

# CITY OF CORCORAN

## 2020 ROAD MAINTENANCE PROJECT

### HIPL 5223(021)

#### CONTRACTOR NOTE

- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING SURVEY MONUMENTS, LOT STAKES (TAGGED), OR BENCH MARKS. ANY MONUMENT DISTURBED DURING CONSTRUCTION SHALL BE PERPETUATED PER THE PROFESSIONAL LAND SURVEYOR'S ACT, BUSINESS AND PROFESSIONS CODE 8771, AT THE CONTRACTOR'S EXPENSE.
- OMIT MICROSURFACING TREATMENT ON PCC PAVEMENT, DIRT PAVEMENT, DRAINAGE INLETS, CURB AND GUTTER, MEDIAN ISLANDS, MANHOLES, AND SURVEY MONUMENTS.
- THE CONTRACTOR IS REQUIRED TO INSTALL TEMPORARY MARKERS IMMEDIATELY AFTER SURFACING FOR THE SAFETY OF TRAFFIC.
- CONTRACTOR IS RESPONSIBLE FOR SUBMITTING A TEMPORARY TRAFFIC CONTROL PLAN TO THE CITY OF CORCORAN FOR APPROVAL.
- BEFORE APPLYING MICROSURFACING PAVEMENT TREATMENT, COVER MANHOLES, VALVE AND MONUMENTS COVERS, GRATES, OR OTHER EXPOSED FACILITIES LOCATED WITHIN THE AREA OF APPLICATION NEED TO BE COVERED IN PLACE USING PLASTIC OR OIL RESISTANT CONSTRUCTION PAPER SECURED BY TAPE OR ADHESIVE TO THE FACILITY BEING COVERED. REFERENCE THE COVERED FACILITIES WITH ENOUGH CONTROL POINTS TO RELOCATE THE FACILITIES AFTER APPLICATION OF THE SEAL COAT.
- NOTIFY THE ENGINEER OF ANY DISCREPANCIES BETWEEN THE PLANS AND THE PROJECT SITE.
- LIMITS OF WORK WILL BE MARKED IN THE FIELD BY THE CITY OF CORCORAN.
- CONTRACTOR SHALL FURNISH AND RE-INSTALL ALL EXISTING PAINTED TRAFFIC STRIPINGS FOR ENTIRE LENGTH OF PROJECT USING PAINT TRAFFIC STRIPE (2-COAT). REFER TO BID SHEET FOR APPROXIMATE QUANTITIES.
- MICROSURFACING TREATMENT TO EXTEND 10' BEYOND THE END OF RAMP AS SHOWN ON THE TRANSITION DETAIL.
- ALL EXISTING PAVEMENT MARKINGS (CROSSWALKS, STOP BARS, STOP MARKING LEGENDS, ECT.) SHALL BE REPAINTED WHEN DISTURBED BY TRANSITION MICROSURFACING.
- CONTRACTOR SHALL SCHEDULE CITY TESTING AT LEAST (5) WORKINGS DAYS IN ADVANCE PRIOR TO ANY PLACEMENT OF MICROSURFACING.
- CONTRACTOR IS RESPONSIBLE FOR PREPARING & SUBMITTING A WATER POLLUTION CONTROL PLAN (WPCP) TO THE ENGINEER FOR APPROVAL FOR ALL CONSTRUCTION ACTIVITIES PRIOR TO THE BEGINNING OF WORK, AND REVISED TO REMAIN CURRENT THROUGHOUT THE PROJECT.
- CONTRACTOR TO PROVIDE 2 WEEKS NOTICE (EXCLUDING WEEKENDS), 3 DAY NOTICE AND 24 HOUR NOTICE TO PROPERTY OWNERS AND RESIDENTS PRIOR TO COMMENCING CONSTRUCTION WORK, NOTIFICATION TO BE BY LETTER AND SHALL BE APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL RESTORE ALL FACILITIES OUTSIDE LIMITS OF WORK DAMAGE BY CONSTRUCTION OPERATION TO THEIR ORIGINAL CONDITION AT NO ADDITIONAL COST. NO MATERIAL MAY BE STORED IN PUBLIC RIGHT OF WAY.
- ALL EXISTING STREET FEATURES (MANHOLES, CLEAN-OUT, MONUMENTS, STRIPING, PAVEMENT MARKINGS, ETC.) SHALL BE TIED OUT AND REFERENCED BY THE CONTRACTOR PRIOR TO THE START OF ANY STREET WORK.
- SOME UTILITY LOCATING HAS BEEN PERFORMED DURING THE DESIGN OF THE PROJECT, HOWEVER THE PLANS MAY NOT SHOW ALL OF THE EXISTING UTILITIES. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE HORIZONTAL AND VERTICAL LOCATION OF EXISTING UTILITIES, INCLUDING SERVICE CONNECTIONS, WHICH THE CONTRACTOR BELIEVES MAY AFFECT OR BE AFFECTED BY THE WORK SHOWN HEREIN. THE CONTRACTOR SHALL FIELD VERIFY/POTHOLE EXISTING UTILITIES AS REQUIRED. PRIOR TO ANY DIGGING, CALL U.S.A. 811 A MINIMUM OF 48 HOURS IN ADVANCE OF EXCAVATION WORK SHALL START WITHIN FIVE DAYS AFTER THE ISSUANCE OF A USA INQUIRY IDENTIFICATION NUMBER, CONTRACTOR SHALL REMOVE U.S.A. MARKINGS WITHIN 2 WORKING DAYS AFTER COMPLETION OF EXCAVATION, BACKFILL AND SURFACE REPLACEMENT.
- CONTRACTOR SHALL, AT THE CONTRACTOR'S EXPENSE, RESTORE, REPAIR, AND RE-STRIPE EXISTING PAVEMENT MARKINGS OR STRIPING DAMAGE OR COATED WITH MATERIALS FROM THE CONTRACTOR'S OPERATION.
- TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE THE CONTRACTOR'S RESPONSIBILITY AND IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL SUBMIT A WRITTEN TRAFFIC CONTROL & SIGNING PLAN (INCLUDING STREET CLOSURE DETAILS) TO THE ENGINEER WITHIN TEN (10) WORKING DAYS AFTER AWARD OF CONTRACT.
- THE CONTRACTOR SHALL PROVIDE ALL LIGHTS, SIGNS BARRICADES, TEMPORARY RAMPS, FLAGMEN AND OTHER DEVICES TO PROVIDE VEHICULAR AND PEDESTRIAN SAFETY.
- CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES, AND SURVEY MONUMENTS WITHIN THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- RIGHT OF WAY LINES ARE SHOWN AT APPROXIMATE LOCATIONS.
- IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY LOCATION, ELEVATIONS, OF EXISTING FACILITIES AND TO IMMEDIATELY NOTIFY THE ENGINEER OF ANY FIELD CONFLICTS OR OMISSIONS.
- THE CONTRACTOR SHALL FURNISH A SELF-LOADING MOTOR SWEEPER WITH SPRAY NOZZLE AND OPERATE AT LEAST ONCE A DAY AND MORE AS NECESSARY TO CONTROL DUST CONDITIONS.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO RESIDENCES AND BUSINESSES THROUGHOUT THE LIFE OF THE CONTRACT.
- ANY DAMAGE TO THE EXISTING FACILITIES INCLUDING, BUT NOT LIMITED TO, TRESS, LANDSCAPING, IRRIGATION, FENCES, WALL, SIDEWALK, ANY OTHER PAVEMENT SURFACES SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE. CONTRACTOR SHALL RESTORE ANY AND ALL PAVEMENT AND OTHER FACILITIES OUTSIDE LIMITS OF WORK AFFECTED BY THE CONSTRUCTION OPERATIONS AT NO ADDITIONAL COST.
- BIDDERS SHOULD NOTE PRESENCE OF OVERHEAD UTILITIES IN THE WORK AREA ALL OVERHEAD UTILITIES MAY NOT BE SHOWN ANY IF SHOWN, MAY BE IN THEIR APPROXIMATE ALIGNMENT. AS PART OF THEIR PRE-BID INSPECTION, BIDDERS SHALL NOTE THE TYPE ANY LOCATION OF OVERHEAD UTILITIES IN THE PROPOSED WORK AREA BIDDERS PRICE SHALL INCLUDE PROVISIONS FOR WORKING IN AREAS WHERE OVERHEAD UTILITIES EXIST AT THE TIME OF BIDDING, WHETHER SHOWN ON THE PLANS OR NOT, AND NO ADDITIONAL COMPENSATION IS ALLOWED.

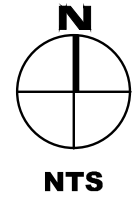
#### CONTRACTOR STRIPING NOTES

ALL EXISTING PAVEMENT STRIPING & MARKINGS (CROSSWALKS, STOP BARS, STOP MARKING LEGENDS, RAILROAD MARKINGS, CENTERLINE, ECT.) SHALL BE REPAINTED WHEN DISTURBED BY MICRO-SURFACING TREATMENT, UNLESS SPECIFIED OTHERWISE.



#### VICINITY MAP

CITY OF CORCORAN



#### CITY & ENGINEER CONTACTS

PUBLIC WORKS DIRECTOR

JOSEPH FAULKNER  
CITY OF CORCORAN  
832 WHITLEY AVE  
CORCORAN, CA 93212  
(559) 992-2151 EXT. 2210

CIVIL ENGINEER

JAVIER ANDRADE, PE, QSD  
A&M CONSULTING ENGINEERS  
220 N LOCUST AVE  
VISALIA, CA 93291  
(559) 429-4747

#### PROJECT UTILITY CONTACTS

PG&E  
JEFF HEIDNGER  
705 "P" ST., FRESNO, CA 93760  
(559) 263-7368

SOUTHERN CALIFORNIA GAS CO  
BOB WALKER  
404 N TIPTON STREET  
VISALIA, CA 93292  
(818) 701-2552

CITY OF CORCORAN WATER AND  
WASTEWATER  
JOSEPH FAULKNER  
832 WHITLEY AVE  
CORCORAN, CA 93212  
(559) 992-2151 EXT. 262

FRONTIER COMMUNICATION  
SOCRATES LUNA  
1359 "G" STREET, REEDLEY, CA 93664  
(559) 637-0666

#### INDEX OF PLANS

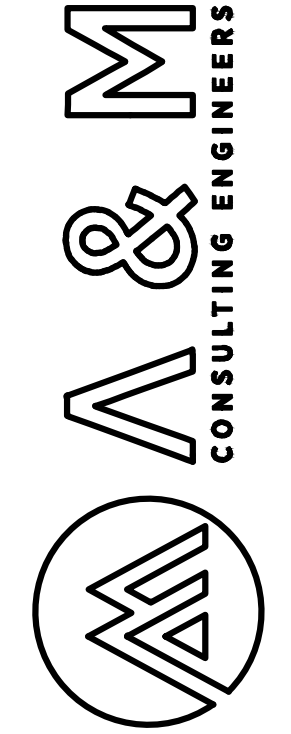
- COVER SHEET
- BASE BID IMPROVEMENT LOCATIONS
- BASE BID GRINDING LOCATIONS
- OTIS AVENUE SIGNING, STRIPING & MARKING PLAN
- OTIS AVENUE & ORANGE AVENUE INTERSECTION SIGNING, STRIPING & MARKING PLAN
- WHITLEY AVENUE SIGNING, STRIPING & MARKING PLAN
- SHERMAN AVENUE SIGNING, STRIPING & MARKING PLAN
- OREGON AVENUE SIGNING, STRIPING & MARKING PLAN
- STRIPING DETAILS

