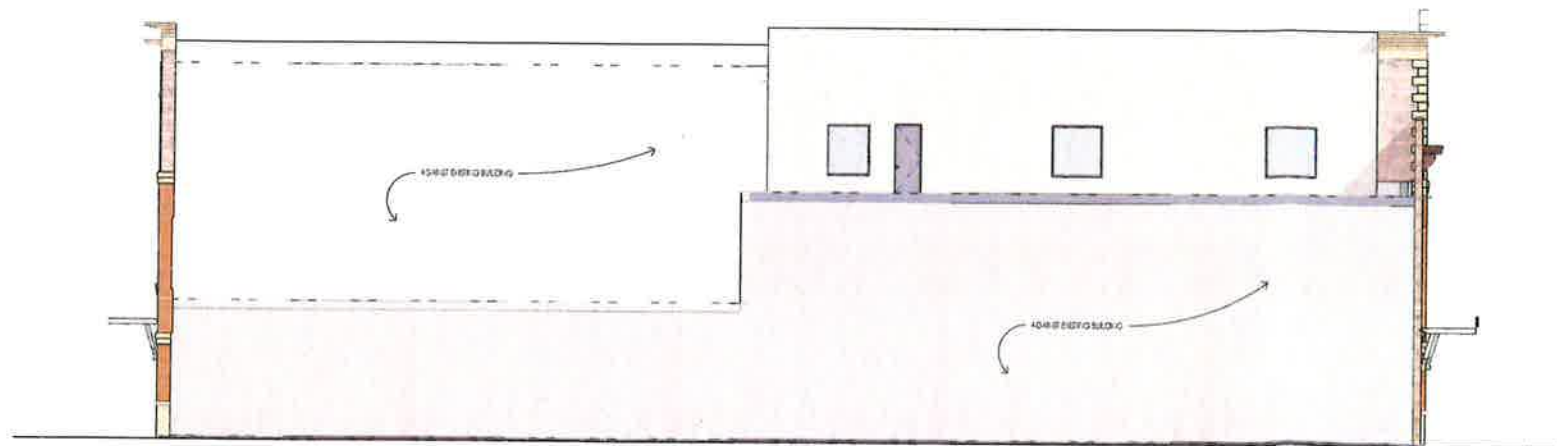
45 / 41 NORTH ELEVATION
1/8" = 1'-0"

2
028



45 / 41 EAST ELEVATION
1/8" = 1'-0"

1
028



45 / 41 SOUTH ELEVATION
1/8" = 1'-0"

3
028



45 / 41 WEST ELEVATION
1/8" = 1'-0"

4
028

LOGAN CITY MAIN STREET ICE SKATE AND PLAZA
EXISTING BUILDINGS 45/41

55 North Main
Logan, Utah

PROJECT NO. 20-028
DATE: 3/5/2021
REVISION:

SHEET TITLE
PLAZA 45 / 41
BUILDINGS

SHEET NO. / REVISION
D208

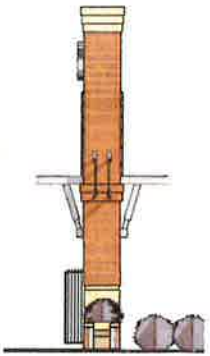
DESIGN REVIEW

ELEVATION / SECTION MATERIAL LEGEND	
MATCH PATTERNS	DESCRIPTION
	PLAZA BRICK VENEER SEE SPECIFICATION FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	ACACIA BRICK VENEER SEE SPECIFICATION FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	ACACIA BRICK VENEER SEE SPECIFICATION FOR TYPE, LAY, AND FINISH SELECTED BRICK TO MATCH EXISTING BUILDING
	CAST STONE SEE SPECIFICATION FOR TYPE AND FINISH
NOTE: REFER TO MATERIAL SPECIFICATIONS DOCUMENT FOR DETAILED INFORMATION REGARDING FINISHES AND MATERIALS.	

