

# CITY OF CORCORAN

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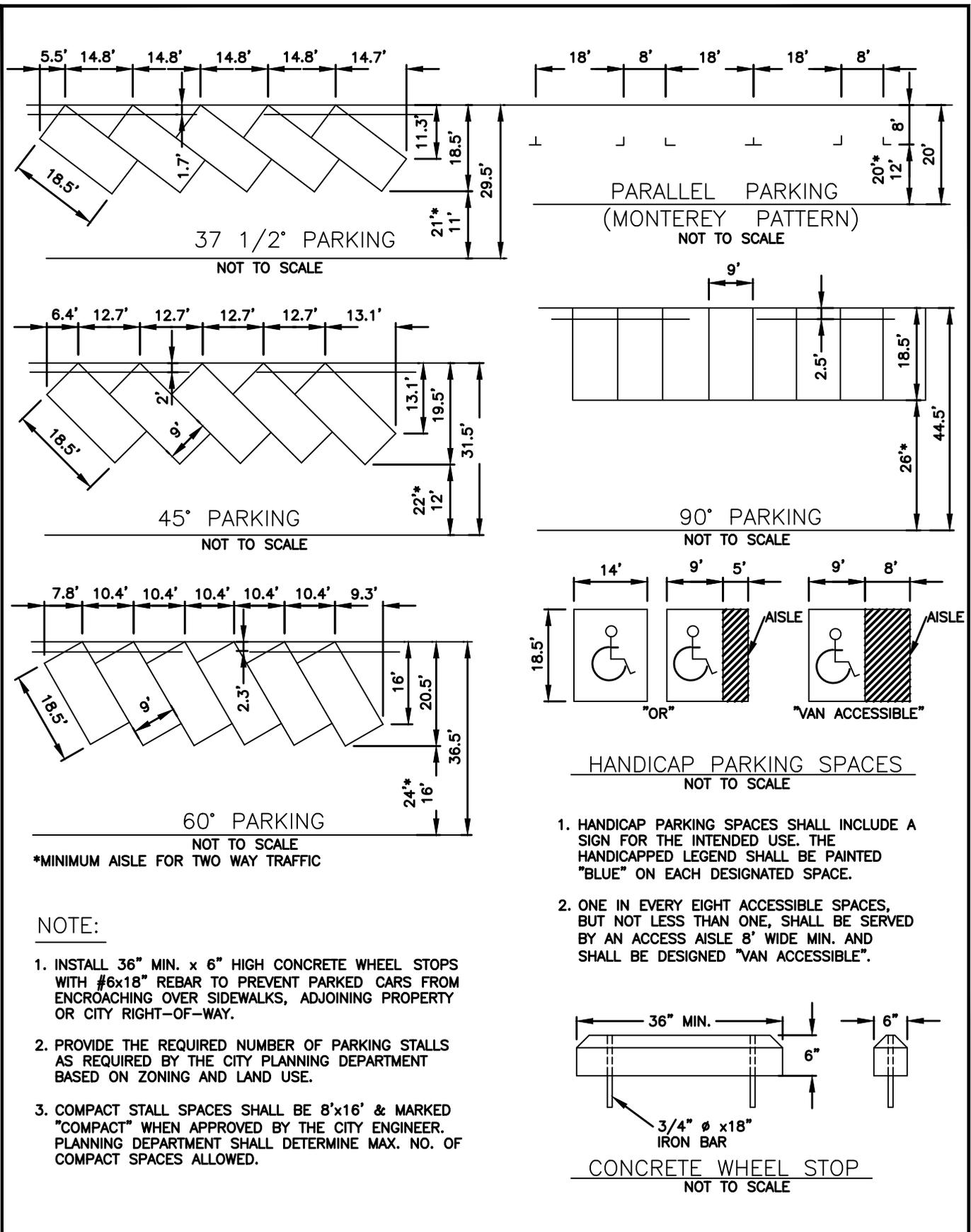
# IMPROVEMENT STANDARDS



