

Josephine County Transit Master Plan

MAY 2018

Josephine Community Transit

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What is the Transit Master Plan?

What is Josephine Community Transit?

Josephine Community Transit (JCT) is the public transportation agency serving Grants Pass, Josephine County and the greater Middle Rogue region.

JCT is part of the Josephine County Public Works Department.

JCT's existing services are illustrated in Figure 1. They include:

- **Four local bus routes** within Grants Pass (Routes 10, 20, 35 and 40).
- **On-demand paratransit service** within $\frac{3}{4}$ -mile of these local bus routes, as required by the Americans with Disabilities Act (ADA).

- **Three long-distance commuter bus routes** connecting Grants Pass to:

- ▶ Cave Junction (Route 50)
- ▶ Merlin, Hugo, Sunny Valley and Wolf Creek (Route 80)
- ▶ Medford via Rogue River and Gold Hill (Rogue Valley Commuter Line).

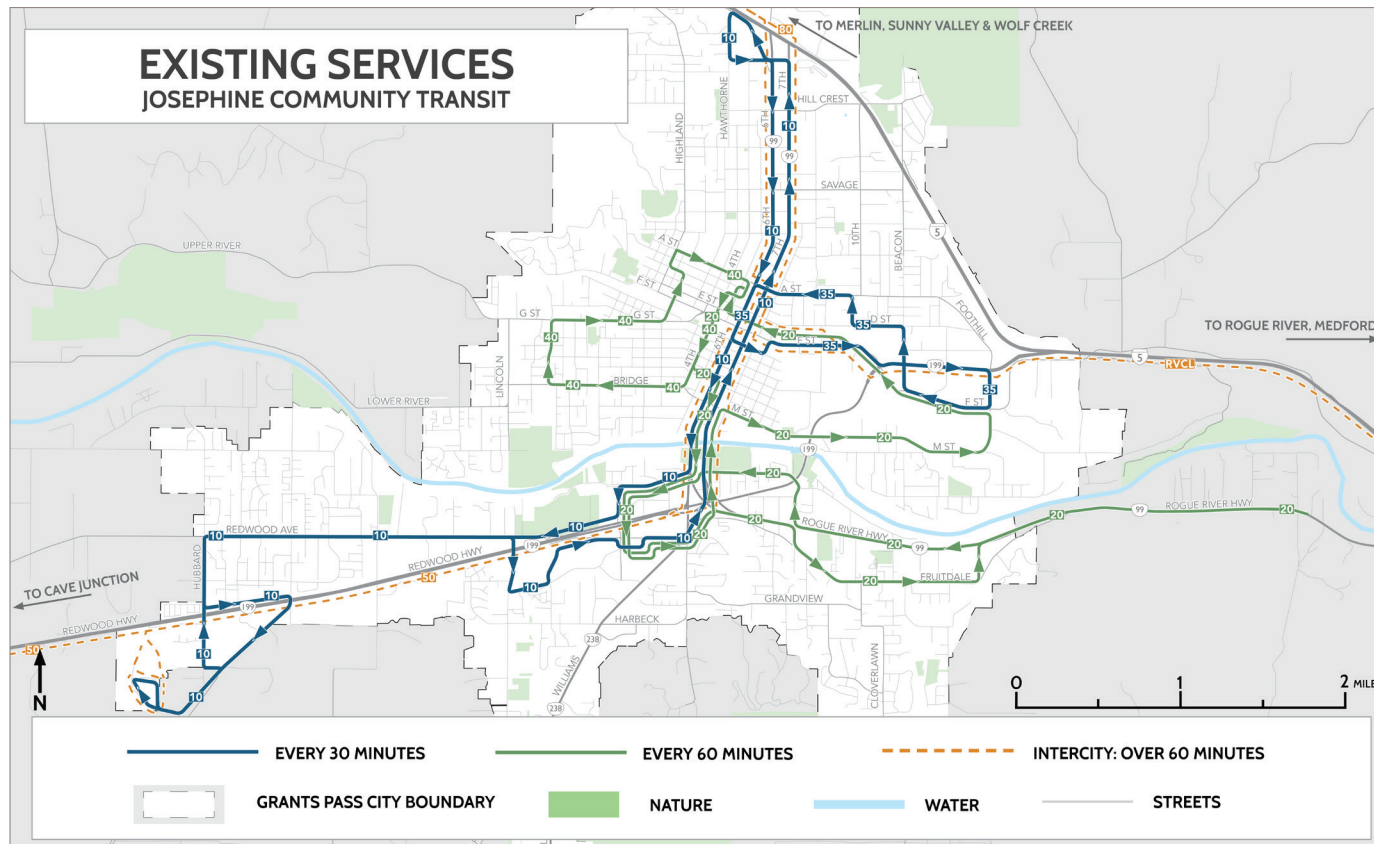


Figure 1: This map shows existing routes operated by Josephine Community Transit.

Colored lines indicate the frequency of each bus route.

Routes 10 and 35 (in blue) operate every 30 minutes.

Routes 20 and 40 (in green) operate every 60 minutes.

Routes 50, 80 and the Rogue Valley Commuter Line (in dotted red) operate 3 to 5 times per day.

All services operate Monday to Friday, mostly in between 7 AM and 7 PM.

Shaping the future transit network

BETTER SERVICE IN THE SHORT AND LONG TERM

The Transit Master Plan describes the future public transit network serving Grants Pass, Josephine County and the Middle Rogue Region. It includes:

- **A redesigned bus network that fixes known issues** with existing service. JCT can begin operating this new network in 2018.
- **Medium-term service improvements based on passenger and public input.** JCT will begin to implement these improvements in 2019 and 2020, as new funding becomes available under the Keep Oregon Moving law (also known as HB 2017).
- **Concepts to adapt transit service in long-term,** depending on available funding and growth in the region.

TRADE-OFFS AND CHOICES – A PUBLIC CONVERSATION

The Transit Master Plan proceeds from choices that were presented in the Existing Conditions and Choices Report, and through a widely publicized online community survey.

SHORT TERM: RIDERSHIP VS. COVERAGE

In the short term, Josephine Community Transit's funding (and options) are extremely limited. We asked the public to compare two scenarios:

- The Ridership Alternative would achieve service every 15 minutes between Downtown Grants Pass, the hospital area, and Rogue Community College. But this would require removing service from the West End and Southeast Grants Pass.
- The Coverage Alternative would maintain bus routes in all areas currently served. But it includes fixes to make service more reliable; some trips would become longer as a result.

Based on survey responses, the recommended short-term network follows the Coverage Alternative.

LONGER TERM: EXPANDING THE NETWORK

If new funding appears, JCT will need to decide how to expand service. The survey asked respondents to prioritize the following types of changes:

- Longer hours of service.
- Higher frequency (shorter waits).
- Service on Saturdays.
- New service to more places.

Survey responses show that the public is most interested in Saturday service, but respondents also expressed significant interest in service to new places, and in longer hours of service.

Thanks to HB 2017, JCT can now contemplate improvements. **The Master Plan proposes increasing service in the following order, as funds become available:**

1. Saturday service on all local and long-distance commuter routes.
2. 30-minute service on Rogue River Highway and in the West End.
3. Evening service on local routes.
4. Two additional runs per day on existing long-distance commuter routes.
5. A new long-distance commuter route connecting Grants Pass to Murphy and Williams.
6. A new local route serving Highland Avenue and Williams Highway.

2 What options exist for transit service?

In the short term, funding (and options) are very limited.

EXISTING FUNDING SOURCES WILL NOT INCREASE...

Josephine Community Transit relies heavily (80%) on federal and state grants for funding.

Local operating funds consists mostly of farebox revenue, agreements with Rogue Community College and revenue from local health and human services agencies.

None of these funding sources is expected to expand significantly anytime soon.

This means that, under current funding, it is not possible to increase service in one place without reducing it somewhere else.

...AND THE NETWORK IS SMALL...

The existing JCT network includes only four vehicles operating all-day locally in Grants Pass, and up to four additional vehicles operating occasional long-distance commuter routes.

There are only so many reasonable ways to deploy an all-day fleet of four vehicles.

...SO THE AVAILABLE CHOICES PRESENT STARK TRADE-OFFS.

In this context, JCT and Jarrett Walker + Associates developed two scenarios for future service that would not require any new operating or capital funds.

Ridership Alternative: A Radical Shift

The Ridership Alternative illustrates what a radical change in policy might look like. Under this scenario, service is concentrated to be as frequent as possible, but only in the areas likely to generate the highest possible number of riders. Specifically, this alternative would:

- Eliminate service in Southeast Grants Pass and the West End currently provided by Routes 20 and 40.
- Extend 30-minute service from Fred Meyer/Walmart through Downtown to Union Avenue and Rogue Community College.
- This would result in service every 15 minutes between Downtown Grants Pass, Union Avenue, and Rogue Community College.

Coverage Alternative: Incremental Fixes

The Coverage Alternative would continue to provide service to as many neighborhoods as possible, but includes several changes intended to fix known issues and vulnerabilities in the network.

Specifically, this alternative would:

- Reduce the number of places in the network where customers transfer from one route to another. Timed transfers would only take place at a new Downtown Transit Center, and at Walgreens on Union Avenue.
- Increase the reliability of transfers in a variety of traffic conditions by allowing several minutes between connecting buses at transfer points. At the moment, transfers are timed to-the-minute and require constant radio communication between drivers and dispatchers.
- Replace one-way service on Rogue River Highway and Fruitdale Ave with two-way service on Rogue River Highway.
- Extend the loop on the West End out to Lincoln Road.