Think
Architecture
 Architecture
 Interior Design
 Landscape Architecture
 Land Planning
 Construction Management

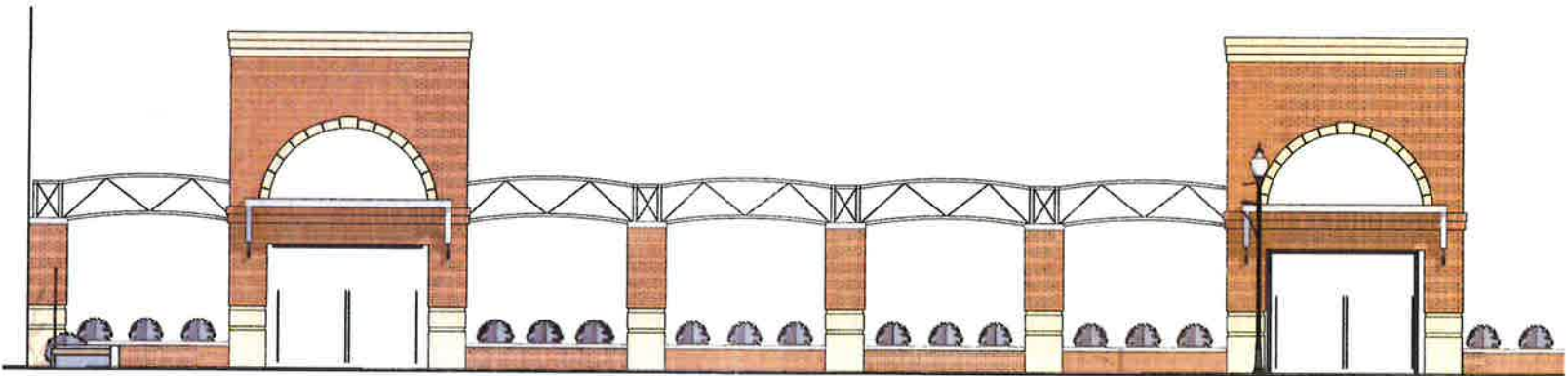
707 So. Highland Parkway, Suite 303
 Ogden, UT 84403
 801.225.2555
 801.225.2555
 www.thinkarc.com

The project is a conceptual design for a new building. The design is based on the client's requirements and the site conditions. The design is a conceptual design and is not intended to be a final design. The design is a conceptual design and is not intended to be a final design. The design is a conceptual design and is not intended to be a final design.



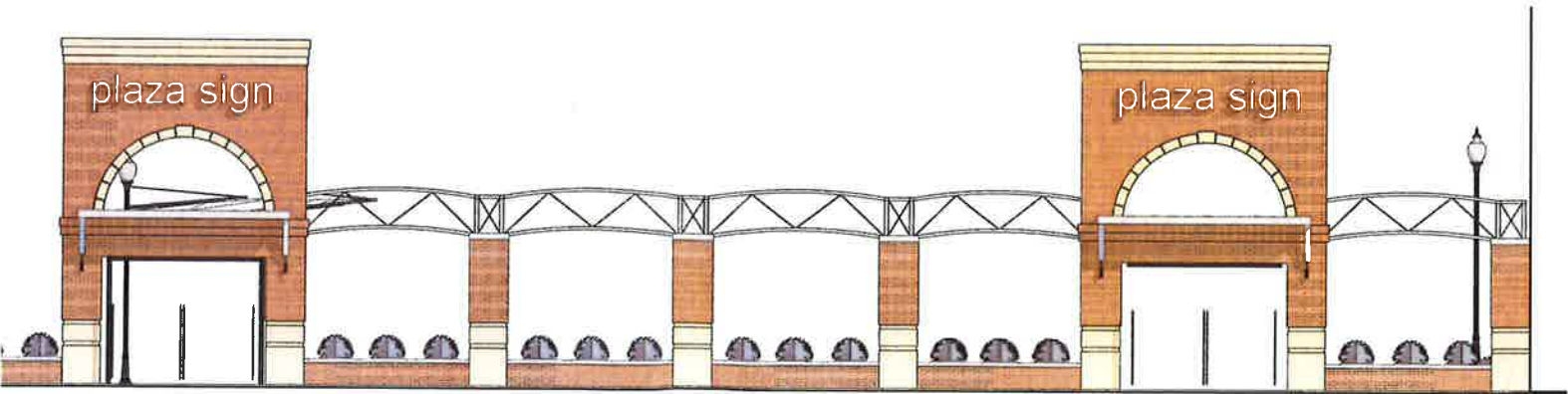
NORTH PRESENTATION

3



WEST PRESENTATION

2



EAST PRESENTATION

1

LOGAN CITY MAIN STREET ICE SKATE AND PLAZA
 MAIN STREET PLAZA MONUMENTS

55 North Main
 Logan, Utah

PROJECT NO. 20-028
 DATE: 3/5/2021
 REVISION:

SHEET TITLE
 MONUMENT
 ELEVATIONS

SHEET NUMBER
 D209

DESIGN REVIEW

EXST. BUILDING

ZAMBONI

STAGE

WOMEN'S

MEN'S

WEST GATE

FIRE PIT

FIRE PIT

SPLASH PAD SUMMER USE

ICE SKATING RINK (WINTER USE)
5,600 Sq Ft (73' x 87')

FIRE PIT

FIRE PIT

NE. GATE

SE. GATE

ELEV.