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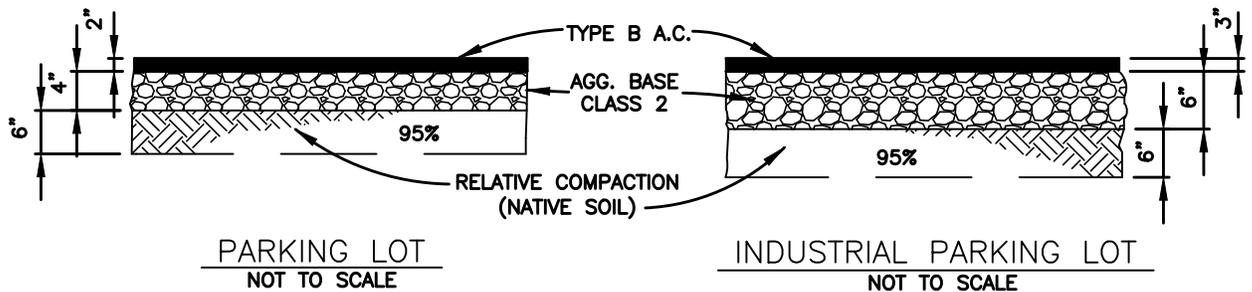
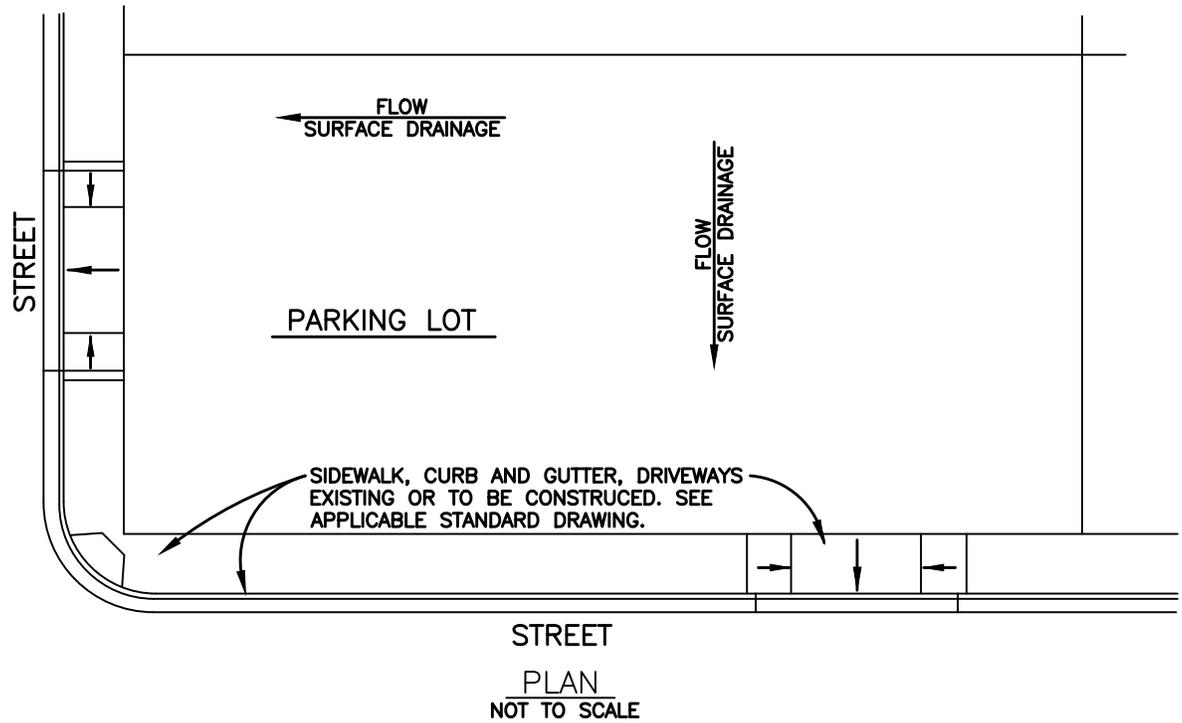
\*MINIMUM AISLE FOR TWO WAY TRAFFIC

**NOTE:**

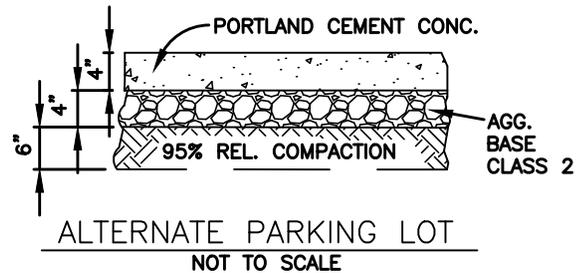
1. INSTALL 36" MIN. x 6" HIGH CONCRETE WHEEL STOPS WITH #6x18" REBAR TO PREVENT PARKED CARS FROM ENCRoACHING OVER SIDEWALKS, ADJOINING PROPERTY OR CITY RIGHT-OF-WAY.
2. PROVIDE THE REQUIRED NUMBER OF PARKING STALLS AS REQUIRED BY THE CITY PLANNING DEPARTMENT BASED ON ZONING AND LAND USE.
3. COMPACT STALL SPACES SHALL BE 8'x16' & MARKED "COMPACT" WHEN APPROVED BY THE CITY ENGINEER. PLANNING DEPARTMENT SHALL DETERMINE MAX. NO. OF COMPACT SPACES ALLOWED.