Short-Term Choices: Ridership Alternative

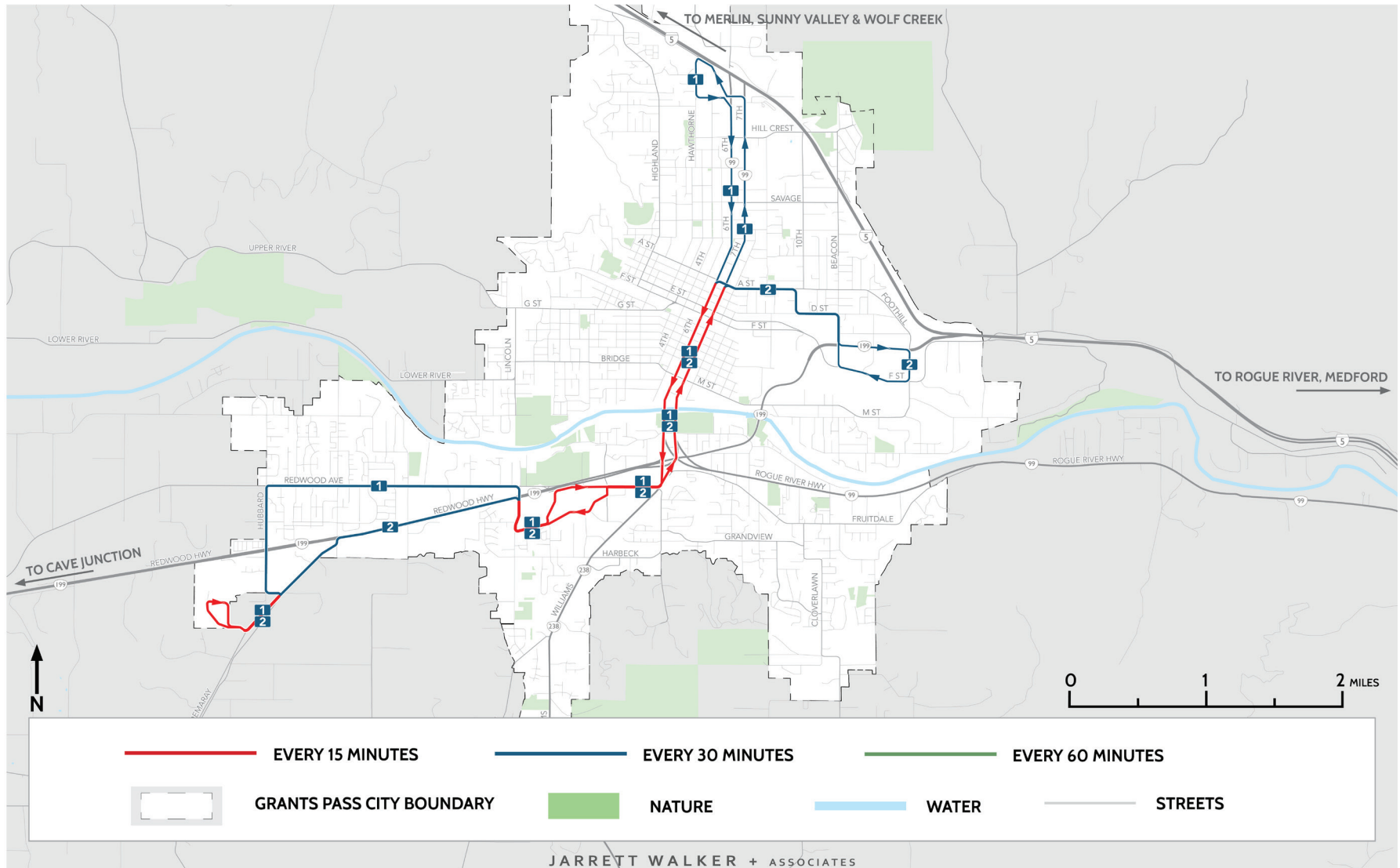


Figure 2: In the Ridership Alternative, Routes 1 and 2 would both operate every 30 minutes in both directions, providing a combined 15 minute frequency between Downtown, Three Rivers Hospital and Rogue Community College. The system would include no timed transfers. However, areas along Rogue River Highway and west of Downtown would lose service.

Short-Term Choices: Coverage Alternative

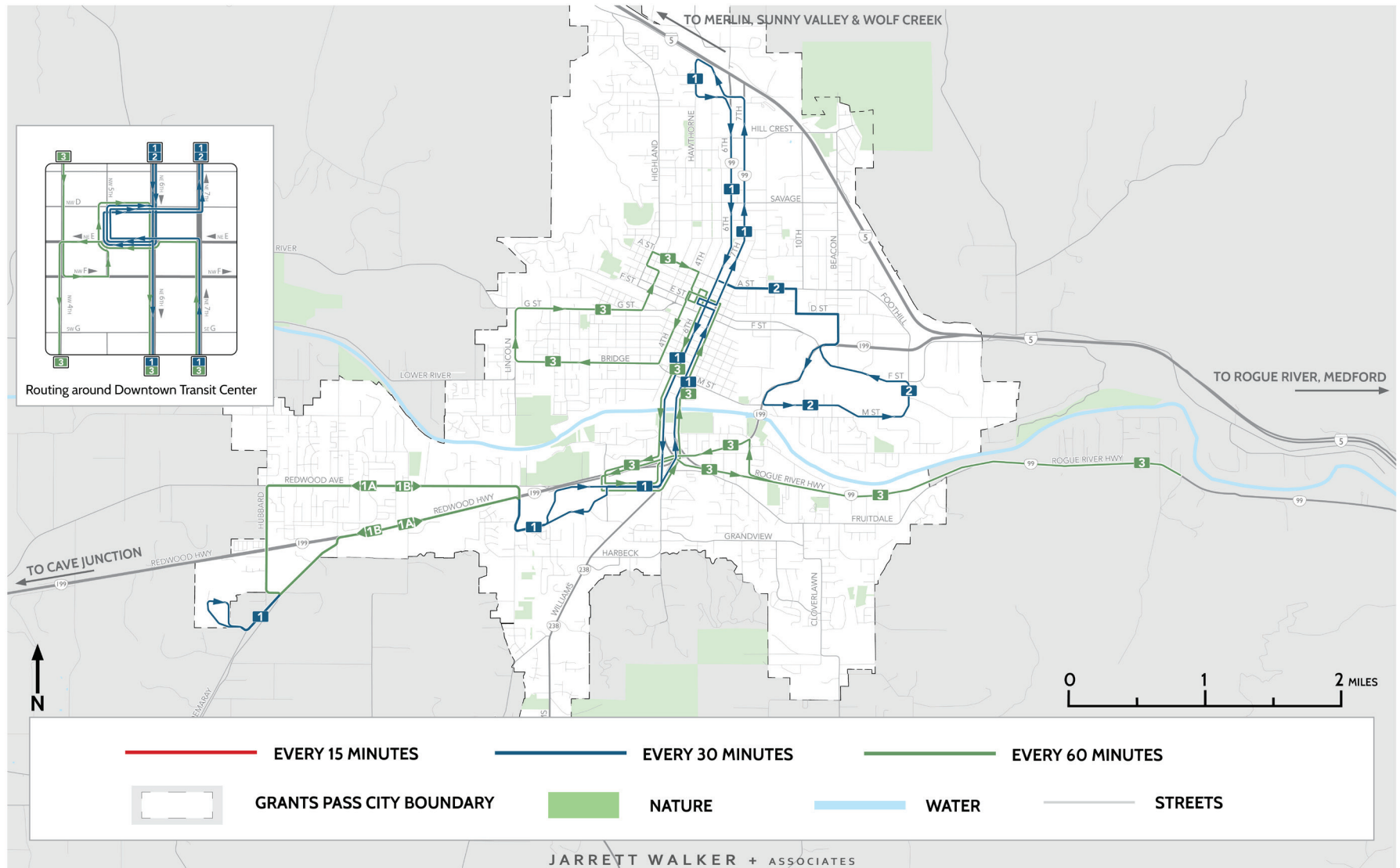


Figure 3: In the Coverage Alternative, Route 1 would operate every 30 minutes between the North End, Downtown, Three Rivers Hospital and Rogue Community College. Route 2 would connect Downtown to commercial, industrial and residential areas to the east. Route 3 would combine hourly service west of Downtown with two-way service on Rogue River Highway.

Many more options will become available in 2019-2020

HOUSE BILL 2017 MASSIVELY INCREASES TRANSIT FUNDING

The Oregon Legislature has created a payroll tax that will provide approximately \$130 million per year of dedicated public transit funding. 90% of these funds will be allocated to local jurisdictions based on a set formula, and a further 5% will be available for competitive applications.

Different estimates show that Josephine County could receive between \$800,000 and \$1.2 million annually for transit operations over the next ten years.

On its own, this would represent a 60 to 90% increase from existing conditions. However, receiving these funds would also allow Josephine Community Transit to claim unused federal matching funds amounting to approximately \$260,000 per year.

In other words, JCT's annual operating budget could increase by \$1 to \$1.5 million annually (not including competitive funds). Assuming that the appropriate fleet and drivers are available, **this could nearly double the amount of service provided by JCT.**

THIS CREATES NEW POSSIBILITIES AND NEW DECISIONS

JCT could use this new funding to increase its fixed-route service in many useful ways. Each of these would serve different purposes. For example:

- **Increased Frequency.** JCT could decide to operate some or all of its routes on a more frequent schedule. This would reduce waits experienced by transit users, and would make the service attractive to more users.
- **Longer Hours of Service.** At the moment, JCT operates most of its routes from around 6 AM to 6 PM. New funds might allow JCT to provide evening service. This would let existing transit users rely on JCT for a broader range of trips, and would allow new users (whose days end later) to take advantage of the network.
- **Saturday Service.** JCT currently operates from Monday to Friday only. As a result, many transit users are stranded on weekends, or forced to rely on inconvenient alternatives, such as long walks, or relying on friends or family for a ride. Provid-

ing service on Saturdays would make transit far more convenient.

- **Service to New Places.** There are known geographic gaps in JCT's service. For example, JCT's long-distance commuter routes serve the I-5 corridor, the Illinois Valley and Northeast Josephine County, but there is no Williams Highway service to Murphy and Williams. JCT consistently hears from surveys and stakeholders that this particular route would be a desired improvement.

3 What choices should JCT make?

JCT consulted the public about its choices.

Jarrett Walker + Associates and JCT prepared an online survey about the Transit Master Plan, and opened to the public from May to October 2017.

The survey gathered 669 responses, including responses from people:

- From all parts of Josephine County.
- Who never ride transit, who ride occasionally, and who ride regularly.
- In all age groups (although seniors are somewhat underrepresented).