EXIST. BUILDING

EXISTING PARKING

EXISTING PARKING

ED SLE

0' 10' 20' 40' 80'

NORTH



27 S.O. HIGHPOINT PARKWAY
SUITE #300
SANDY, UTAH 84094
ph. 801.269.0055
www.thinkaec.com

These designs are available for limited review and evaluation by selected consultants, architects, engineers, government and agencies, individuals and office personnel only in accordance with the following:

55 North Main
Logan, Utah

REVISIONS:

D001

DESIGN REVIEW



ALTA / NSPS LAND TITLE SURVEY

SURVEYOR'S CERTIFICATE

TO LOGAN CITY, NORTHERN TITLE COMPANY, AND FIRST AMERICAN TITLE INSURANCE COMPANY:

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 4, 7a, 7b, 7c, 7d, 8, 9, 10b, 11, 18, AND 20 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON JANUARY 8, 2021.

DATE OF PLAT OR MAP: 1/14/21



LEGAL DESCRIPTIONS

06-025-0009 (47 NORTH MAIN ST.)
PART OF THE SOUTHEAST QUARTER OF SECTION 33, TOWNSHIP 12 NORTH, RANGE 1 EAST, SALT LAKE MERIDIAN, DESCRIBED AS FOLLOWS:
BEGINNING AT THE NORTHEAST CORNER OF LOT 8, BLOCK 14, PLAT "A", LOGAN CITY SURVEY;
THENCE S12°55'4"W 20.00 FEET;
THENCE N88°38'14"W 69.50 FEET;
THENCE N12°55'4"E 20.00 FEET;
THENCE S88°38'14"E 69.50 FEET TO THE POINT OF BEGINNING.

06-025-0011 (41 NORTH MAIN ST.)
PART OF THE SOUTHEAST QUARTER OF SECTION 33, TOWNSHIP 12 NORTH, RANGE 1 EAST, SALT LAKE MERIDIAN, DESCRIBED AS FOLLOWS:
BEGINNING AT A POINT S12°55'4"W 20.00 FEET FROM THE NORTHEAST CORNER OF LOT 8, BLOCK 14, PLAT "A", LOGAN CITY SURVEY;
THENCE S12°55'4"W 45.00 FEET;
THENCE N88°38'14"W 148.50 FEET;
THENCE N12°55'4"E 64.24 FEET (65.00 FEET BY RECORD);
THENCE S88°38'14"E 79.00 FEET;
THENCE S12°55'4"W 20.00 FEET;
THENCE S88°38'14"E 69.50 FEET TO THE POINT OF BEGINNING.

06-028-0015 (67 NORTH MAIN ST.)
PART OF THE SOUTHEAST QUARTER OF SECTION 33, TOWNSHIP 12 NORTH, RANGE 1 EAST, SALT LAKE MERIDIAN, DESCRIBED AS FOLLOWS:
BEGINNING AT A POINT N12°55'4"E 84.00 FEET FROM THE SOUTHEAST CORNER OF LOT 7, BLOCK 14, PLAT "A", LOGAN CITY SURVEY;
THENCE N88°38'14"W 148.50 FEET;
THENCE NORTHERLY 11.02 FEET (11 FEET BY RECORD) ALONG A CURVE CONCAVE TO THE EAST WITH A RADIUS OF 23.80 FEET AND A LONG CHORD BEARING N14°41'32"E 10.92 FEET;
THENCE NORTHEASTERLY 15.65 FEET ALONG A CURVE CONCAVE TO THE NORTHWEST WITH A RADIUS OF 133.00 FEET (134.5 FEET BY RECORD) AND A LONG CHORD BEARING N24°34'53"E 15.64 FEET;
THENCE S88°38'14"E 139.85 FEET;
THENCE S12°55'4"W 25.00 FEET TO THE POINT OF BEGINNING.