#### GENERAL CONSTRUCTION NOTES

- EXISTING UTILITIES AND EXISTING IMPROVEMENTS MAY BE SHOWN AT APPROXIMATED LOCATIONS DUE TO THE AVAILABLE RECORD INFORMATION AT THE TIME. OTHER UTILITY LINES MAY EXIST. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY ALL EXISTING UTILITIES BY POT HOLING OR LOCATING SERVICES (811) IF FOUND NECESSARY.
- CONTRACTOR SHALL NOTIFY ALL CORRESPONDING UTILITY COMPANIES AND CALL '811' WITHIN 2-3 WORKING DAYS BEFORE THE COMMENCEMENT OF ANY WORK WHICH MAY REQUIRE UTILITY VERIFICATION.
- IT IS THE REASONABILITY OF THE CONTRACTOR TO REVIEW THE FULL SET OF PLANS FOR ANY DISCREPANCIES AND OMISSIONS PRIOR TO THE COMMENCEMENT OF WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY WORK NOT IN CONFORMANCE WITH THE PLANS OR IN CONFLICT WITH ANY CODE.
- THE CONTRACTOR WILL NOTIFY THE CITY OF CORCORAN FOR ANY REPAIR/ POTHOLE WORK WHERE NECESSARY PRIOR TO START OF WORK.
- AN APPROVED SET OF PLANS MUST BE AVAILABLE ON THE JOB SITE AT ALL TIMES.
- THE CONTRACTOR SHALL POSSESS THE CLASS (OR CLASSES) OF LICENSE AS SPECIFIED IN THE 'NOTICE TO BIDDERS.'

#### TRAFFIC CONTROL NOTES

CONTRACTOR SHALL HAVE THE OPTION TO CLOSE DOWN ROAD SECTIONS AS NECESSARY TO AVOID ADDED EXPENSES FOR TRAFFIC CONTROL. CONTRACTOR SHALL SUBMIT TO THE ENGINEER A TRAFFIC CONTROL PLAN AND HAVE ENGINEER'S APPROVAL BEFORE COMMENCING WORK.



ENGINEERING PLANS FOR:  
2020 ROAD MAINTENANCE PROJECT

SHEET TITLE:

COVER SHEET

REVISIONS

NO.

SCALE: AS SHOWN

JOB NO: 210-011

ON/OC: GR

FILE: COVER\_SHEET.DWG

DATE: 1/25/2021

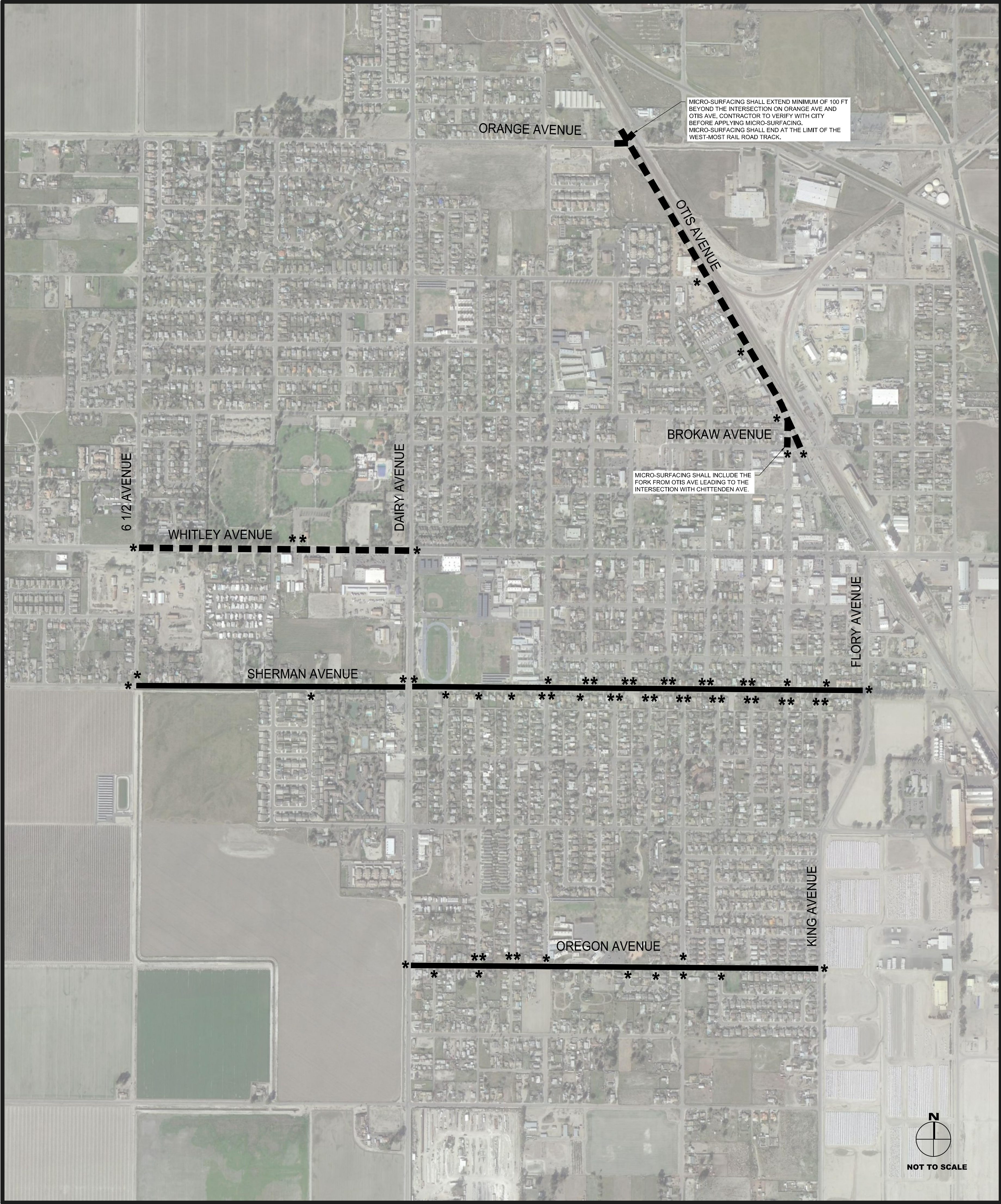


SHEET NO.

1

OF 9





ESTIMATED QUANTITIES				
ROAD	APPROX. LENGTH (FT)	AVERAGE WIDTH (FT)	AREA	
			SF	SY
BASE BID				
OTIS AVENUE	1615	34	54,910	6,101
	1865	60	111,900	12,433
WHITLEY AVENUE	2645	65	171,925	19,103
SHERMAN AVENUE	2606	34	88,604	9,845
	4383	48	210,384	23,376
OREGON AVENUE	3,950	38	150,100	16,678
TOTAL			787,823	87,536

MICRO-SURFACING SPREAD RATES		
TYPE	LOCATION	RANGE (LB OF DRY AGGREGATE/ SQ YD)
TYPE II, 0.15-0.20% FIBERIZED MICRO SURFACING (BLACK ROCK)	FULL LANE WIDTH	10 - 20
TYPE III, 0.15-0.20% FIBERIZED MICRO SURFACING (BLACK ROCK)	FULL LANE WIDTH	20 - 32

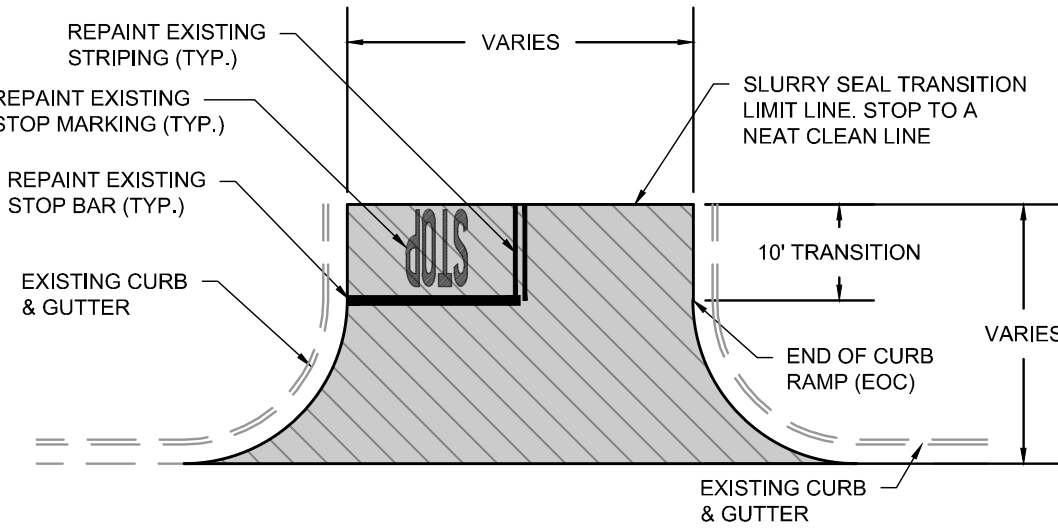
LEGEND

- LIMITS OF TYPE II OVER TYPE III (0.15 TO 0.20%) FIBERIZED MICRO-SURFACE PAVEMENT TREATMENT WITH BLACK ROCK
- LIMITS OF TYPE II (0.15 TO 0.20%) FIBERIZED MICRO-SURFACE PAVEMENT TREATMENT WITH BLACK ROCK
- \* SEE TYPICAL MICRO SURFACING TRANSITION DETAIL (THIS SHEET)
- \*\* SEE TYPICAL MICRO SURFACING LIMIT AT CROSS GUTTER DETAIL (THIS SHEET)

BASE BID DESCRIPTIONS

- BASE BID
- OTIS AVENUE FROM THE INTERSECTION OF ORANGE AND OTIS AVENUE TO BROKAW AVENUE
  - SHERMAN AVENUE FROM 6 1/2 AVENUE TO FLORY AVENUE
  - WHITLEY AVENUE FROM 6 1/2 AVENUE TO DAIRY AVENUE
  - OREGON AVENUE FROM DAIRY AVENUE TO KING AVENUE