People were reached through many different means, including:

- E-mail sent to 5,000+ Rogue Community College students in May 2017.
- Direct outreach by JCT to bus riders, social service agencies, and medical providers in August 2017.
- Letter sent by mail to all 20,000+ members of AllCare Health in September 2017.

The sheer number of respondents and their social diversity suggests that the results of the survey are representative of the views of the general public.

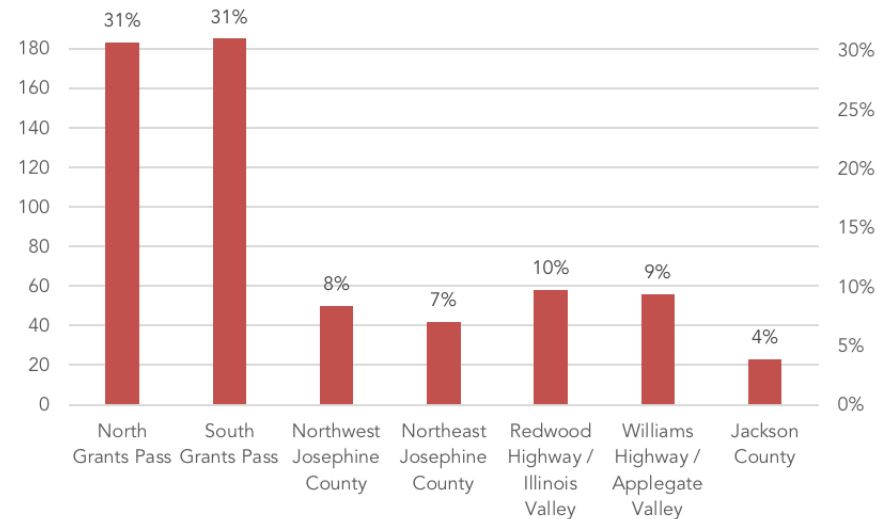


Figure 4: JCT heard from 669 members of the public, from all parts of Josephine County.

Figure 5: There were respondents in all age groups.

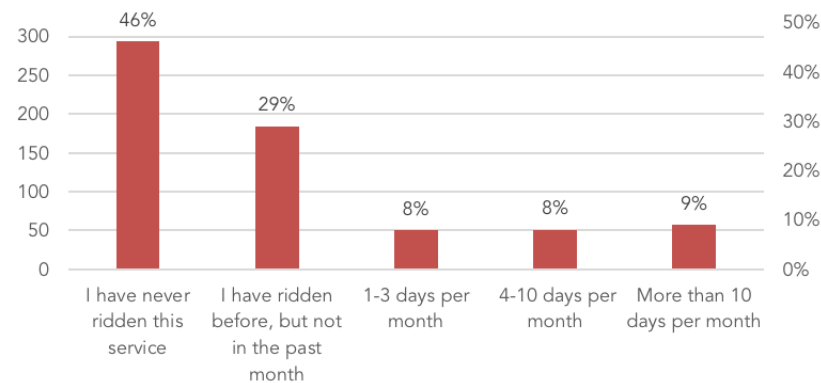
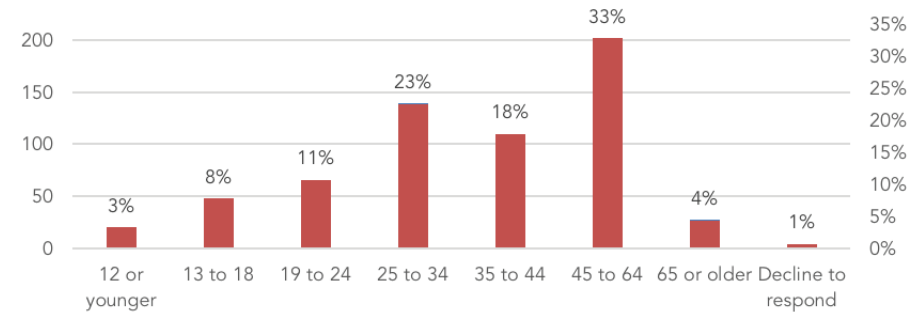


Figure 6: Respondents included regular transit users, occasional users, and people who have never ridden transit.

In the short term, the Coverage Alternative is preferred.

MOST PEOPLE WOULD LIKE SOMETHING IN BETWEEN THE TWO ALTERNATIVES...

When asked directly about the Ridership and Coverage alternatives:

- A slim majority (52%) of survey respondents said that they would prefer “something in between”.
- Of the remaining 48%, more than twice as many respondents strongly preferred the Coverage Alternative (33%), than strongly preferred the Ridership Alternative (15%).

...BUT MOST PEOPLE ALSO WANT JCT SERVICE TO COVER MORE PLACES

When asked about future service improvements (see next page), nearly 55% of respondents listed “service to new places” as their first or second priority out of 4 options.

In practice, the “in between” option means cutting service in either the West End, or Southeast Grants Pass, neither of which is likely to be acceptable.

Therefore, JCT has opted to proceed with the Coverage Alternative.

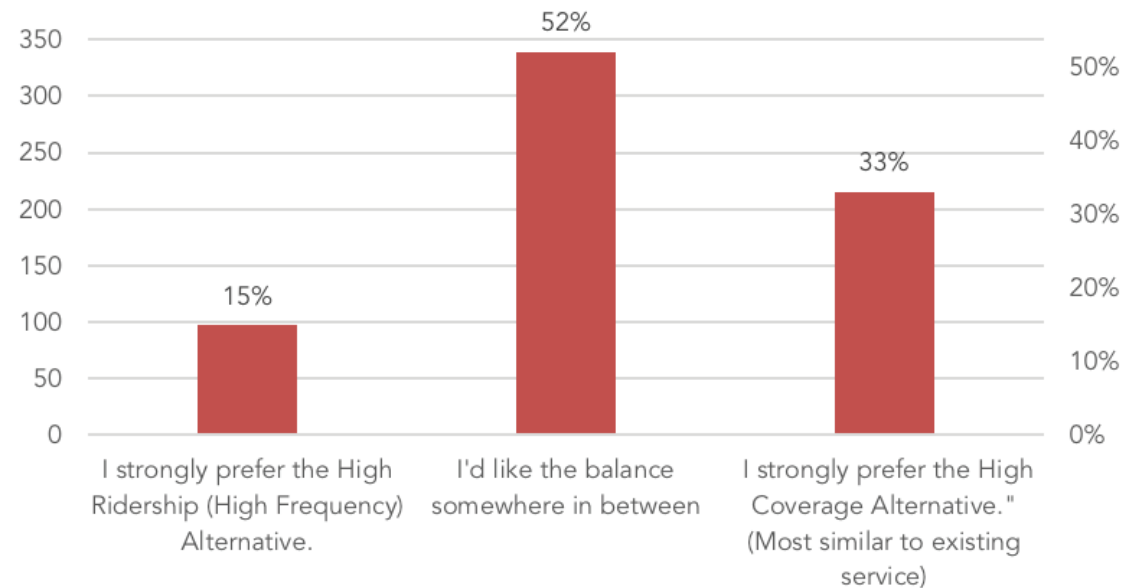


Figure 7: Most survey respondents would like something “in between” the Ridership Alternative and Coverage Alternative, but among those with a strong preference, over twice as many preferred the Coverage Alternative.

Priorities for spending new funds in the medium-term

Survey respondents rated four possible ways to improve JCT's fixed route services, if new funding became available.

PRIORITY 1: SERVICE ON SATURDAYS

Nearly 60% of those who responded said service on Saturdays would be their first or second priority. This rises to 70% among those who ride JCT local buses at least once per month.

PRIORITY 2: SERVICE TO MORE PLACES

Nearly 55% listed service to new places as their first or second priority. This rises to 60% among those living outside of Grants Pass, and those age 45 and older.

PRIORITY 3: LONGER HOURS

Slightly less than 50% of respondents listed longer hours of service as their first or second priority. This figure rises to nearly 55% among those aged 18 to 34.

LOWEST PRIORITY: HIGH FREQUENCY

Just over 35% of respondents listed higher frequency service as their first or second priority. This low level of interest is consistent with the low percentage of respondents who strongly prefer the Ridership Alternative.

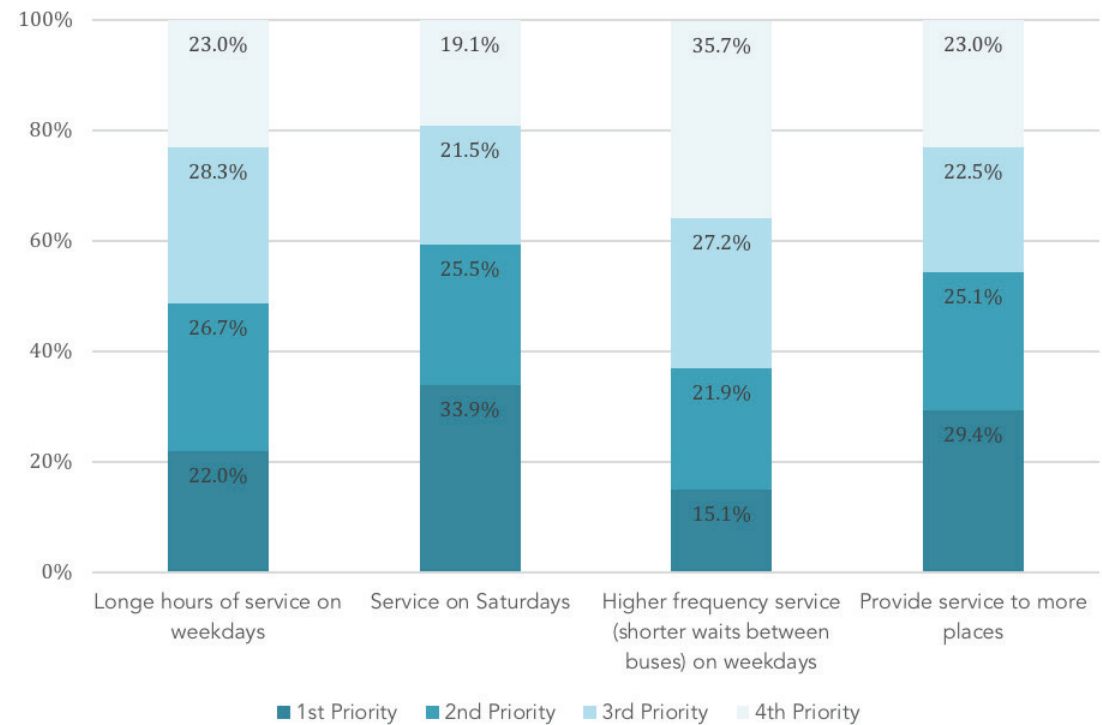


Figure 8: If more funding were available, respondents made it clear that they would be interested in service on Saturdays, and service to more places. There is also some interest in longer weekday hours of service, but much less interest in higher frequency services.