06-028-0016 (55 NORTH MAIN ST.)
PART OF THE SOUTHEAST QUARTER OF SECTION 33, TOWNSHIP 12 NORTH, RANGE 1 EAST, SALT LAKE MERIDIAN, DESCRIBED AS FOLLOWS:
BEGINNING AT THE SOUTHEAST CORNER OF LOT 7, BLOCK 14, PLAT "A", LOGAN CITY SURVEY;
THENCE N88°38'14"W 148.50 FEET;
THENCE N12°55'4"E 84.00 FEET;
THENCE S88°38'14"E 148.50 FEET;
THENCE S12°55'4"W 84.00 FEET TO THE POINT OF BEGINNING.

SURVEY NARRATIVE

THIS SURVEY WAS ORDERED BY KIRK JENSEN FOR THE PURPOSE LOCATING THE BOUNDARIES OF THE SUBJECT PARCELS AND TO INVESTIGATE POTENTIAL ENCROACHMENTS. LOGAN CITY OFFSET STREET MONUMENTS WERE USED TO RE-ESTABLISH MAIN STREET AND 100 NORTH STREET. POSSESSION LINES OVER A BROAD AREA WERE USED TO DETERMINE BOUNDARIES BETWEEN ADJOINING PARCELS.

NO MAJOR DISCREPANCIES WERE FOUND.

LEAD EXPANSION NAILS WITH WASHERS STAMPED "S.C. EARL PLS 318575" WERE SET AT THE CORNERS INDICATED HEREON, EXCEPTIONS NOTED.

GENERAL NOTES

- NO PARKING STALLS ON SUBJECT PROPERTIES.
- THERE IS ONE BUILDING ON EACH PARCEL AS SHOWN.
- THERE IS NO EVIDENCE OF RECENT EARTH MOVING WORK, BUILDING CONSTRUCTION, OR BUILDING ADDITIONS OBSERVED.
- THERE IS AT LEAST ONE PARTY WALL BETWEEN BUILDINGS AS LABELED (EXCEPTIONS: 19 AND 20.) IT IS SUSPECTED THAT THERE MAY BE OTHER PARTY WALLS BETWEEN THE SUBJECT PARCELS OWNED BY LOGAN CITY, ESPECIALLY AT 45 N. MAIN.

LEGEND

- LOGAN CITY GIS MONUMENT
- FOUND SURVEY MONUMENT
- SET SURVEY MONUMENT
- FIRE HYDRANT
- SANITARY SEWER MANHOLE
- STORM DRAIN MANHOLE
- WATER VALVE
- WATER METER
- STORM DRAIN BOX
- UTILITY POLE
- TRASH RECEPTACLE
- SURVEY CONTROL LINE
- BOUNDARY
- PUBLIC RIGHT-OF-WAY
- STREET CENTERLINE
- EASEMENT LINE
- MINIMUM BUILDING SETBACK
- PARCEL LINE
- EDGE OF ASPHALT
- FENCE
- ASPHALT
- CONCRETE
- BUILDING
- LANDSCAPE AREA
- OVERHEAD CANOPY
- WATER PIPELINE
- SEWER PIPELINE
- SD - STORM DRAIN PIPELINE
- GAS - NATURAL GAS
- CON - BURIED POWER



VICINITY MAP

CURVE TABLE

CURVE	LENGTH	RADIUS	Δ	CH. BEARING	CHORD
C1	11.02'	23.80'	26°31'16"	N14°41'32"E	10.92'
C2	15.65'	133.00'	6°44'34"	N24°34'53"E	15.64'

SE4 S33 T12N R1E SLM

EMPORIUM & PLAZA 45
PARCELS 06-025-0009, -0011, 06-028-0015, -0016
LOGAN, UT 84321

ALTA / NSPS SURVEY
FOR
LOGAN CITY



Cache & Landmark
Engineers
Surveyors
Planners

95 Golf Course Rd.
Suite 101
Logan, UT 84321
435.713.0099

DATE:
14 JANUARY 2021

SCALE:
1" = 20'

CALCULATED BY:
S. EARL

CHECKED BY:
S. CROOKSTON

APPROVED BY:
S. EARL

PROJECT NUMBER:
620-2003

ALTA / NSPS LAND TITLE SURVEY



TITLE EXCEPTIONS

PARCELS 06-025-0011, 06-028-0015, 06-028-0016
TITLE INSURANCE COMMITMENT NUMBER: NT-77216, COMMITMENT DATE: APRIL 8, 2016 AT 7:00AM
SCHEDULE B, SECTION 2
THE FOLLOWING IS A LIST OF TITLE POLICY EXCEPTIONS PERTINENT TO THIS SURVEY AS THEY
APPEAR IN SCHEDULE B - SECTION 2 OF THE ABOVE-REFERENCED TITLE COMMITMENT.