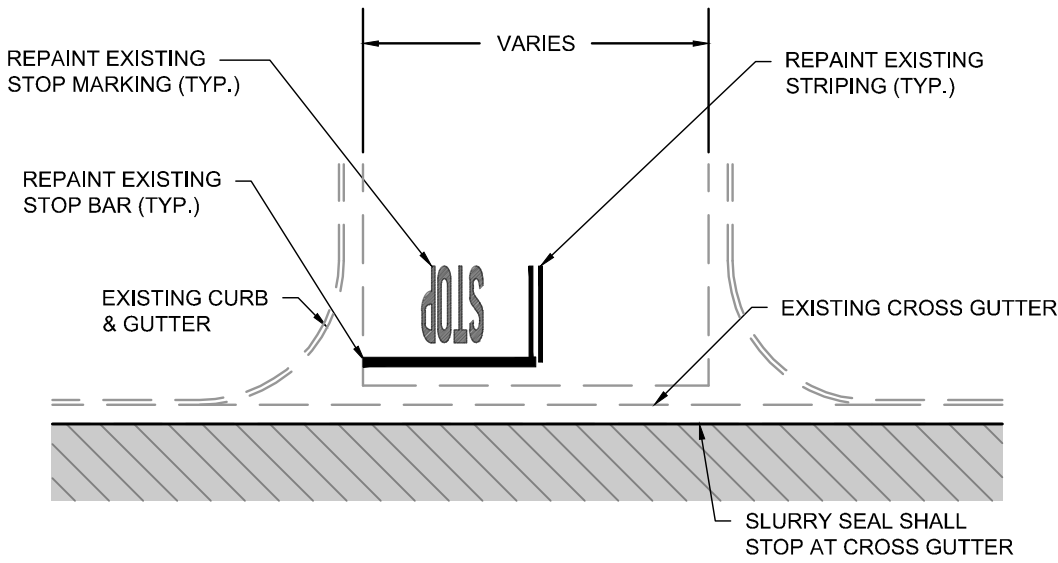
- NOTE:
- MIRCO SURFACING PAVEMENT TREATMENT TO EXTEND 10' BEYOND THE END OF RAMP AS SHOWN BELOW.
  - ALL EXISTING PAVEMENT STRIPING & MARKINGS (CROSSWALKS, STOP BARS, STOP MARKING LEGENDS, CENTERLINE, ECT.) SHALL BE REPAINTED WHEN DISTURBED BY SLURRY TREATMENT UNLESS SPECIFIED OTHERWISE ON PLANS.



TRANSITION DETAIL - PLAN VIEW

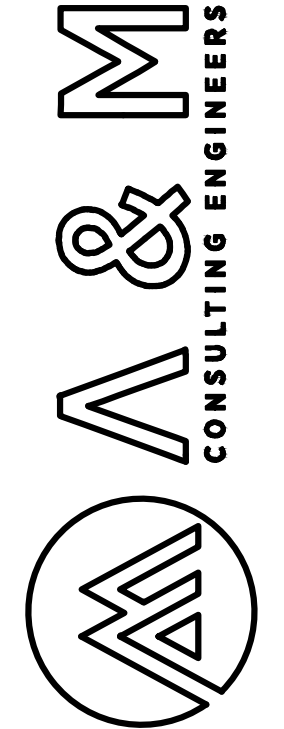
SCALE: N.T.S.

- NOTE:
- MIRCO SURFACING PAVEMENT TREATMENT TO STOP AT CROSS GUTTER AS SHOWN BELOW.
  - ALL EXISTING PAVEMENT STRIPING & MARKINGS (CROSSWALKS, STOP BARS, STOP MARKING LEGENDS, CENTERLINE, ECT.) SHALL BE REPAINTED WHEN ADJACENT TO CROSS GUTTER UNLESS SPECIFIED OTHERWISE ON PLANS.



CROSS GUTTER TRANSITION DETAIL - PLAN VIEW

SCALE: N.T.S.



ENGINEERING PLANS FOR:  
2020 ROAD MAINTENANCE PROJECT  
SHEET TITLE:  
BASE BID IMPROVEMENT LOCATIONS

REVISIONS

NO.

AS SHOWN  
JOB NO: 210-011  
ON/OC: GR  
FILE: COVER\_SHEET.DWG  
DATE: 1/25/2021



SHEET NO.



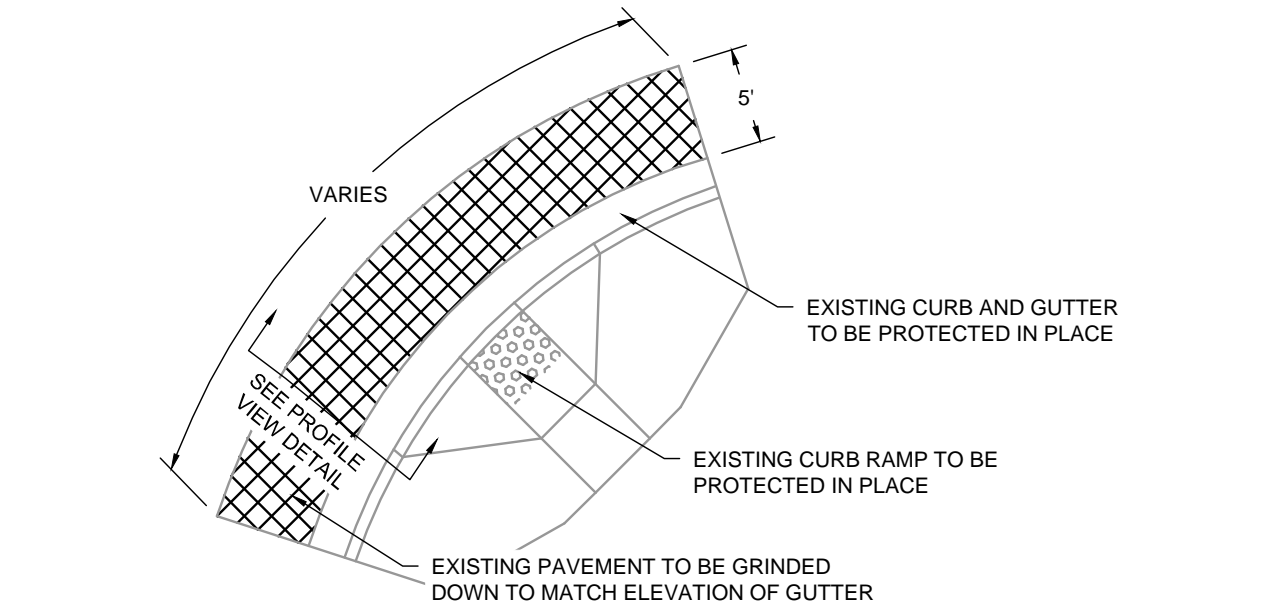


GRINDING NOTES

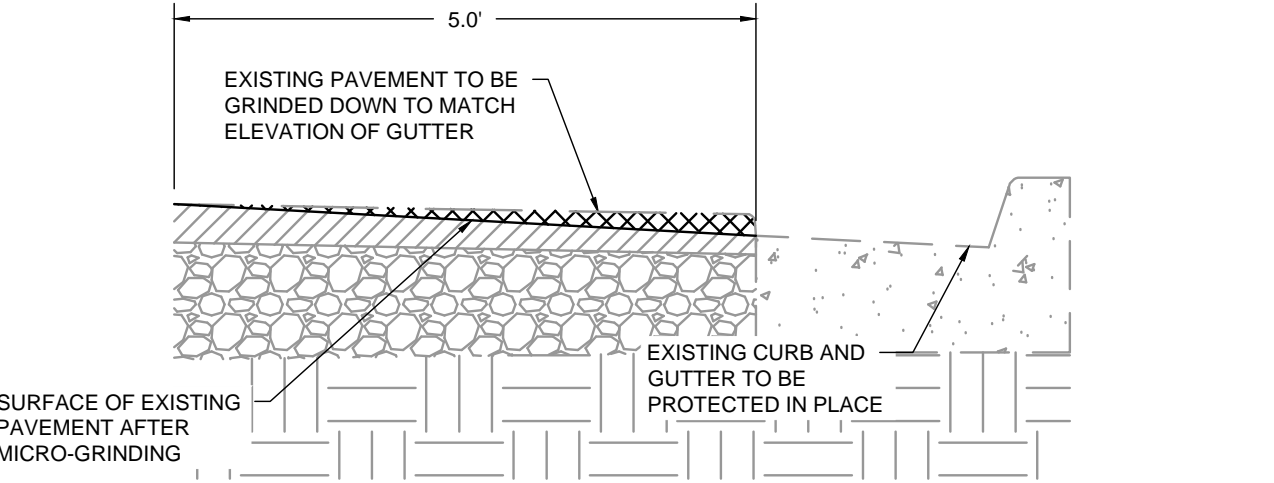
- 1. ALL MICRO GRINDING SHALL BE 5 FEET WIDE.
- 2. EXISTING PAVEMENT SHALL BE GRINDED TO BE FLUSH WITH EXISTING LIP OF GUTTER. SEE MICRO-GRINDING DETAILS (THIS SHEET)
- 3. LENGTH OF MICRO GRINDING SECTIONS SHALL BE FROM BEGINNING OF CURVATURE OF GUTTER PAN OF ROADWAY TO BE RESURFACED TO THE END OF CURVATURE OF THE GUTTER PAN ON THE INTERSECTING ROADWAY.
- 4. WHERE CROSS-GUTTERS EXIST, THE LENGTH OF THE MICRO-GRINDING SECTIONS SHALL BE FOR THE WIDTH OF THE INTERSECTION AND PARALLEL TO THE CROSS-GUTTER.