What about the Rogue Valley Commuter Line?

STRONG MARKET, BUT LOW RIDERSHIP

Medford and Grants Pass are the two economic centers of the Rogue Valley. Census employment data suggest that over 2,500 people commute between the two on an average weekday.

But the Rogue Valley Commuter Line (RVCL)'s ridership is barely higher than ridership on Route 50 between Grants Pass and Cave Junction, despite providing the same level of service (5 trips per day in each direction) to a much larger market.

POSSIBLE REASONS WHY

Two key factors likely underlie the low ridership on the RVCL:

- **The schedule works against commuters to Grants Pass.** It's not possible to arrive in Grants Pass in the morning, work a full day, and come home.
- **The schedule takes 1 hour from Downtown Grants Pass to Downtown Medford.** This distance can often be driven in 35 minutes. Even in peak hour traffic, it's rare for the trip to take over 50 minutes.

HOW COULD RIDERSHIP INCREASE?

- **Earlier and later service.** To serve the commuter market, it should be possible to arrive in Grants Pass in the morning, and leave on a bus 9 to 10 hours later. This would require:
 - ▶ Earlier service from Medford to Grants Pass, such as a bus leaving Medford before 7 AM and arriving in Grants Pass before 8 AM. This bus should be based in Medford.
 - ▶ Later service from Grants Pass to Medford. One or two runs leaving Grants Pass between 5 and 7 PM.
- **Less time in Rogue River and Gold Hill.** Stops in these towns add 10 to 15 minutes of travel on each trip.
 - ▶ Freeway off-ramp stops in Rogue River would allow the bus to avoid crossing a railroad and then turning around in the center of town.
 - ▶ Consider not stopping in Gold Hill. Because of Gold Hill's location relative to I-5, it's very hard to serve conveniently. The bus stop is located 1 mile (by rural two-lane road) from the center of town. As a result, ridership is extremely low.

Stop Name	Average Daily Boardings
Grants Pass	24
Rogue River	8
Gold Hill	2
Medford	27
Average Total Boardings	61

Figure 9: RVCL Ridership averages around 60 per day, which is 6 boardings per trip.

Grants Pass to Medford

	Grants Pass	Rogue River	Gold Hill	RVTD FSS
AM	6:35	6:55	7:10	7:35
	7:35	7:55	8:10	8:35
	11:05	11:25	11:40	12:05
PM	3:15	3:35	3:50	4:15
	4:15	4:35	4:50	5:15

Medford to Grants Pass

	RVTD FSS	Gold Hill	Rogue River	Grants Pass
AM	7:45	8:10	8:25	8:45
	8:45	9:10	9:25	9:45
PM	12:15	12:40	12:55	1:15
	4:25	4:50	5:05	5:25
	5:25	5:50	6:05	6:25

Figure 10: The RVCL schedule makes it possible to commute from Grants Pass to Medford, but not the other way around.

Freeway off-ramp stops could speed up service to Medford.

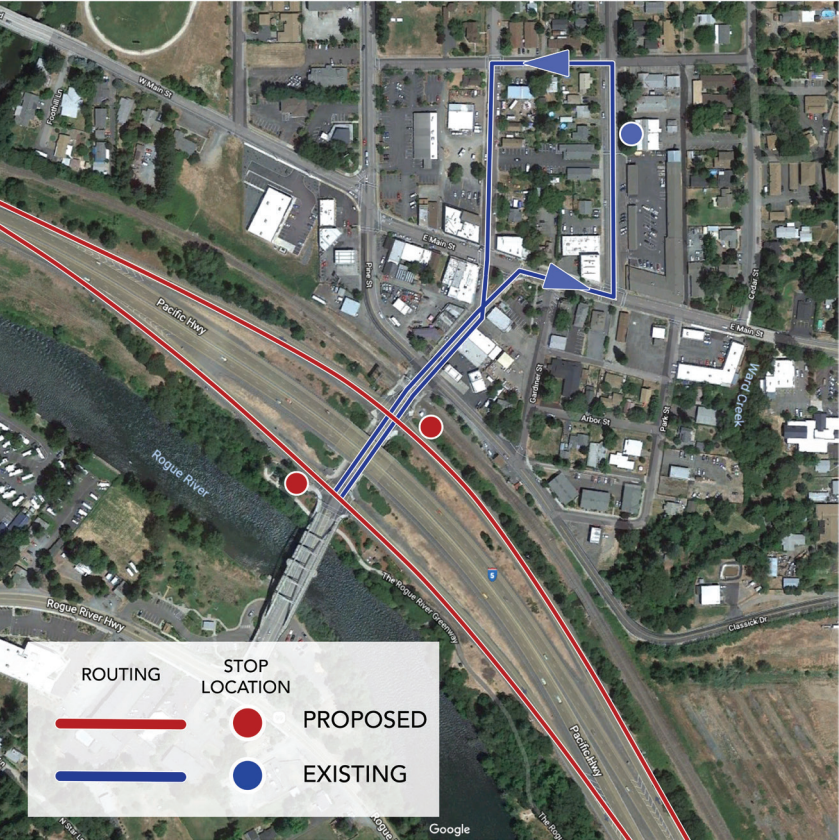


Figure 11: In Rogue River, the bus currently exits I-5, crosses the COP&R railroad, and stops at Rogue River Community Center. This requires up to 10 minutes of schedule padding, due to multiple turns and unpredictable freight trains. This extra time could be eliminated if the RVCL used “flyer” stops on the freeway off-ramps, and then proceeded directly back onto I-5.

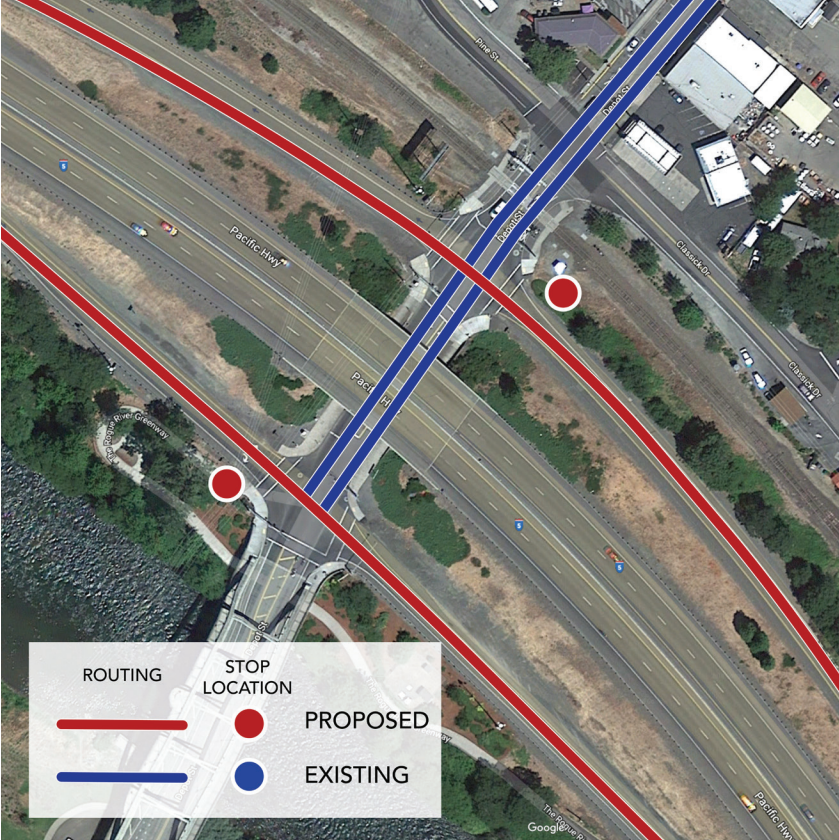


Figure 12: There are already sidewalks and signaled crossings protecting pedestrian movements to and from the off-ramps, and across the highway underpass. This means that off-ramp stops could be implemented with grading and bus pads on the side of the off-ramps themselves, and protected crosswalks across Pine Street/Classick Drive.

4 Planned Service Changes and Improvements

Short and Medium-Term Improvements (2018 - 2030)

This plan establishes a set of steps to improve transit service in the short and medium-term as funds become available.

SHORT TERM (2018 - 2019)

Step 1: Implement the Coverage Alternative

As described in other parts of this report, the Coverage Alternative seeks to maintain transit in all areas currently served, while reducing the network's vulnerability to congestion. This is a budget-neutral action.

MEDIUM TERM (2020 - 2030)

The steps described in the medium term require new funding. We have estimated the cost of each step based on new revenue hours, revenue miles, fuel and maintenance, paratransit, and overhead costs.

The exact implementation timeline for each step remains unclear, because the amount of funding available directly for JCT service under House Bill 2017 is not exactly known (see page 10). Nonetheless, most of the steps may be implemented by 2020-2021, with remaining actions to be taken in following years.

Step 2: Saturday Service

This will bring service on Saturdays to all JCT routes, at the same level of service provided Monday to Friday.

Step 3: 30-minute service on Route 3

Passengers on the West End and along Rogue River Highway will receive the same level of service as in other parts of Grants Pass.

Step 4: Evening Service

All local routes will operate for 3 hours longer every evening, with service ending at 9:30 PM.

Long-distance commuter routes will operate 2 extra trips per day in both directions. These trips may be added at any time of day.

Step 5: Service to Murphy and Williams

A new long-distance commuter route will operate 5 trips per day in both directions from Grants Pass to Williams, going through Murphy.