THE FOLLOWING AFFECT PARCEL 06-025-0011

EXCEPTION 3. REDEVELOPMENT PLAN, VIA ENTRY 467887, RECORDED IN BOOK 332 PAGE 491 ON
APRIL 2, 1984.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 4. RESOLUTION, VIA ENTRY 553645, RECORDED IN BOOK 514 PAGE 313 ON JANUARY 24,
1992.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 6. CERTIFICATE OF DECISION, VIA ENTRY 653376, RECORDED IN BOOK 731 PAGE 874 ON
JANUARY 7, 1997.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 7. MEMORANDUM OF AGREEMENT, VIA ENTRY 764976, RECORDED IN BOOK 1022 PAGE
932 ON JULY 11, 2001.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 8. MEMORANDUM OF AGREEMENT TO COMMUNICATIONS SITE LEASE AGREEMENT, VIA
ENTRY 961430, RECORDED IN BOOK 1495 PAGE 1616 ON DECEMBER 24, 2007.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 9. CERTIFICATE OF DECISION, VIA ENTRY 774521, RECORDED IN BOOK 1056 PAGE 243 ON
NOVEMBER 16, 2001.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 10. DOWNTOWN REDEVELOPMENT PROJECT, VIA ENTRY 845076, RECORDED IN BOOK 1268
PAGE 1647 ON OCTOBER 29, 2003.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 11. ADOPTION OF AMENDED DOWNTOWN REDEVELOPMENT PROJECT AREA, VIA ENTRY
988472, RECORDED IN BOOK 1550 PAGE 1362 ON JANUARY 27, 2009.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 12. FINANCE STATEMENT, VIA ENTRY 881076, RECORDED IN BOOK 1336 PAGE 213 ON
JANUARY 11, 2005.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 13. CONTINUATION OF FINANCE STATEMENT, VIA ENTRY 1002710, RECORDED IN BOOK
1583 PAGE 237 ON JULY 16, 2008.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 14. LESSOR'S AGREEMENT, VIA ENTRY 891180, RECORDED IN BOOK 1354 PAGE 840 ON
MAY 26, 2005.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 15. DEED OF TRUST, VIA ENTRY 981021, RECORDED IN BOOK 1535 PAGE 1781 ON
SEPTEMBER 25, 2006.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 16. DEED OF TRUST, VIA ENTRY 660708, RECORDED IN BOOK 749 PAGE 605 ON MAY 19,
1997.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

SUBORDINATION AGREEMENT, VIA ENTRY 981022, RECORDED IN BOOK 1535 PAGE 1794 ON SEPTEMBER
25, 2006.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

THE FOLLOWING AFFECT PARCEL 06-028-0016

EXCEPTION 18. PARTY WALL AGREEMENT, VIA ENTRY 194756, RECORDED IN BOOK 16 PAGE 389 ON
APRIL 14, 1942.
SURVEY FINDINGS: AFFECTS WALL, STAIRWAY AND FLUES BETWEEN 06-025-0009 AND 06-028-0016.
PARAPET WALL ALONG SOUTH AND WEST SIDE OF STAIRWAY AS PLATTED HEREON.

EXCEPTION 20. PARTY WALL AGREEMENT, VIA ENTRY 194757, RECORDED IN BOOK 16 PAGE 391 ON
APRIL 14, 1942.
SURVEY FINDINGS: AFFECTS WALL BETWEEN 06-025-0011 AND 06-028-0016. AS PLATTED.