GRINDING LOCATION NOTES

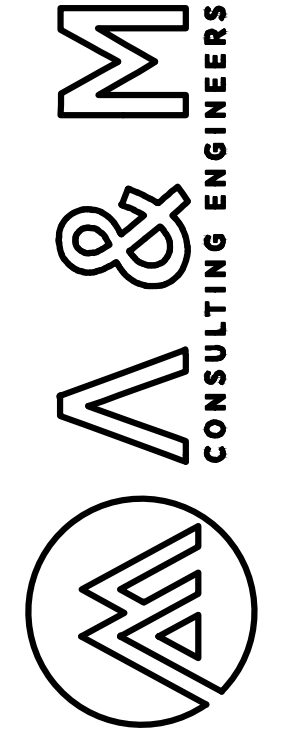
- 1 GRIND NORTHWEST AND NORTHEAST CORNERS OF DRIVE APPROACH OF ENTRANCE INTO HOUSING COMPLEX, PER GRINDING DETAILS (THIS SHEET)
- 2 GRIND NORTHEAST CORNER OF DRIVE APPROACH OF ENTRANCE INTO HOUSING COMPLEX, PER GRINDING DETAILS (THIS SHEET)
- 3 GRIND SOUTHEAST CORNER OF SHERMAN AVE & HUME LAKE AVE, PER GRINDING DETAILS (THIS SHEET)
- 4 GRIND NORTHEAST, & NORTHWEST CORNERS OF SHERMAN AVE & DAIRY AVE, PER GRINDING DETAILS (THIS SHEET)
- 5 GRIND SOUTHWEST CORNER AND ALONG CROSS GUTTER ON THE NORTH SIDE OF THE INTERSECTION OF SHERMAN AVE & JOSEPHINE AVE, PER GRINDING DETAILS (THIS SHEET)
- 6 GRIND NORTHEAST, SOUTHEAST, AND SOUTHWEST CORNERS OF SHERMAN AVE & LETTS AVE, PER GRINDING DETAILS (THIS SHEET)
- 7 GRIND NORTHEAST AND NORTHWEST CORNERS OF SHERMAN AVE & CHITTENDEN AVE, PER GRINDING DETAILS (THIS SHEET)
- 8 GRIND SOUTHEAST CORNER OF OREGON AVE & DAIRY AVE, PER GRINDING DETAILS (THIS SHEET)
- 9 GRIND SOUTHEAST AND SOUTHWEST CORNERS OF OREGON AVE & YOSEMITE DR, PER GRINDING DETAILS (THIS SHEET)
- 10 GRIND SOUTHEAST AND SOUTHWEST CORNERS OF OREGON AVE & YOSEMITE DR, PER GRINDING DETAILS (THIS SHEET)
- 11 GRIND NORTHEAST, SOUTHEAST, AND SOUTHWEST CORNERS OF OREGON AVE & VAN DORSTEN AVE, PER GRINDING DETAILS (THIS SHEET)
- 12 GRIND SOUTHEAST AND SOUTHWEST CORNERS OF OREGON AVE & CEDAR AVE, PER GRINDING DETAILS (THIS SHEET)
- 13 GRIND NORTHWEST CORNER OF OREGON AVE & KING AVE, PER GRINDING DETAILS (THIS SHEET)



TYP. RAMP GRINDING DETAIL (PLAN VIEW)  
NOT TO SCALE



TYP. GRINDING DETAIL (PROFILE VIEW)  
NOT TO SCALE

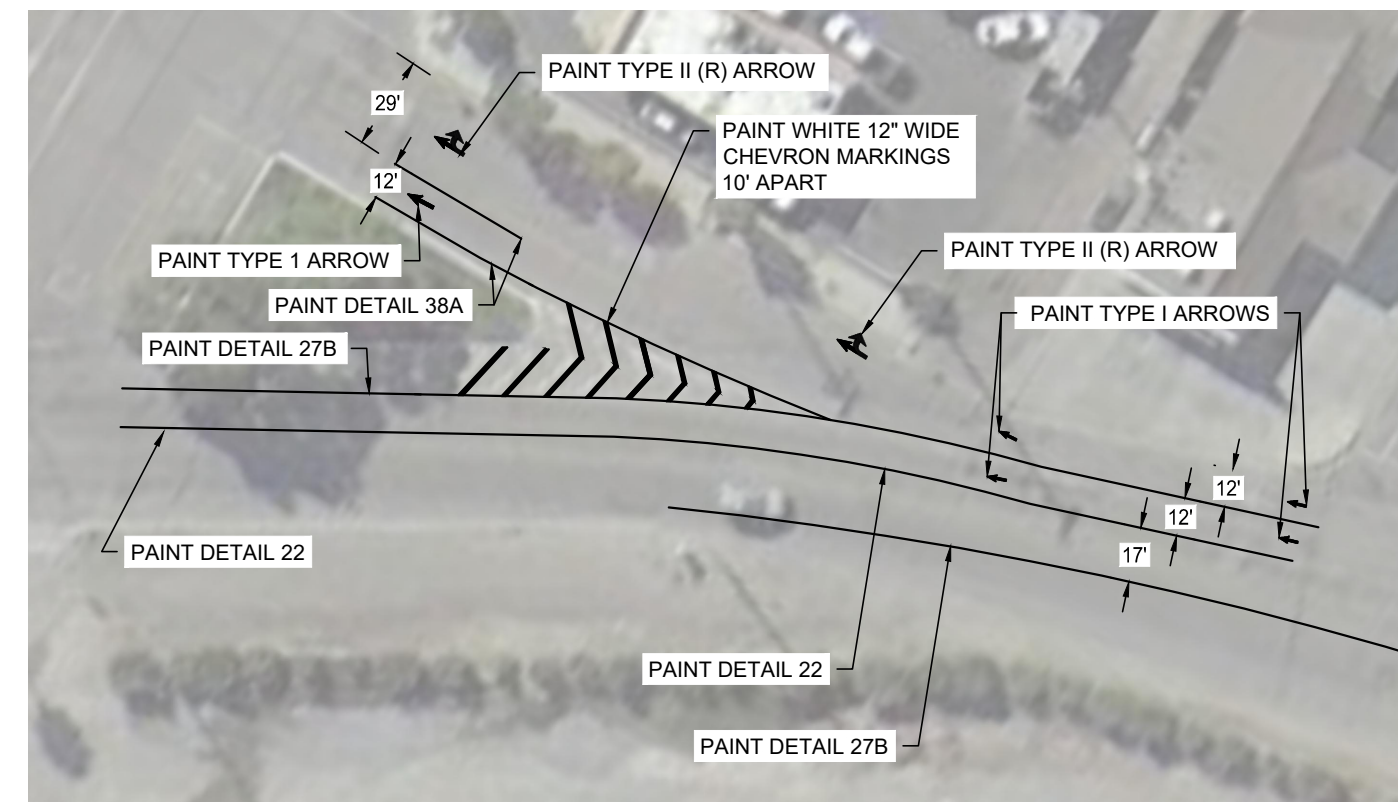


ENGINEERING PLANS FOR:  
**2020 ROAD MAINTENANCE PROJECT**  
SHEET TITLE:  
**BASE BID GRINDING LOCATIONS**

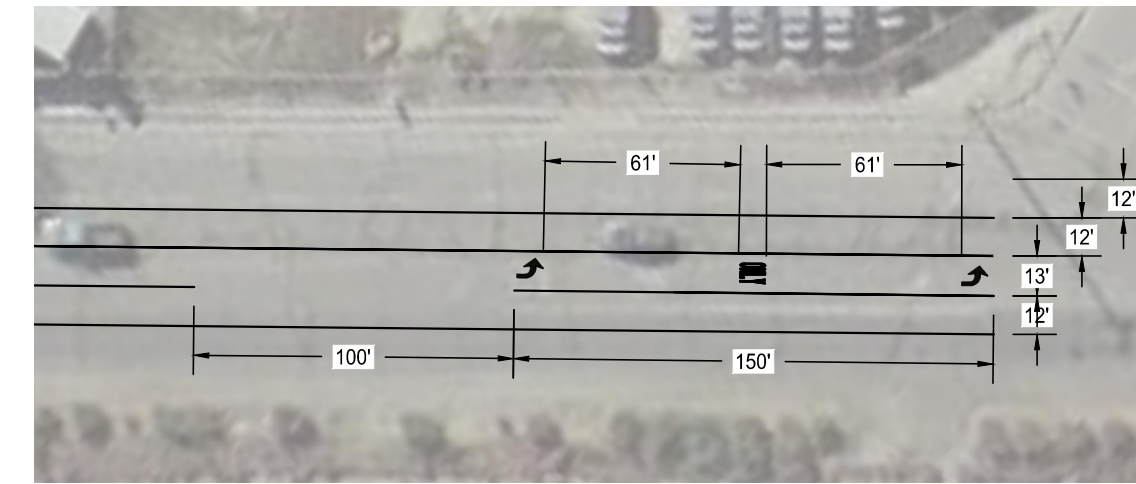
REVISIONS			
NO.	AS SHOWN	DATE	BY
1	JOB NO: 210-011	GR	GRINDING LOCATIONS.DWG
2	ON/OC:	FILE:	DATE:



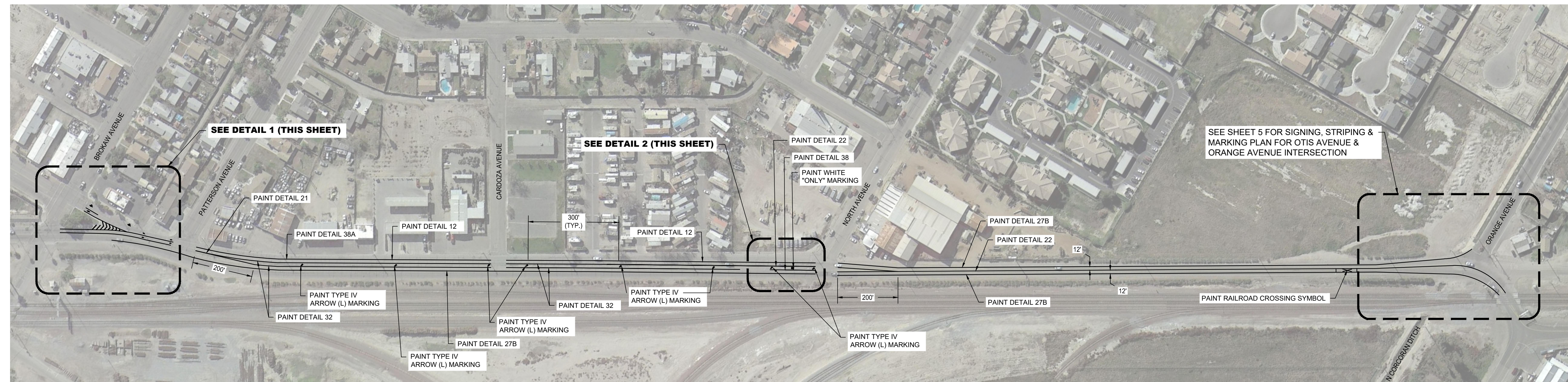




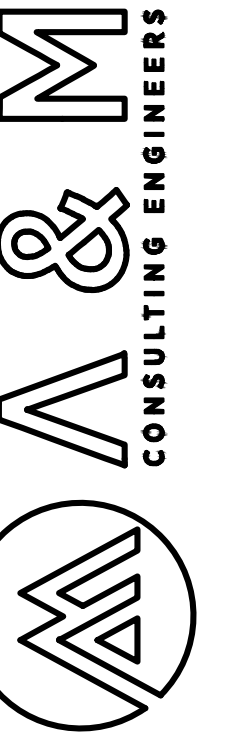
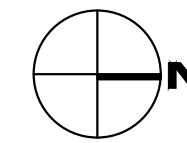
**DETAIL - 1**  
SCALE: N.T.S.



**DETAIL - 2**  
SCALE: N.T.S.



## OTIS AVENUE - PLAN VIEW



2020 ROAD MAINTENANCE PROJECT

SHEET TITLE:

OTIS AVENUE SIGNING, STRIPING & MARKING PLAN

JOB NO: 210-011  
QA/QC: GR  
FILE: SSM PLAN.DWG  
DATE: 1/25/2021



SHEET NO.