Step 6: Highland Ave / Williams Hwy

A local route will operate on Highland Avenue and Williams Highway, every 30 to 60 minutes depending on funding.

Added Cost of Short and Medium-Term Improvements					
Improvement	Local		Commuter Routes	Overhead	Added Annual Operating Cost
	Fixed Routes	Paratransit			
1 Implement the Coverage Alternative	-	-	-	-	\$ -
2 Saturday Service	\$ 129,000	\$ 55,000	\$ 87,000	\$ 239,000	\$ 510,000.00
3 30-minute service on Route 3	\$ 187,000		-	-	\$ 187,000.00
4 Evening Service	\$ 197,000	\$ 41,000	\$ 191,000	-	\$ 429,000.00
5 Service to Murphy & Williams	-	-	\$ 142,000	-	\$ 142,000.00
6a 60-minute service on Highland Ave / Williams Hwy	\$ 183,000	\$ 122,000	-	-	\$ 305,000.00
6b 30-minute service on Highland Ave / Williams Hwy	\$ 366,000	\$ 122,000	-	-	\$ 488,000.00
Added Annual Operating Cost*	\$ 664,000*	\$ 250,000*	\$ 420,000*	\$ 239,000*	\$ 1,573,000*

* All totals assume improvement 6a only

Figure 13: After the Coverage Alternative, each following improvement involves an increase in service. This table summarizes the operating costs (in 2019 dollars) associated with each service increase. Some or all of these may be paid for with funds coming out of House Bill 2017.

Step 1. Implement the Coverage Alternative (cont'd).

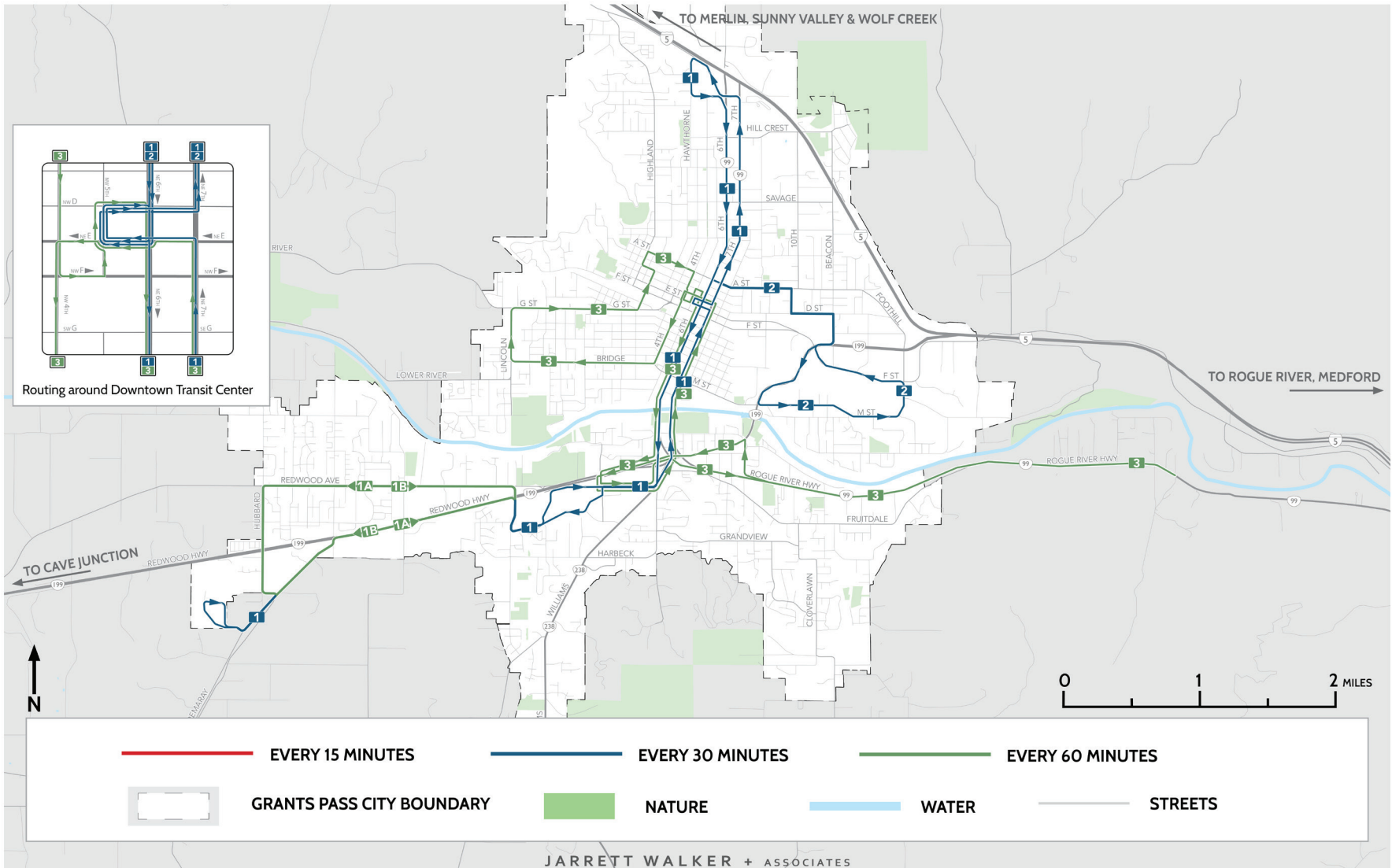


Figure 15: After this first service change, Route 1 will operate every 30 minutes between the North End, Downtown, Three Rivers Hospital and Rogue Community College. Route 2 will connect Downtown to commercial, industrial and residential areas to the east. Route 3 will combine hourly service west of Downtown with two-way service on Rogue River Highway.

Step 2. Saturday service on all routes.

THE COMMUNITY'S TOP PRIORITY

Survey results show that adding service on Saturday should be the highest priority use for any new transit funding.

This will be especially useful for existing JCT riders, who would otherwise get a ride from friends, walk long distances, or avoid travelling altogether.

As a result, this plan recommends implementing Saturday service as soon as possible, at the same level of service currently offered on weekdays.

WHY NOT PROVIDE LESS FREQUENT SATURDAY SERVICE AT A LOWER COST?

Most of the cost of implementing Saturday service is related to increased overhead and paratransit requirements. Reducing service wouldn't save much:

- Operating Route 1 hourly on Saturdays would save only \$32,000 per year.
- Routes 2 and 3 cannot operate less frequently; JCT cannot send a driver home for a half-hour every hour (or an hour every two hours).
- Operating long-distance routes only three times per day would save only about \$27,000 per year.

Local: Saturdays						
Day	New Annual Rev Hrs	New Annual Rev Miles	New Annual Overhead	New Annual Cost Local	New Annual Cost Paratransit	Total New Annual Cost
Weekday	-	-	\$198,000	\$ -	\$ -	\$ 198,000
Saturday	2,500	36,500	\$ 41,000	\$129,000	\$ 55,000	\$ 225,000
Sunday	-	-	\$ -	\$ -	\$ -	\$ -
			\$239,000	\$129,000	\$ 55,000	\$423,000

Figure 16: Most of the cost of adding service on Saturday is related to a very significant increase in office staff time, which increases overhead costs.

Long Distance: Saturdays			
Day	New Annual Rev Hrs	New Annual Rev Miles	Total New Annual Cost
Weekday	-	-	\$ -
Saturday	1,300	40,000	\$ 87,000
Sunday	-	-	\$ -
			\$ 87,000

Figure 17: If local service is being provided on Saturday, it would be relatively inexpensive to also provide long-distance services.

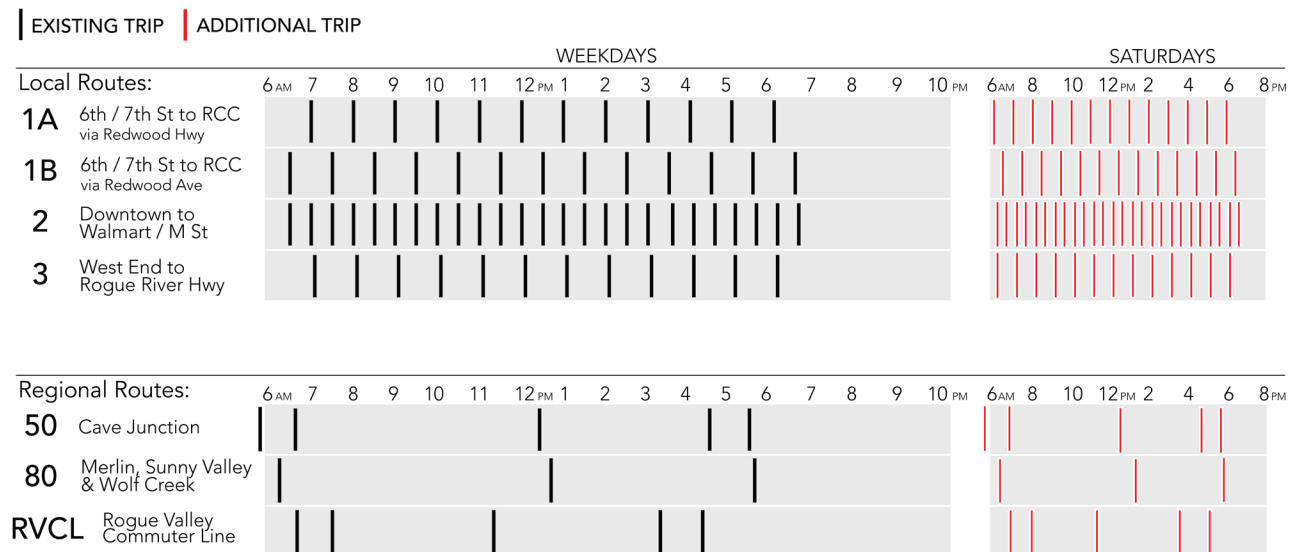


Figure 18: This graphic shows approximate departure times for each route from Downtown Grants Pass. The red lines indicate new trips added once Saturday service is implemented.

Step 3. 30-minute service on Route 3.