EXCEPTION 21. REDEVELOPMENT PLAN, VIA ENTRY 467887, RECORDED IN BOOK 332 PAGE 491 ON
APRIL 2, 1984.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 22. RESOLUTION, VIA ENTRY 553645, RECORDED IN BOOK 514 PAGE 313 ON JANUARY
24, 1992.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 23. DEED OF TRUST, VIA ENTRY 660708, RECORDED IN BOOK 749 PAGE 605 ON MAY 19,
1997.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 24. MEMORANDUM OF AGREEMENT, VIA ENTRY 764976, RECORDED IN BOOK 1022 PAGE
932 ON JULY 11, 2001.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 25. MEMORANDUM OF AGREEMENT TO COMMUNICATIONS SITE LEASE AGREEMENT, VIA
ENTRY 961430, RECORDED IN BOOK 1495 PAGE 1616 ON DECEMBER 24, 2007.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 26. DOWNTOWN REDEVELOPMENT PROJECT, VIA ENTRY 845076, RECORDED IN BOOK 1268
PAGE 1647 ON OCTOBER 29, 2003.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 27. ADOPTION OF AMENDED DOWNTOWN REDEVELOPMENT PROJECT AREA, VIA ENTRY
988472, RECORDED IN BOOK 1550 PAGE 1362 ON JANUARY 27, 2009.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 28. CERTIFICATE OF APPROPRIATENESS, VIA ENTRY 957548, RECORDED IN BOOK 1488
PAGE 248 ON OCTOBER 25, 2007.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

THE FOLLOWING AFFECT PARCELS 06-025-0011 AND 06-028-0016

EXCEPTION 29. DEED OF TRUST, VIA ENTRY 1027866, RECORDED IN BOOK 1633 PAGE 1554 ON
AUGUST 30, 2010.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

THE FOLLOWING AFFECT PARCEL 06-025-0009

THE FOLLOWING IS A LIST OF TITLE POLICY EXCEPTIONS PERTINENT TO THIS SURVEY AS THEY
APPEAR IN SCHEDULE B - SECTION 2 OF THE ABOVE-REFERENCED TITLE COMMITMENT.

EXCEPTION 3. REDEVELOPMENT PLAN, VIA ENTRY 467887, RECORDED IN BOOK 332 PAGE 491 ON
APRIL 2, 1984.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 4. RESOLUTION, VIA ENTRY 553645, RECORDED IN BOOK 514 PAGE 313 ON JANUARY 24,
1992.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 6. DOWNTOWN REDEVELOPMENT PROJECT, VIA ENTRY 845076, RECORDED IN BOOK 1268
PAGE 1647 ON OCTOBER 29, 2003.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

EXCEPTION 7. ADOPTION OF AMENDED DOWNTOWN REDEVELOPMENT PROJECT AREA, VIA ENTRY
988472, RECORDED IN BOOK 1550 PAGE 1362 ON JANUARY 27, 2009.
SURVEY FINDINGS: AFFECTS. NOT PLATTED. BLANKET IN NATURE.

SE4 S33 T12N R1E SLM

BUILDING DETAIL

SHEET DESCRIPTION:

ALTA / NSPS SURVEY
FOR
LOGAN CITY



Cache & Landmark

Engineers
Surveyors
Planners

95 Golf Course Rd.
Suite 101
Logan, UT 84321
435.713.0099

DATE:
14 JANUARY 2021

SCALE:
1" = 10'