4

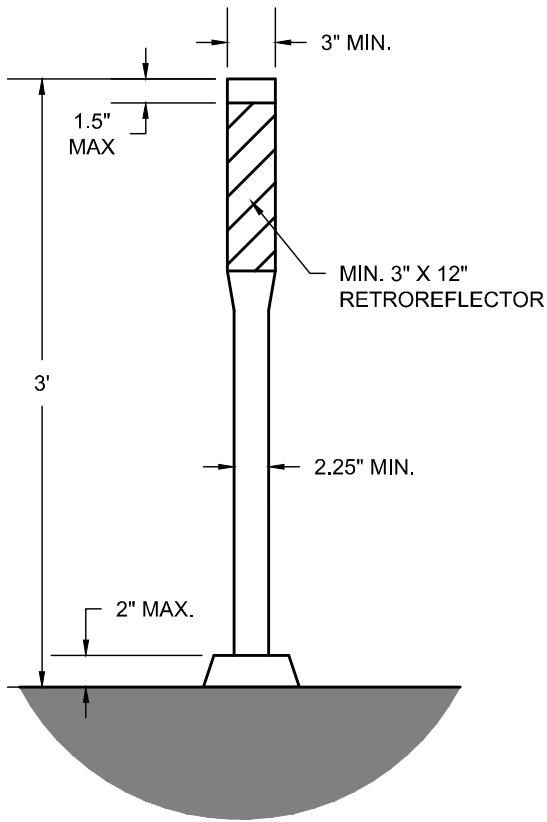
3



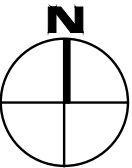


**SIGNING, STRIPING & MARKINGS NOTES**

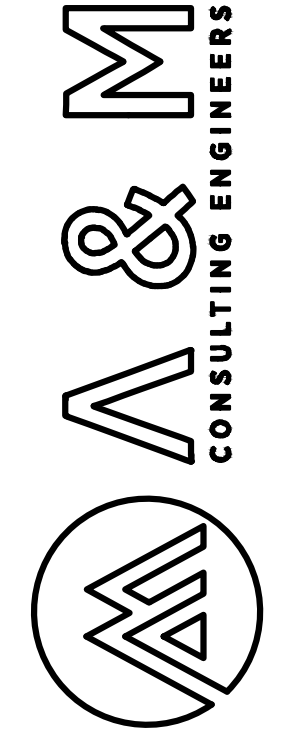
1. ALL WORK AND MATERIALS SHALL CONFORM TO CALTRANS STANDARD PLANS (2018 REVISED STANDARD PLAN RSP) AND THE CA 2014 MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS MUTCD (REVISION 5).
2. ANY CONTRACTOR PERFORMING WORK ON THIS PROJECT SHALL FAMILIARIZE HIMSELF WITH THIS SITE AND SHALL BE SOLELY RESPONSIBLE FOR ANY DAMAGE TO EXISTING FACILITIES RESULTING DIRECTLY OR INDIRECTLY FROM HIS OPERATIONS, WHETHER OR NOT SUCH FACILITIES ARE SHOWN ON THESE PLANS.
3. SIGNS NOT MOUNTED ON EXISTING POST(S) SHALL BE MOUNTED USING TELES PAR 20F12-2, 12 GAUGE POST(S) WITH QUICK PUNCH HOLES, TELES PAR 25K-12-S-2.4 ANCHORS AND TELES PAR TL-220 DRIVE NUTS OR EQUIVALENT.
4. NEW BIKE LANE STRIPING SHALL BE 5 FEET WIDE.
5. THE GAP BETWEEN THE LINES ON THE CROSSWALK SHALL NOT BE LESS THAN 6 FEET. THE GAP BETWEEN THE LINES ON THE YELLOW SCHOOL CROSSWALK SHALL BE 10 FEET WITH HIGH VISIBILITY 24\"/>



**DETAIL 1 - DELINEATOR**  
NOT TO SCALE



NOT TO SCALE

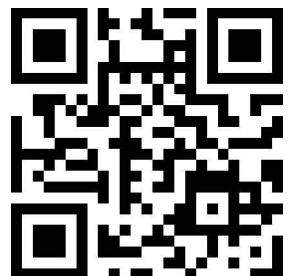


ENGINEERING PLANS FOR:  
**2020 ROAD MAINTENANCE PROJECT**  
SHEET TITLE:  
**OTIS AVENUE & ORANGE AVENUE INTERSECTION SIGNING, STRIPING & MARKING PLAN**

REVISIONS

NO.

AS SHOWN  
JOB NO: 210-011  
QA/QC: GR  
FILE: OTIS SSM PLAN.DWG  
DATE: 1/25/2021

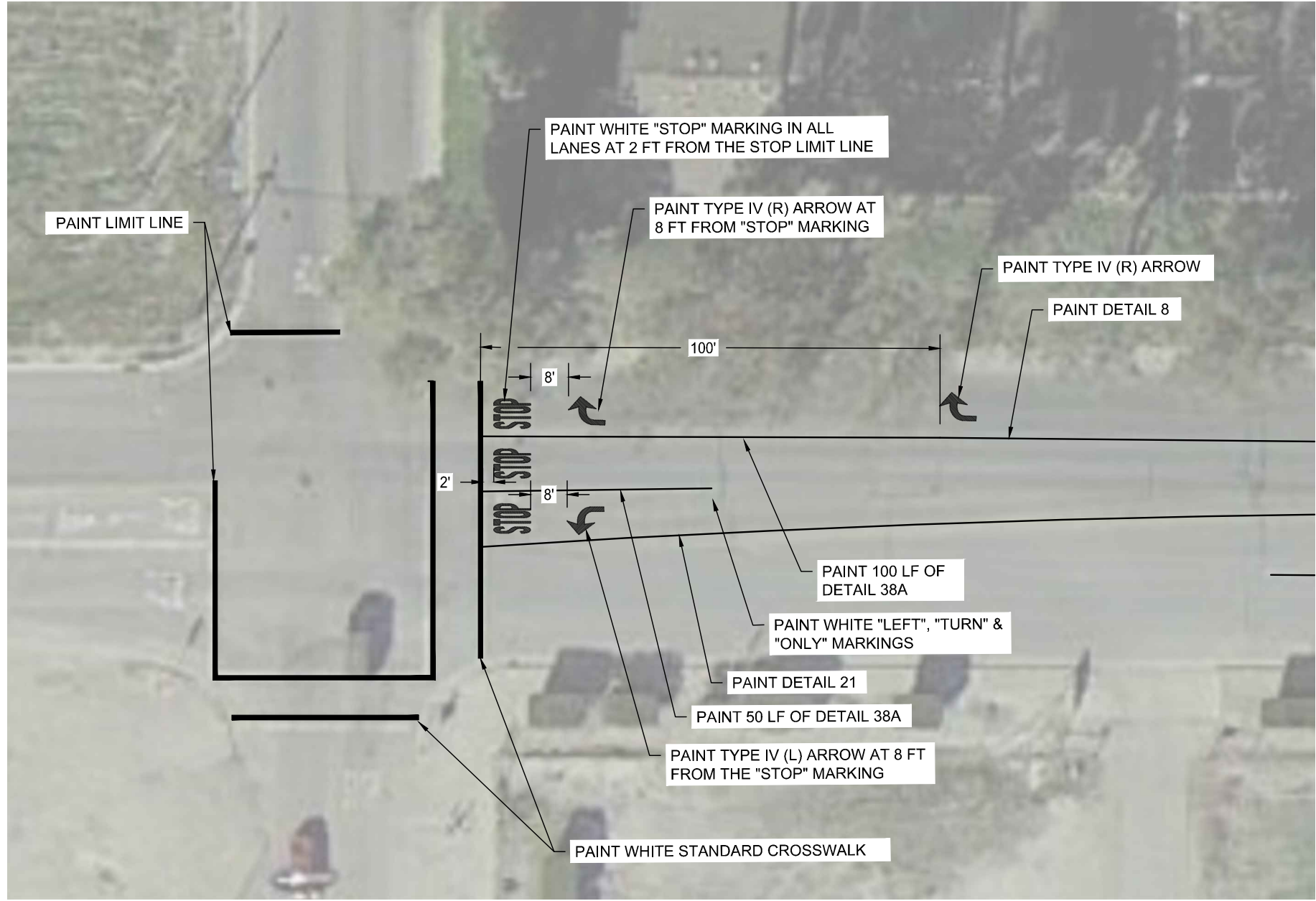


SHEET NO.