GETTING THE SAME LEVEL OF SERVICE IN MORE NEIGHBORHOODS

At the moment, areas on Routes 10 and 35 receive service every 30 minutes, while areas on Routes 20 and 40 receive service only once an hour.

This will continue similarly under the budget-neutral Coverage Alternative. Routes 1 and 2 will operate every half-hour (except on Redwood Avenue and Redwood Highway), whereas Route 3 will only operate hourly.

Adding 30-minute service to Route 3 will ensure that several thousand more residents will receive the same level of service provided on Routes 1 and 2.

This will likely significantly increase ridership on all JCT local routes.

- Ridership on Route 3 itself may increase significantly due to more frequent and convenient service.
- Ridership on Routes 1 and 2 may also increase slightly due to more frequent connections with Route 3.

Local: 30-Minute Service on Route 3						
Day	New Annual Rev Hrs	New Annual Rev Miles	New Annual Overhead	New Annual Cost Local	New Annual Cost Paratransit	Total New Annual Cost
Weekday	3,000	41,000	\$ -	\$155,000	\$ -	\$ 155,000
Saturday	600	8,400	\$ -	\$ 32,000	\$ -	\$ 32,000
Sunday	-	-	\$ -	\$ -	\$ -	\$ -
			\$ -	\$187,000	\$ -	\$187,000

Figure 19: Service wouldn't be added in new places or on new days, so the cost of more frequent service is only related to direct operations and vehicle maintenance.

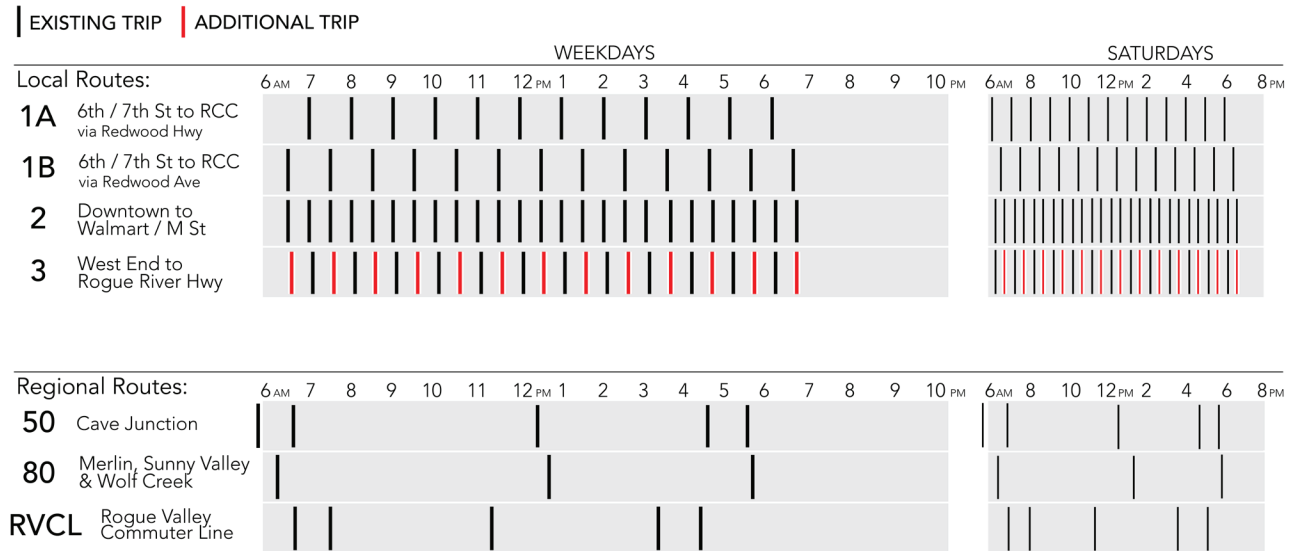


Figure 20: This graphic shows approximate departure times for each route from Downtown Grants Pass. The red lines indicate new trips added to provide 30-minute service on Route 3.

Step 3. 30-minute service on Route 3. (cont'd)

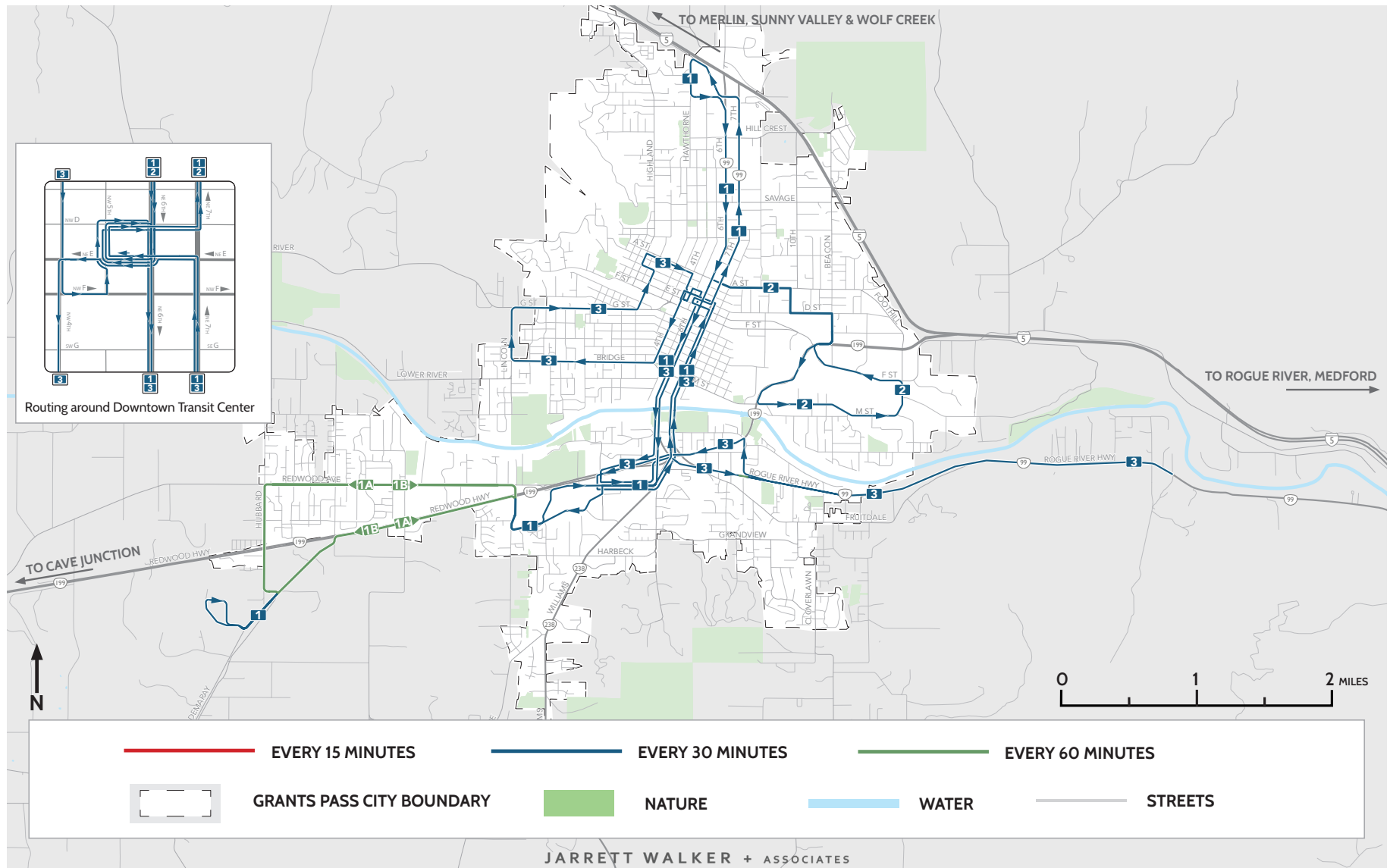


Figure 21: At this stage, nearly all neighborhoods in Grants Pass would have service every 30 minutes from, 6:30 AM to 6:30 PM, Monday to Saturday. Note that this is still not the case for Redwood Avenue, due to timing issues on Route 1.

Step 4. Evening service.

ENABLING DIFFERENT TYPES OF TRIPS

JCT's existing service operates from 6:30 AM to 6:30 PM. This limits how many types of trips JCT can serve. This plan proposes that JCT extend service three hours later in the evening, until 9:30 PM on local routes.

There are many reasons why one might need to travel earlier or later in the day, such as:

- Holding a job with non-traditional hours, or even just the occasional later shift.
- Attending evening classes at Rogue Community College.
- Evening appointments, shopping or socializing.
- Sometimes, it's just important to know that there's another, later bus in case you miss the one you intended.

The plan also recommends adding two runs per day on long-distance commuter routes. The timing of these trips remains to be determined, as JCT has received many different requests. One or both of those may be in the evening, particularly for the Rogue Valley Commuter Line.

Local: Evening Service						
Day	New Annual Rev Hrs	New Annual Rev Miles	New Annual Overhead	New Annual Cost Local	New Annual Cost Paratransit	Total New Annual Cost
Weekday	3,800	54,900	\$ -	\$197,000	\$ 41,000	\$ 238,000
Saturday	-	-	\$ -	\$ -	\$ -	\$ -
Sunday	-	-	\$ -	\$ -	\$ -	\$ -
			\$ -	\$197,000	\$ 41,000	\$238,000

Figure 22: Adding evening service on local routes would require extending paratransit service into the evening, which adds to cost. However, new overhead is already captured under Step 2 (Saturday service).

Long Distance: Added Runs			
Day	New Annual Rev Hrs	New Annual Rev Miles	Total New Annual Cost
Weekday	3,000	88,300	\$ 191,000
Saturday	-	-	\$ -
Sunday	-	-	\$ -
			\$191,000

Figure 23: The estimated cost of new long-distance trips is proportional to added time and miles.