CALCULATED BY:
S. EARL

CHECKED BY:
S. CROOKSTON

APPROVED BY:
S. EARL

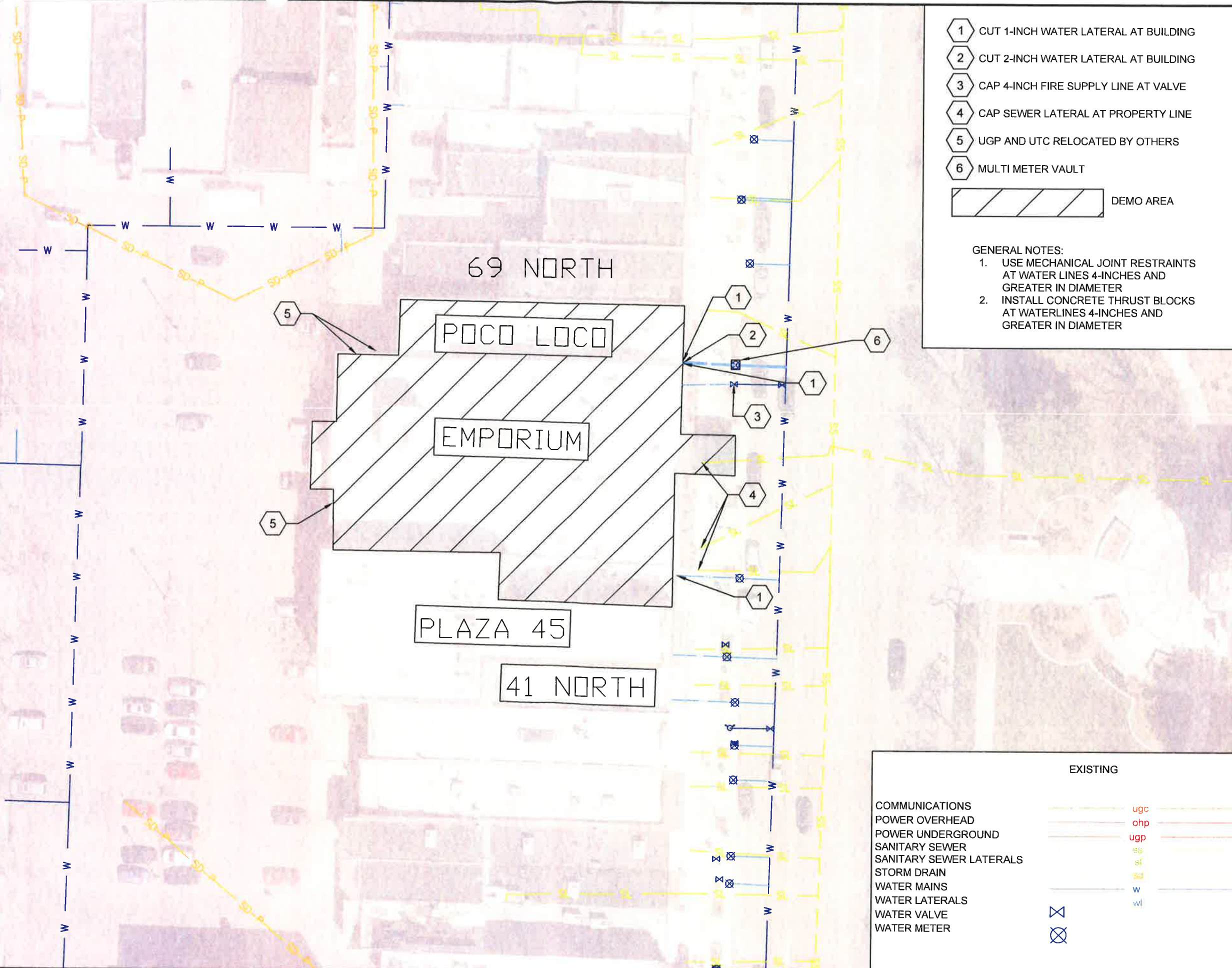
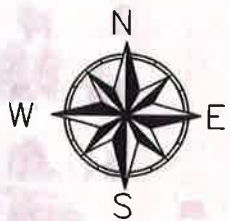
PROJECT NUMBER:
620-2003

2 / 2

1/14/2021 8:53 AM Z:\2020 PROJECTS\620-2003 LOGAN CITY - EMPORIUM ALTA SURVEY\ACAD\SURVEY\BOUNDARY\620-2003 BUILDING DETAIL (CURRENT) DWG



0 5 10 20
GRAPHIC SCALE 1" = 10'



- 1 CUT 1-INCH WATER LATERAL AT BUILDING
- 2 CUT 2-INCH WATER LATERAL AT BUILDING
- 3 CAP 4-INCH FIRE SUPPLY LINE AT VALVE
- 4 CAP SEWER LATERAL AT PROPERTY LINE
- 5 UGP AND UTC RELOCATED BY OTHERS
- 6 MULTI METER VAULT



GENERAL NOTES:
1. USE MECHANICAL JOINT RESTRAINTS AT WATER LINES 4-INCHES AND GREATER IN DIAMETER
2. INSTALL CONCRETE THRUST BLOCKS AT WATERLINES 4-INCHES AND GREATER IN DIAMETER

LOGAN CITY ENGINEERING 290 NORTH 100 WEST LOGAN, UTAH 84321		EMPORIUM BUILDING DEMOLITION		DESIGNED: DRAFTED: TUCKERSON CHECKED: DATE: 29-Mar-21 ENG #: ENG18008	REVISION BLOCK: BIDDING	0' 15' 40' SCALE 1"=40'	SHEET NO: UT-01
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