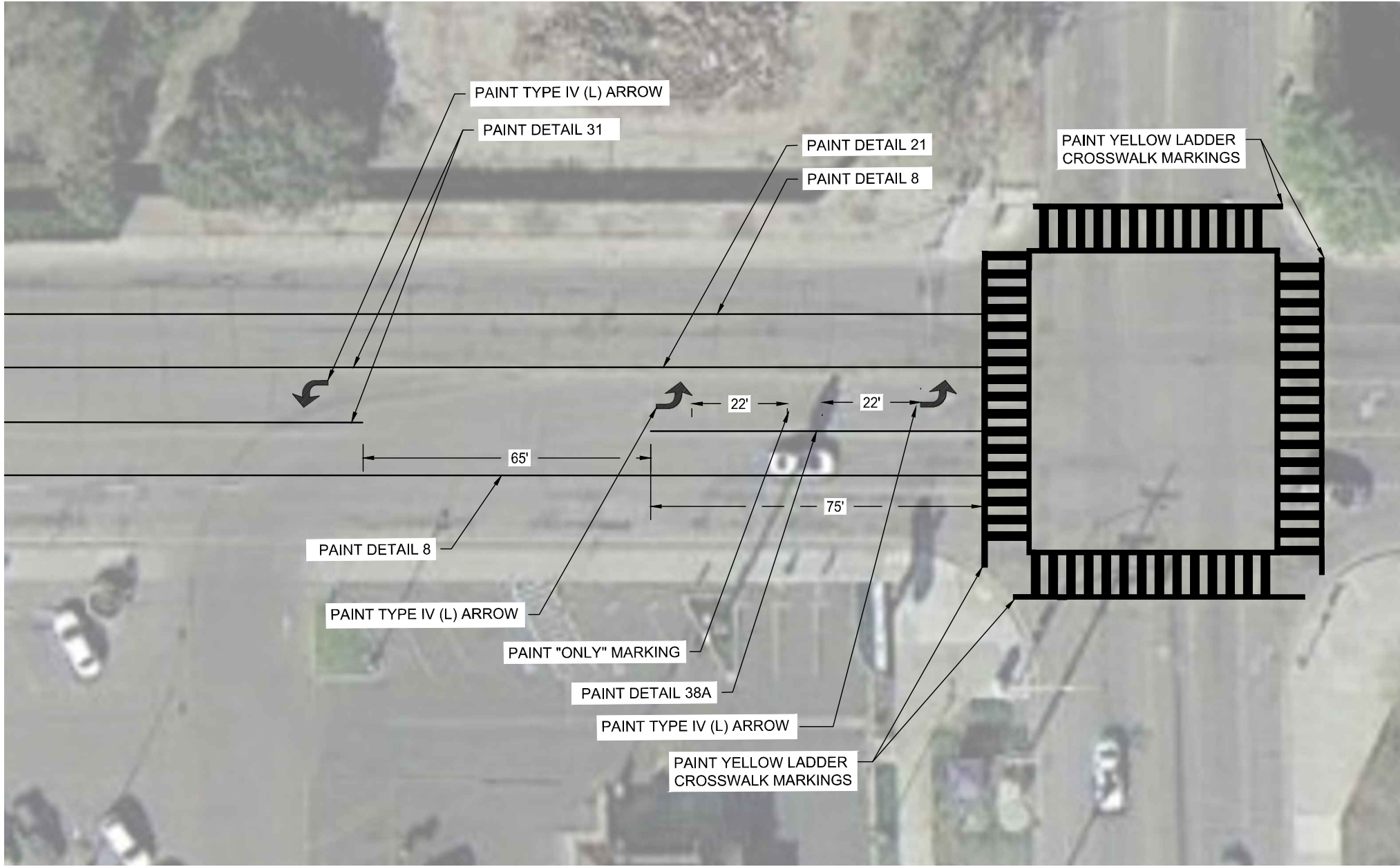
**5**

OF 9

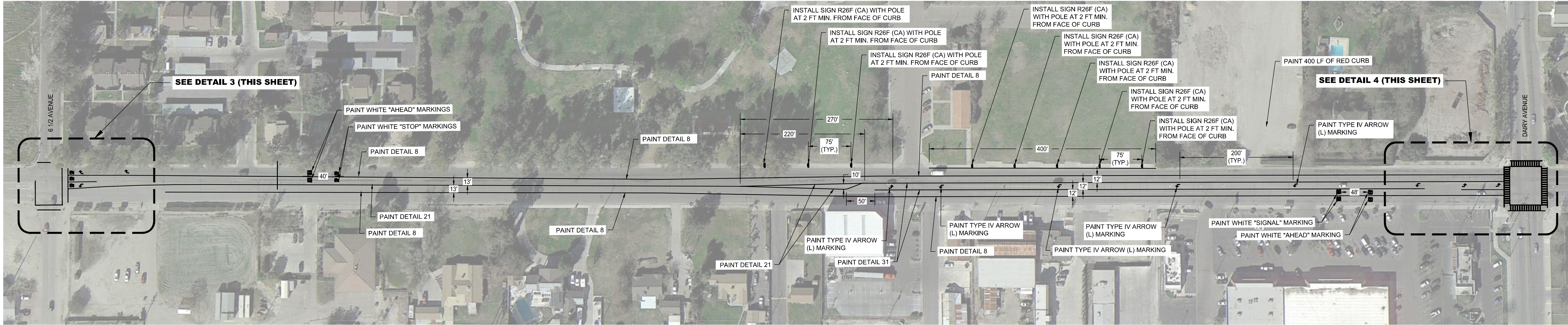




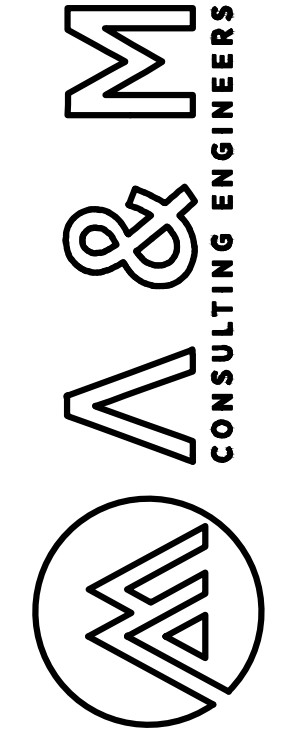
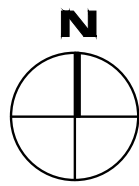
**DETAIL 3**  
SCALE: N.T.S.



**DETAIL 4**  
SCALE: N.T.S.



**WHITLEY AVENUE - PLAN VIEW**  
SCALE: N.T.S.



ENGINEERING PLANS FOR:  
**2020 ROAD MAINTENANCE PROJECT**  
SHEET TITLE:  
**WHITLEY AVENUE SIGNING, STRIPING & MARKING PLAN**

REVISIONS

NO.

SCALE: AS SHOWN  
JOB NO: 210-011  
ON/OC: GR  
FILE: SSM PLAN.DWG  
DATE: 1/25/2021



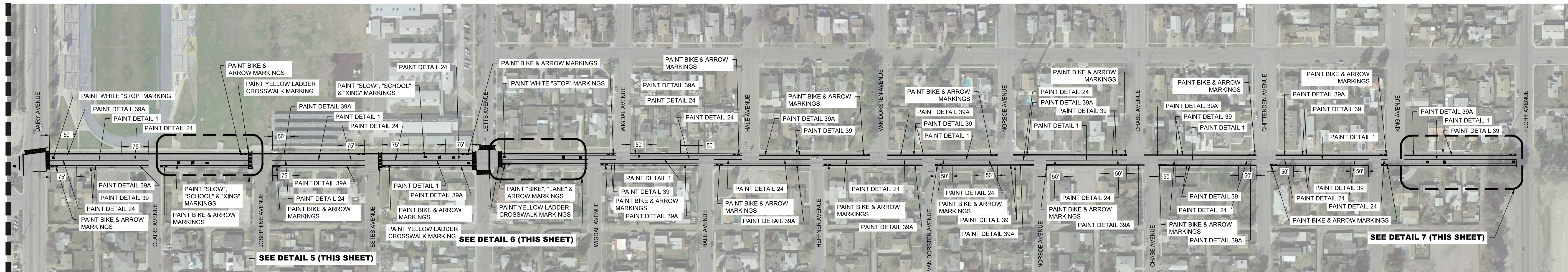
SHEET NO.

**6**

OF 9

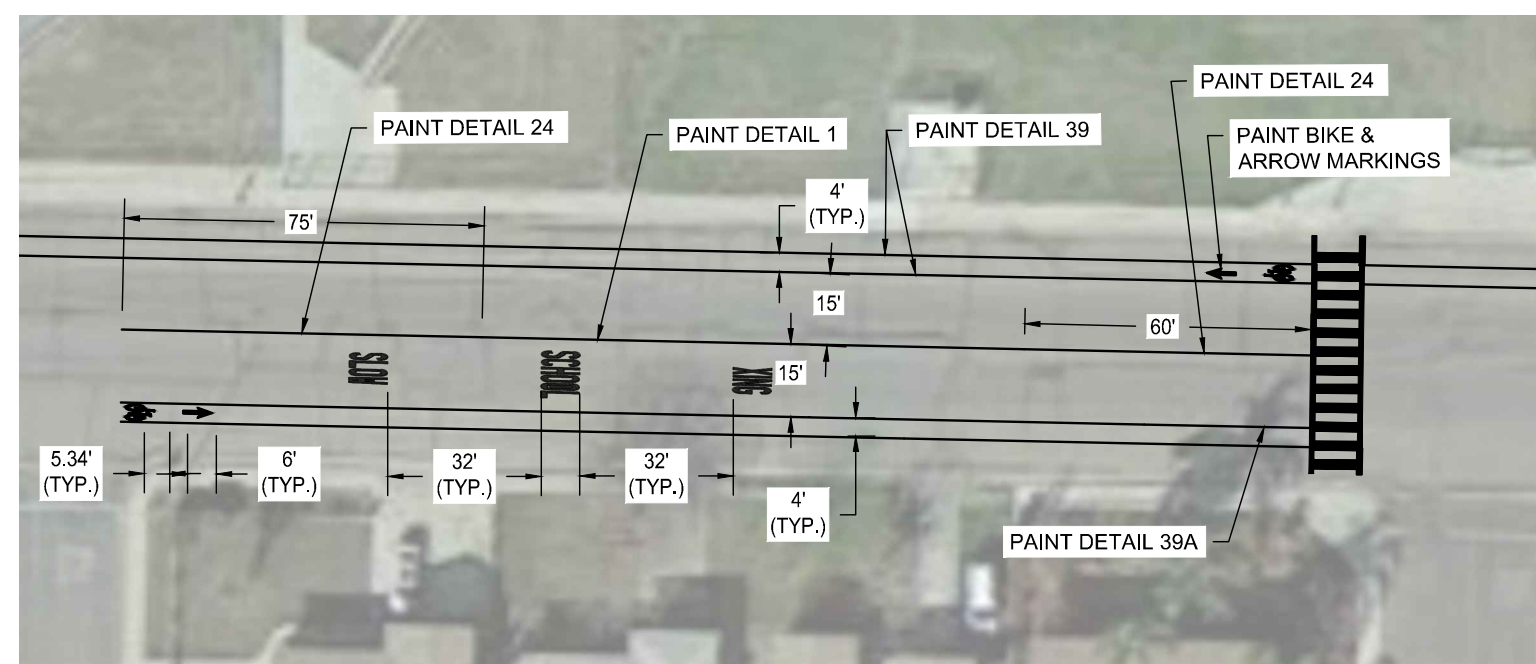
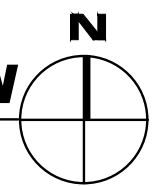