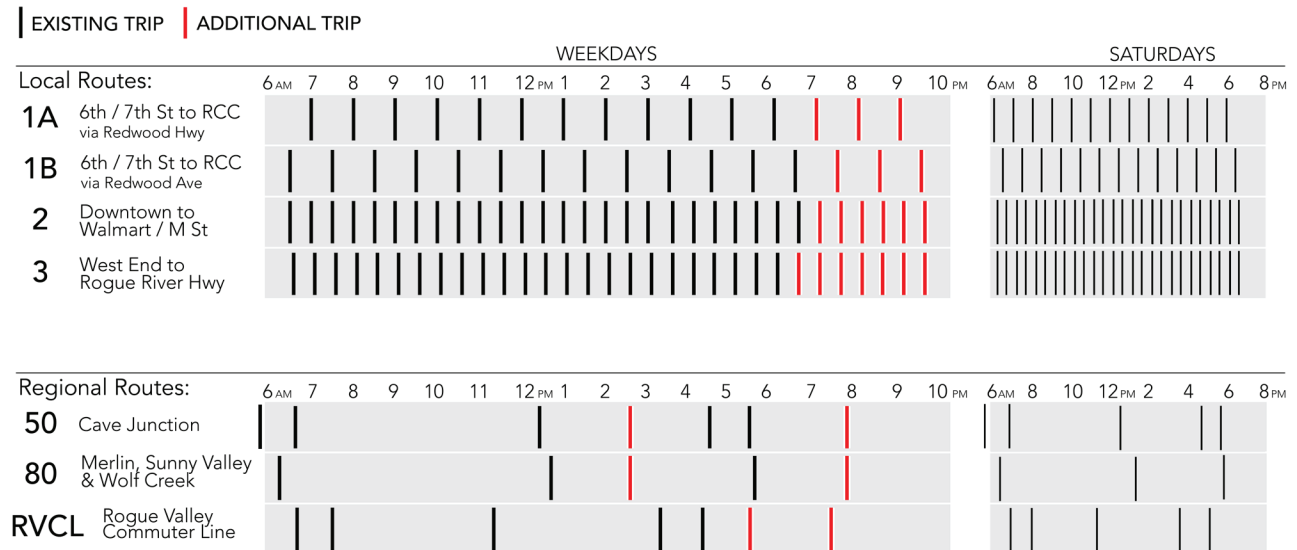


Figure 24: This graphic shows approximate departure times for each route from Downtown Grants Pass. Red lines indicate new evening service on local routes, and added trips on long-distance commuter routes. Departures shown for these long-distance routes are just one possible scenario; the actual new trips may be at different times.

Step 5. Route 60 to Murphy and Williams.

SERVICE TO MORE COMMUNITIES

Survey respondents made it clear that most community members would like Josephine Community Transit service extended to cover new areas.

In addition, JCT has received numerous requests over several years for service to outlying communities. The most frequent of these requests are for service along the Williams Highway corridor to Murphy and Williams.

As a result, this plan proposes creating a new long-distance commuter Route 60 that can serve these areas.

Route 60 would make five round-trips per day on weekdays, and three round-trips on Saturday.

The exact timing of those trips remains to be determined, though it's likely that it would follow a similar schedule to Route 80, which would operate at the same service level.

Long Distance: New Rt 60			
Day	New Annual Rev Hrs	New Annual Rev Miles	Total New Annual Cost
Weekday	1,900	49,700	\$ 118,000.00
Saturday	400	10,100	\$ 24,000.00
Sunday	-	-	\$ -
			\$142,000

Figure 25: The estimated cost of a new long-distance route is proportional to added time and miles.

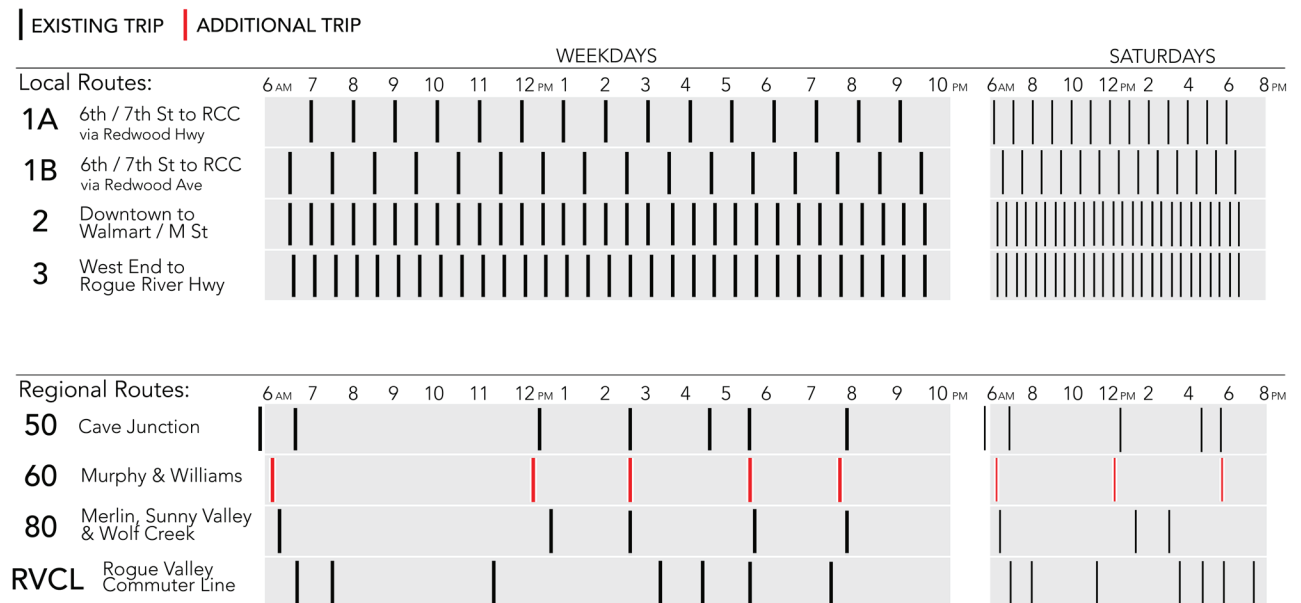


Figure 26: This graphic shows approximate departure times for each route from Downtown Grants Pass. Red lines indicate new service on Route 60 to Murphy and Williams. Departures for Route 60 are just one possible scenario; the actual new trips may be at different times.

Step 5. Route 60 to Murphy and Williams. (cont'd)

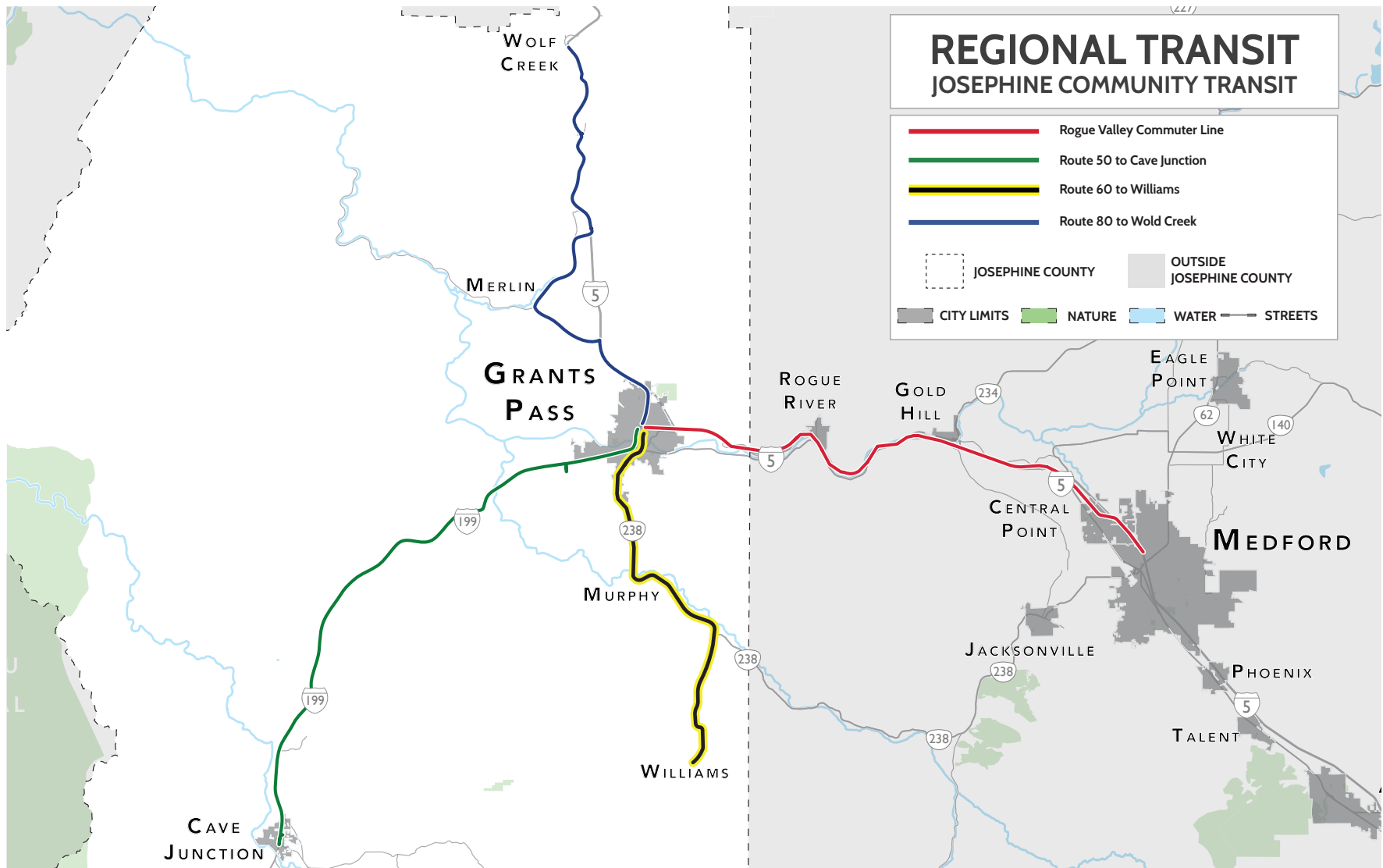


Figure 27: This map shows one possible alignment for a future Route 60, taking Williams Hwy (Route 238) from Downtown Grants Pass past Murphy, and using Water Gap Road to reach Williams. Other alignments are possible and should be discussed closer to implementation. For example, it may be desirable to serve New Hope Road on the way to Murphy, or to stay on Route 238 until Provolt.