MATCH LINE  
SEE TOP RIGHT



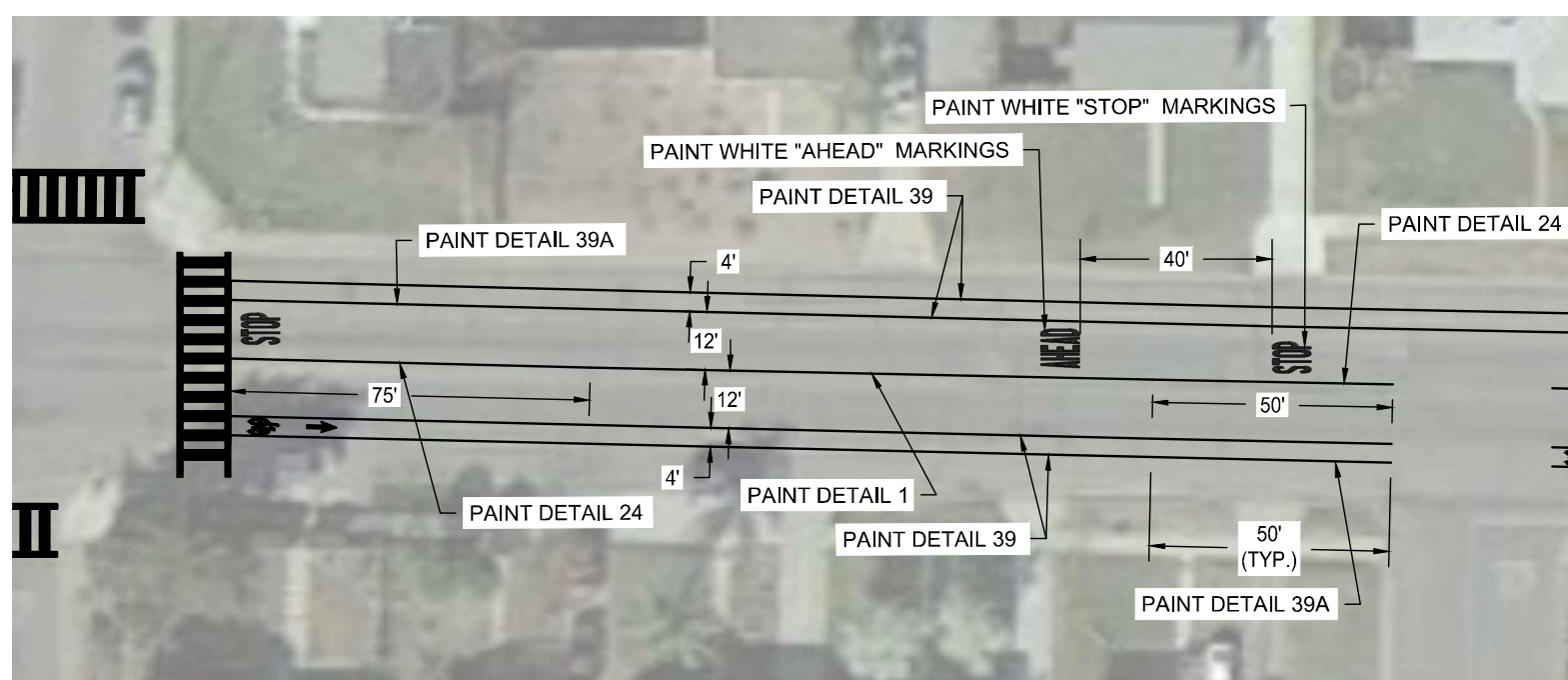
**SHERMAN AVENUE - PLAN VIEW**

SCALE: N.T.S.



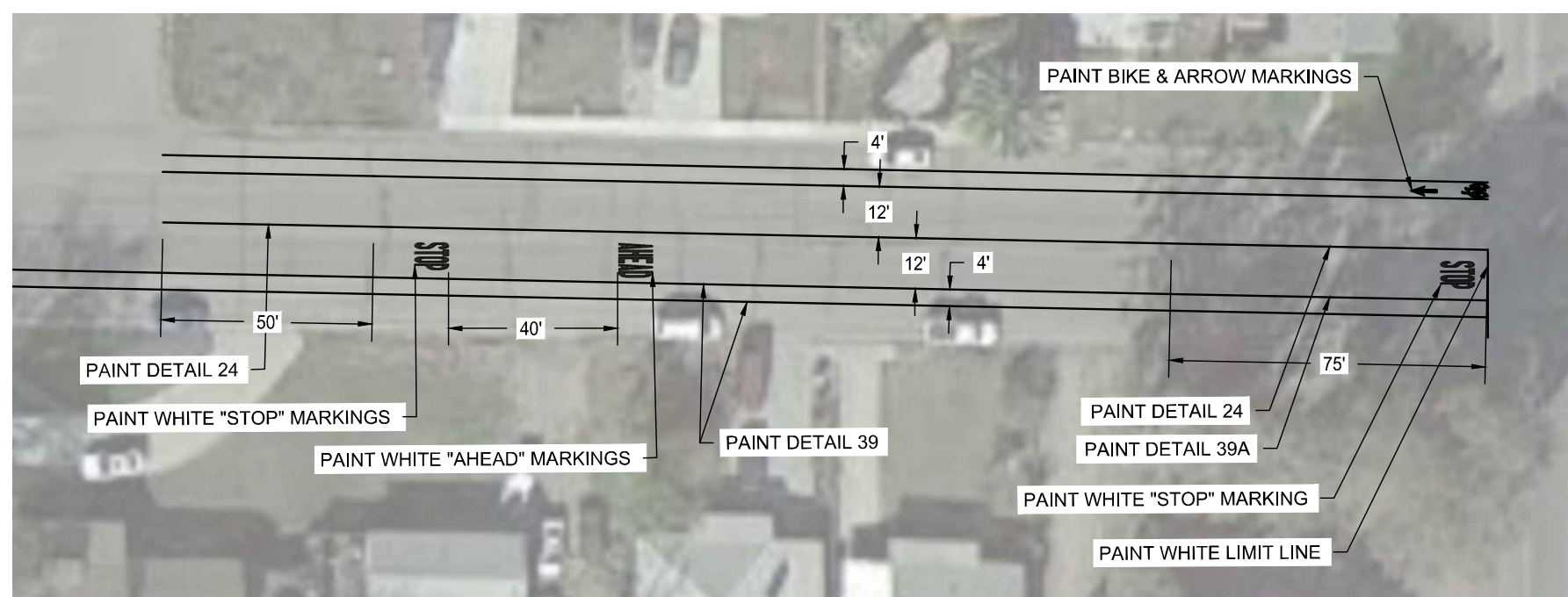
**DETAIL 5**

SCALE: N.T.S.



**DETAIL 6**

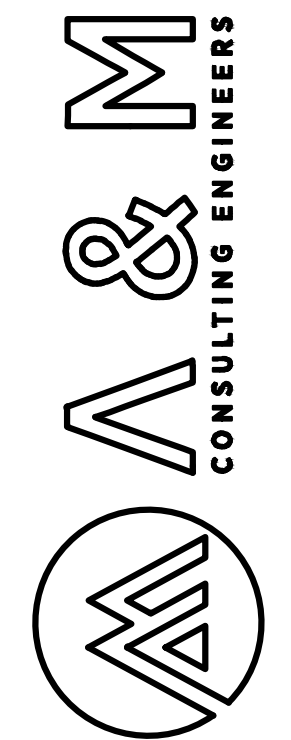
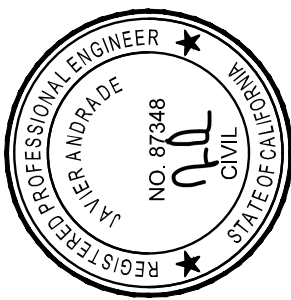
SCALE: N.T.S.



**DETAIL 7**

SCALE: N.T.S.

MATCH LINE  
SEE BOTTOM LEFT



ENGINEERING PLANS FOR:  
**2020 ROAD MAINTENANCE PROJECT**

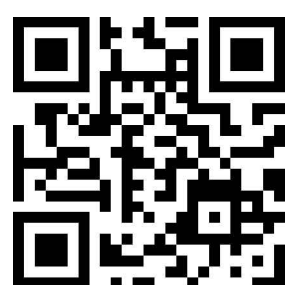
SHEET TITLE:

SHERMAN AVENUE SIGNING, STRIPING & MARKING PLAN

REVISIONS

NO.

AS SHOWN  
SCALE: JOB NO: 210-011  
ON/OC: GR  
FILE: SSM PLAN.DWG  
DATE: 1/25/2021

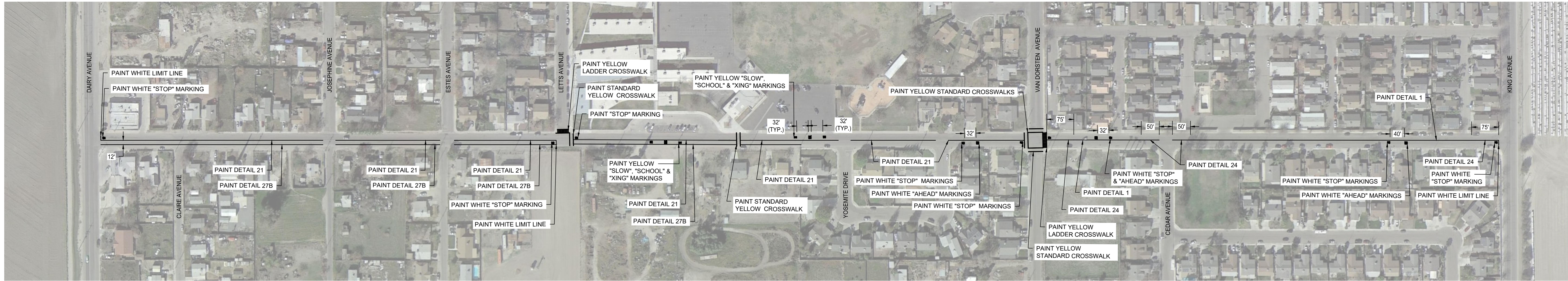


SHEET NO.

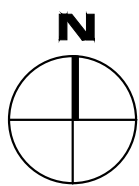
**7**

OF 9





**OREGON AVENUE - PLAN VIEW**  
SCALE: N.T.S.



SHEET NO.

8

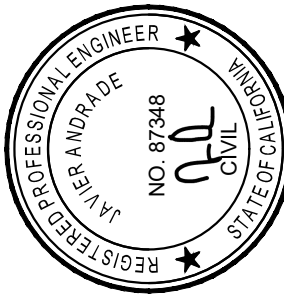
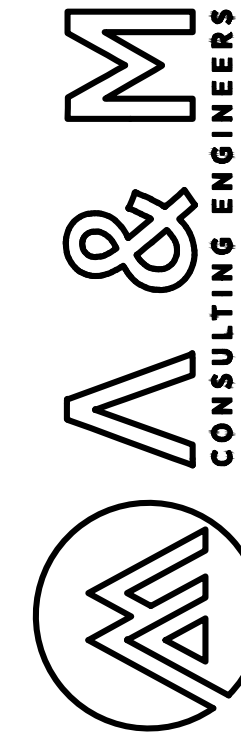
OF 9

SCALE: AS SHOWN  
JOB NO: 210-011  
QA/QC: GR  
FILE: SSM PLAN DWG  
DATE: 12/2/2021

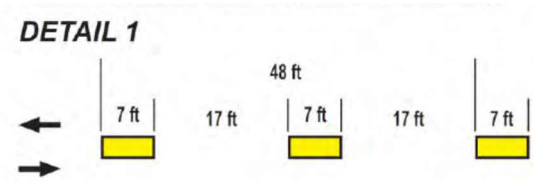
NO.

REVISIONS

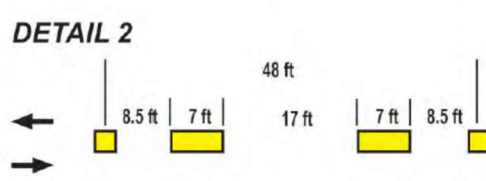
ENGINEERING PLANS FOR:  
**2020 ROAD MAINTENANCE PROJECT**  
SHEET TITLE:  
**OREGON AVENUE SIGNING, STRIPING & MARKING PLAN**



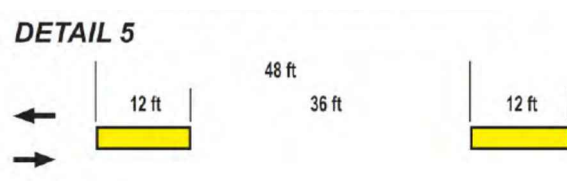




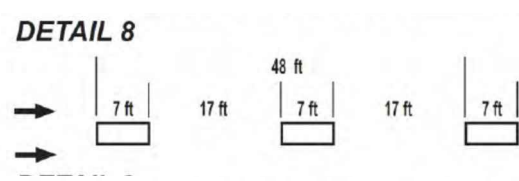
Centerline pattern for use on two-lane streets and highways (normally used on local streets and highways).