Step 6. Route 4 on Highland Ave & Williams Hwy

FILLING IN THE GAPS

Like the existing network, the Coverage Alternative won't bring local bus service to every neighborhood in Grants Pass.

This step would help expand service to some of the neighborhoods located farther from JCT service.

Given the relatively high population near Highland Avenue, and the presence of the Department of Veterans Affairs on Williams Highway, Route 4 is the next logical step in local service.

Adding this type of new route is consistent with the priorities expressed by respondents to the community survey.

However, neither Highland Avenue nor Williams Highway features a great concentration of destinations useful to a broad swath of potential transit riders.

As a result, Step 6 should be viewed as a contingent possibility, if available funding turns out to be on the very high end of what seems possible.

Local: New Route 4 (60-Minute Service)						
Day	New Annual Rev Hrs	New Annual Rev Miles	New Annual Overhead	New Annual Cost Local	New Annual Cost Paratransit	Total New Annual Cost
Weekday	3,000	38,000	\$ -	\$152,000	\$122,000	\$ 274,000
Saturday	600	7,700	\$ -	\$ 31,000	\$ -	\$ 31,000
Sunday	-	-	\$ -	\$ -	\$ -	\$ -
			\$ -	\$183,000	\$122,000	\$305,000

Figure 28: Route 4 would expand JCT's paratransit obligation to new areas with significant senior and mobility-challenged populations. As a result, there is a relatively high cost to this service, even at a 60-minute frequency.

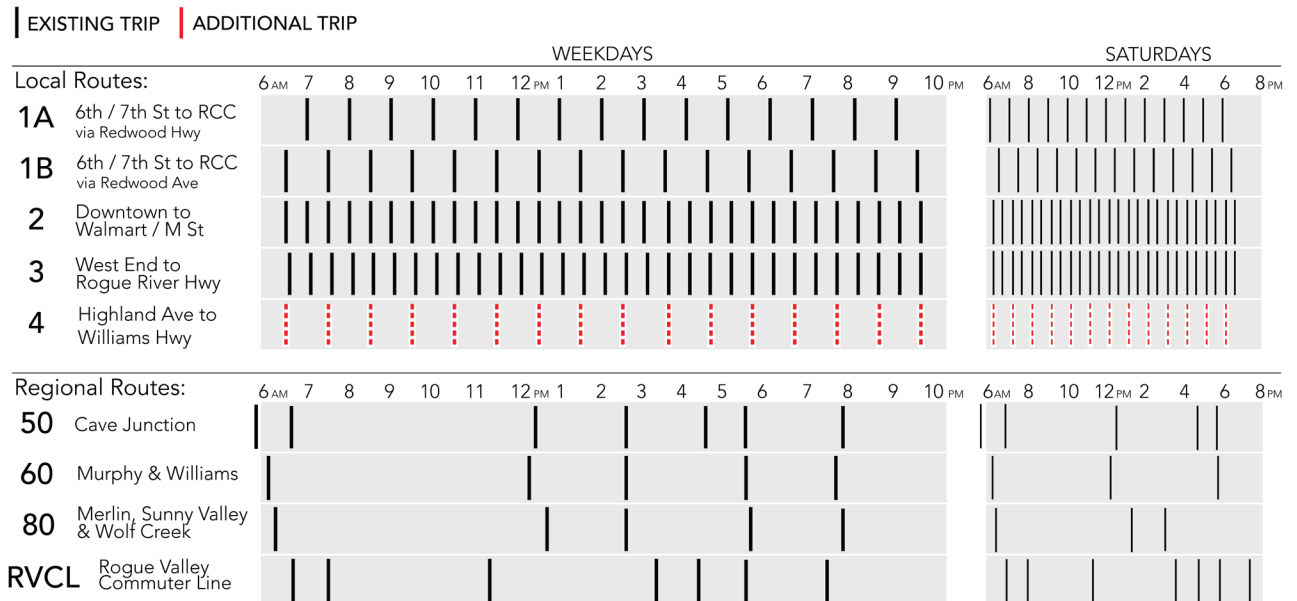


Figure 29: This graphic shows approximate departure times for each route from Downtown Grants Pass. Dashed red lines indicate potential new service on Route 4, if the route is implemented with service every 60 minutes.

Step 6. Route 4 on Highland Ave & Williams Hwy (cont'd)

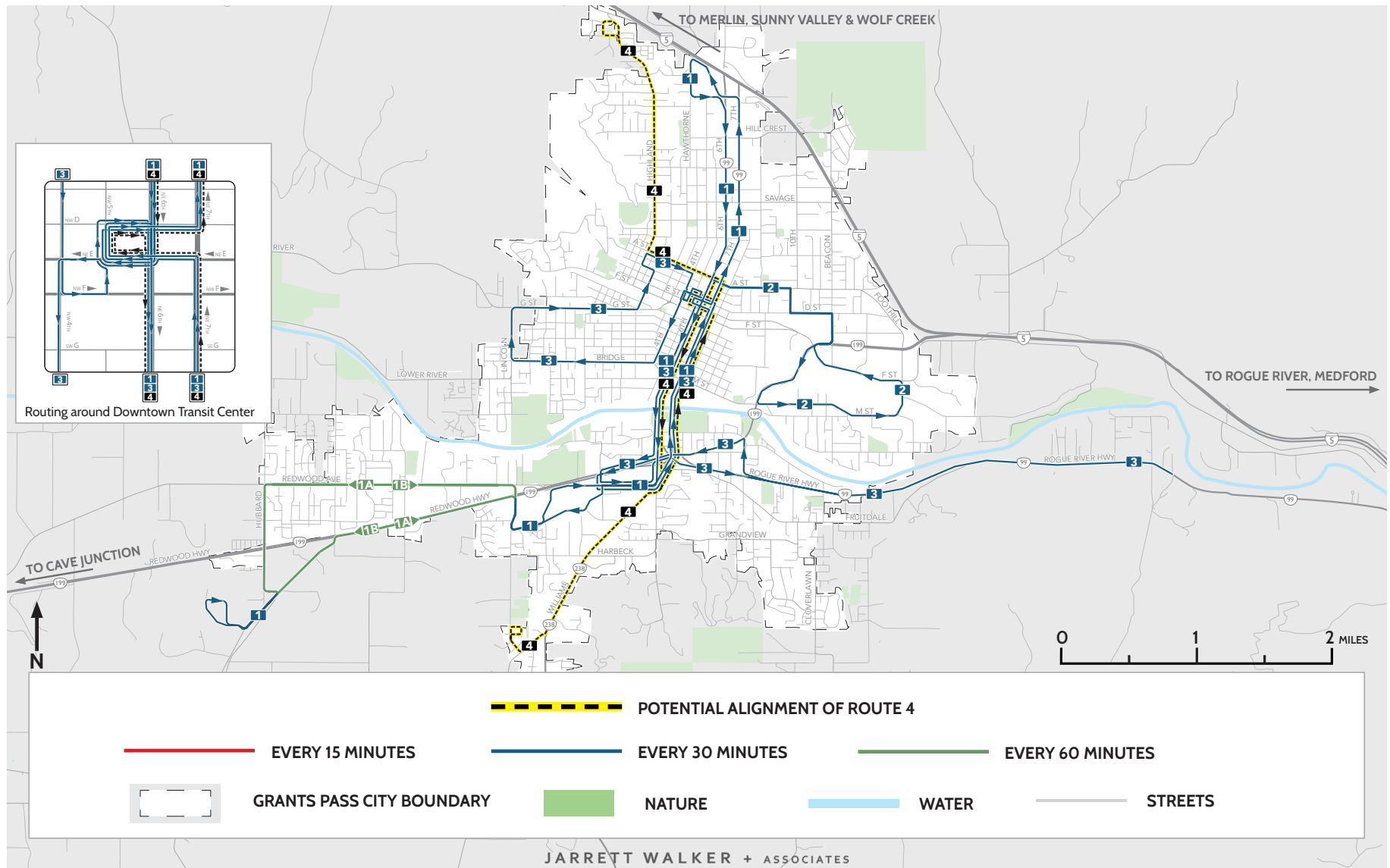


Figure 30: This map shows the most likely alignment of a future local Route 4 serving Highland Avenue and Williams Highway in Grants Pass. The northern and southern ends of the route are drawn based on the location of existing streets that would allow a bus to turn around. These termini should be reviewed closer to implementation.

Potential Long-Term Improvements (2031 - 2040)

The long-range environment for public transit contains many uncertainties. Nevertheless, the following opportunities should remain on the table for discussion.

DEDICATED LOCAL FUNDING SOURCE

Josephine Community Transit relies on State and federal grants for 90% of its operating funds. Dependence on State funds will likely grow as HB 2017 funds come online.

Given the long-term uncertainty around State and federal funds, JCT should continue to consider options to obtain dedicated local funding for transit service.

Based on analysis for the Existing Conditions and Choices Report, two options stand out:

- **Special District.** This would both provide funding and enable JCT to function independently of other agencies. It could be funded either through a property or payroll tax.
- **City utility fee.** The City of Grants Pass could assess a utility fee to its residents to fund a higher level of local service on local routes.

FREQUENT SERVICE

Routes 1 and 2 described in the Coverage Alternative serve the highest-demand segments of Josephine Community Transit's network. Downtown Grants Pass, Union Avenue, Rogue Community College, and the commercial area around Fred Meyer, Walmart and Big Lots.

To the extent funds become available for more frequent service, these are the routes where a 15-minute service has the most potential to generate significant ridership.

Operating service every 15 minutes would loosen the scheduling constraints on the network, since more frequent service reduces the need for exactly timed connections.

Another alternative would be to upgrade all local routes to 20 minutes. This would distribute the benefits of frequency more broadly, but would require much tighter scheduling to succeed.

LOCAL SERVICE TO NEW AREAS

Long-range land use plans suggest future development in Grants Pass will occur largely in three areas: outer Redwood, outer Highland Avenue to Grants Pass airport, and near Cloverlawn Road.

It will be worth considering new service to those areas as they are developed and as funds become available.

Nonetheless, JCT should consider whether the benefits of serving these new areas outweighs the benefits of increasing service on existing routes.