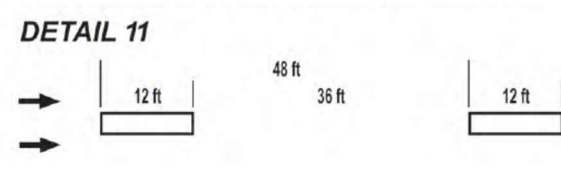
Centerline pattern with pavement markers for use on two-lane streets and highways.



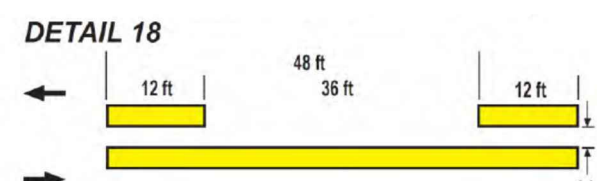
Centerline pattern for use on two-lane streets and highways (normally used on local streets and highways).



Lane Line pattern for use on multilane streets and highways (normally used on local streets and highways).



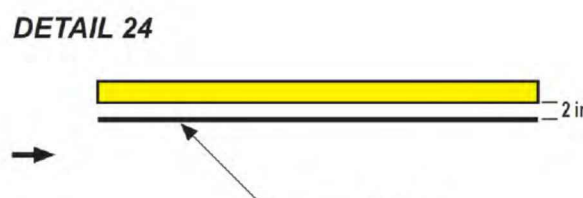
Lane Line pattern for use on multilane streets and highways (normally used on local streets and highways).



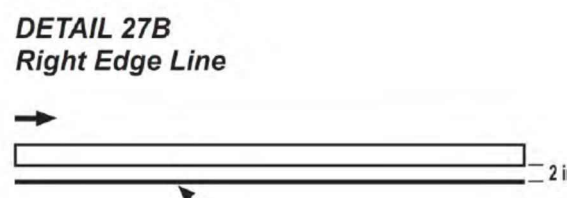
One direction no-passing pattern for use on two-lane streets and highways (normally used on local streets and highways). See Note 2.



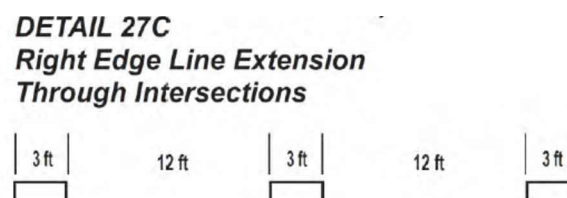
Two-direction no-passing pattern for use on two-lane streets and highways (normally used on local streets and highways). See Note 2.



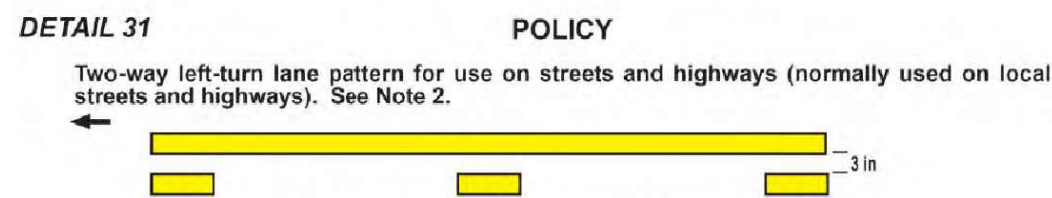
Left Edge Line pattern for use on streets and highways (normally used on local streets and highways).



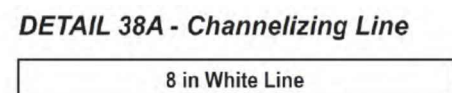
Right Edge Line pattern for use on all State highways may be used on local streets and highways. It is generally dropped at the beginning of the intersection flares on conventional highways. See also Detail 27C. On freeways, it may be flared in advance of the exit ramp as shown in Figure 3B-8 (CA).



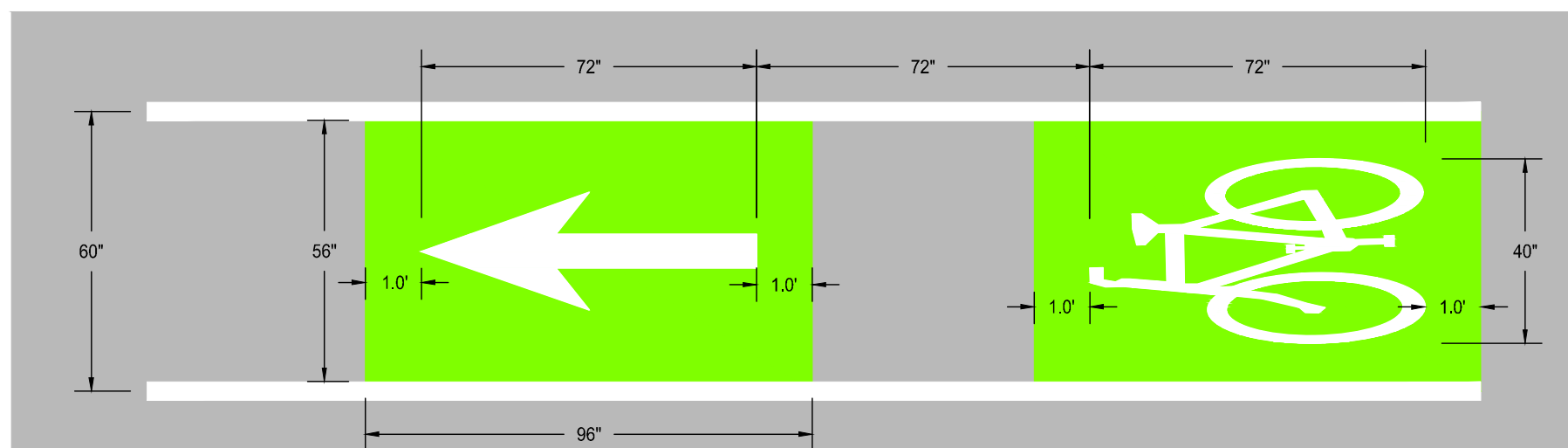
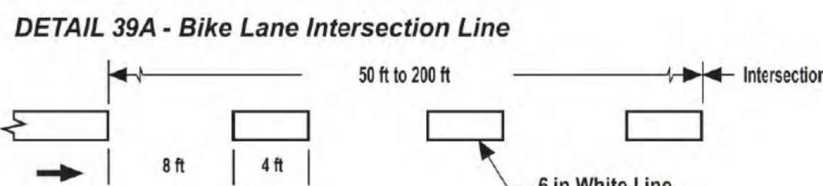
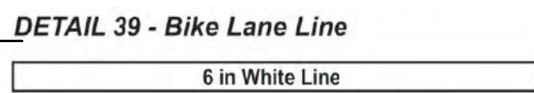
Right Edge Line Extension Through Intersections pattern for use to extend the right edgeline through an intersection where climatic conditions, such as areas of heavy fog, may require additional guidance.



NOTES: 1. Pavement markers shown off the solid line in Detail 32 may be placed on the line. 2. If the material used for centerline marking is paint, a 3 in black line shall be placed between the 4 in yellow lines on State highways and may be placed on streets and highways under local jurisdiction.

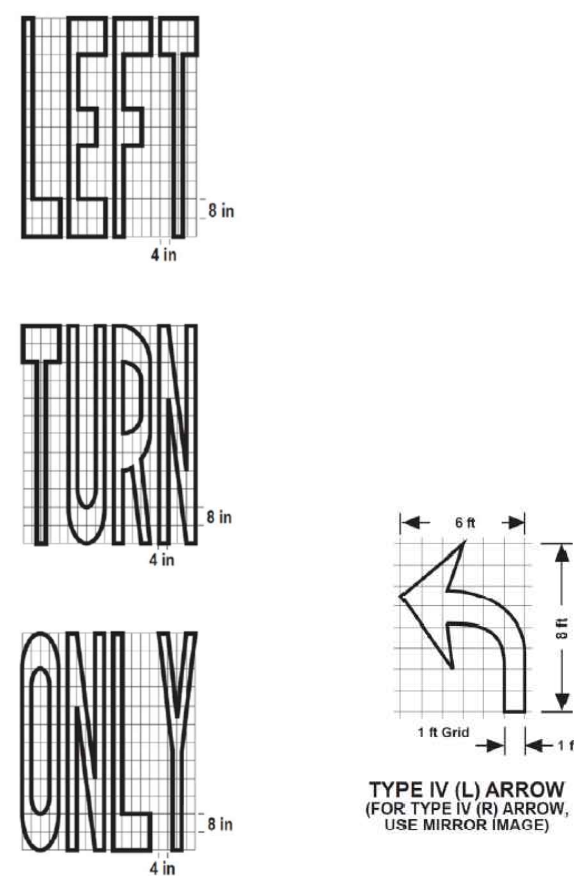


Typical channelizing line for use on Left-Turn or Right-Turn lanes on local streets and highways and freeway off-ramp terminals.



- NOTES:
1. PAINT AN 56" X 96" GREEN BACKGROUND FRAME BEFORE PAINTING BIKE LANE SYMBOL MARKINGS. BIKE LANE MARKINGS SHALL BE CENTERED IN THE FRAME AND SHALL EXTEND APPROX. 1' BEFORE AND AFTER THE MARKING. SEE FIGURE 9C-3 OF THE CA MUTCD.
  2. CONTRACTOR TO SUBMIT BACKGROUND FRAME COLOR SAMPLE FOR APPROVAL BY THE CITY BEFORE APPLYING ONTO ROADWAY.
  3. CONTRACTOR SHALL USE THERMOPLASTIC PAVEMENT MARKING PAINT FOR BIKE LANE SYMBOL MARKINGS AND GREEN BACKGROUND FRAMES.
  4. BIKE LANE LINES SHALL BE 6" WHITE LINES. SEE DETAIL 39 AND FIGURE 9C-101 (CA) OF CA MUTCD.

### BICYCLE LANE MARKING DETAIL



### LEFT TURN MARKING DETAIL

### LEGEND

- 4 in Yellow
- Two-Way Yellow Retroreflective Markers
- Direction of Travel
- Non-Retroreflective Yellow Markers
- 4 in White
- One-Way Clear Retroreflective Markers
- Direction of Travel
- Red-Clear Retroreflective Markers