1. HANDICAP PARKING SPACES SHALL INCLUDE A SIGN FOR THE INTENDED USE. THE HANDICAPPED LEGEND SHALL BE PAINTED "BLUE" ON EACH DESIGNATED SPACE.
2. ONE IN EVERY EIGHT ACCESSIBLE SPACES, BUT NOT LESS THAN ONE, SHALL BE SERVED BY AN ACCESS AISLE 8' WIDE MIN. AND SHALL BE DESIGNED "VAN ACCESSIBLE".

<b>CITY of CORCORAN • Department of Public Works</b>	
Standard Drawing for:	APPROVED BY: <b>03/01/17</b> <small>City Engineer R.C.E 53350 Date</small>
<b>PARKING LOT REQUIRMENTS</b>	Revised: <span style="float: right;"><b>M-1</b></span>



1. A COMPLETE DRAINAGE PLAN AND PARKING STALL LAYOUT SHALL BE SUBMITTED TO THE CITY ENGINEERING DEPARTMENT FOR APPROVAL PRIOR TO ANY CONSTRUCTION.
2. MINIMUM SLOPE SHALL BE 1.0% FOR ASPHALT SURFACE AND 2% FOR ASPHALT CONCRETE FLOWLINES, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
3. PROVIDE ADEQUATE DRAINAGE THRU APPROVED CATCH BASINS AND PIPES TO THE NEAREST STREET GUTTER. FLOW OVER SIDEWALKS IS NOT PERMITTED. FLOW OVER DRIVE APPROACHES IS ACCEPTABLE.
4. SURFACE DRAINAGE TO ALLEY MAY BE ALLOWED BY CITY ENGINEER, WHEN IMPRACTICAL TO DRAIN OTHERWISE. IMPROVEMENTS TO ALLEY MAY BE REQUIRED.
5. WEED KILLER SHALL BE APPLIED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
6. STRUCTURAL SECTIONS SHOWN ABOVE ARE MINIMUM THICKNESS. ACTUAL SECTIONS TO BE DETERMINED BY AN ACCEPTED FLEXIBLE PAVEMENT DESIGN METHOD.

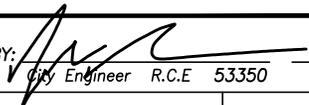


PARKING LOTS CONTINUALLY USED BY REFUSE & OTHER TYPES OF HEAVY TRUCKS SHALL REQUIRE ADDITIONAL STRUCTURAL STRENGTH. CONCRETE PAVEMENTS SHALL BE DESIGNED TO CONFORM WITH AMERICAN CONCRETE INSTITUTE (ACI) RECOMMENDATIONS.

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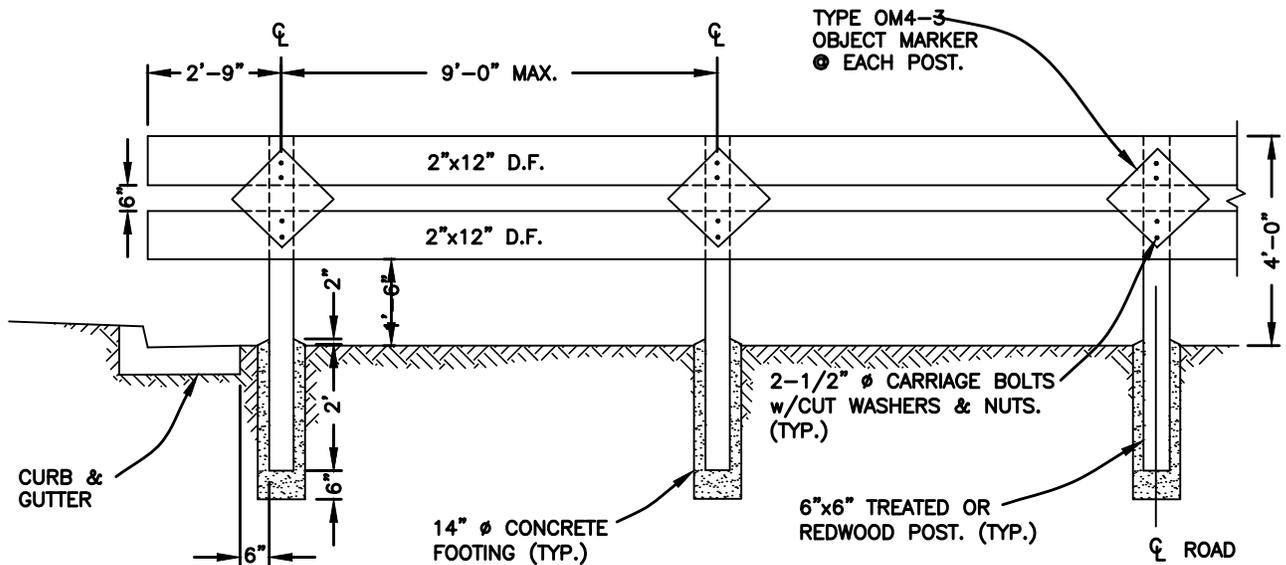
Standard Drawing for:

**PARKING LOT  
REQUIRMENTS**

APPROVED BY:  **03/01/17**  
City Engineer R.C.E 53350 Date

Revised:

**M-2**



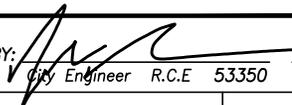
NOTES:

1. THE BARRICADE MUST EXTEND ACROSS THE FULL WIDTH OF PAVEMENT.
2. WOOD PRESSURE TREATED WITH A PRESERVATIVE MAY BE SUBSTITUTED FOR REDWOOD MATERIAL.
3. BARRICADES SHALL BE PAINTED WITH 2 COATS OF COMMERCIAL QUALITY WHITE BEADED ENAMEL. MARKINGS ON BARRICADE RAILS SHALL BE CALIFORNIA DEPT. OF TRANSPORTATION OM4-3 OBJECT MARKER (18"x18" RED REFLECTIVE SIGN WITH BLACK BORDER) ON EACH POST.
4. CONCRETE SHALL BE MINIMUM FIVE SACK MIX. (3200 P.S.I. MIN. IN 28 DAYS).

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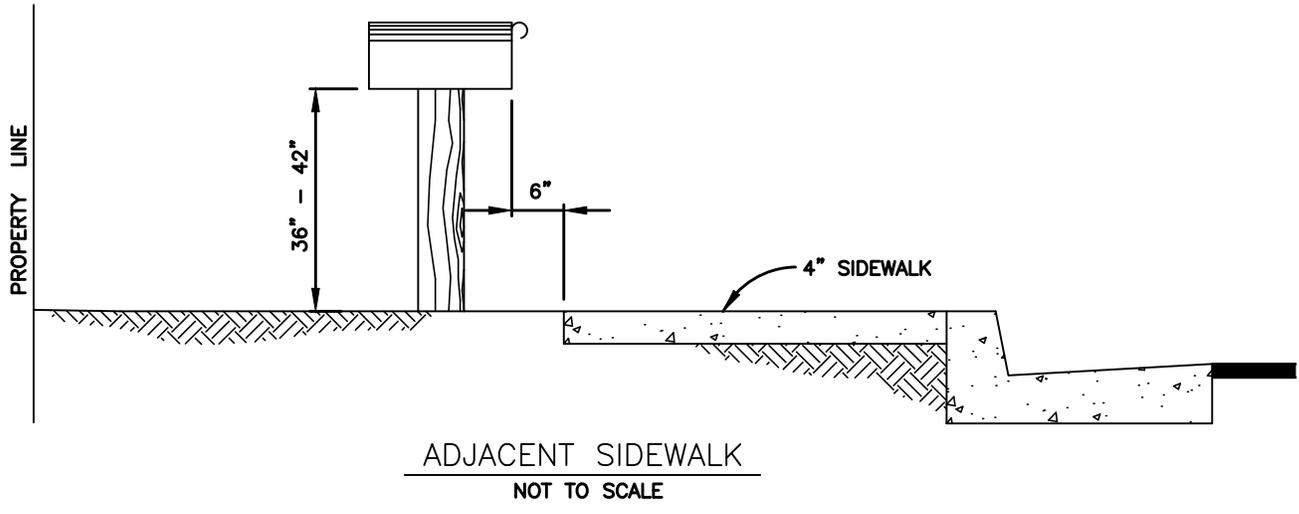
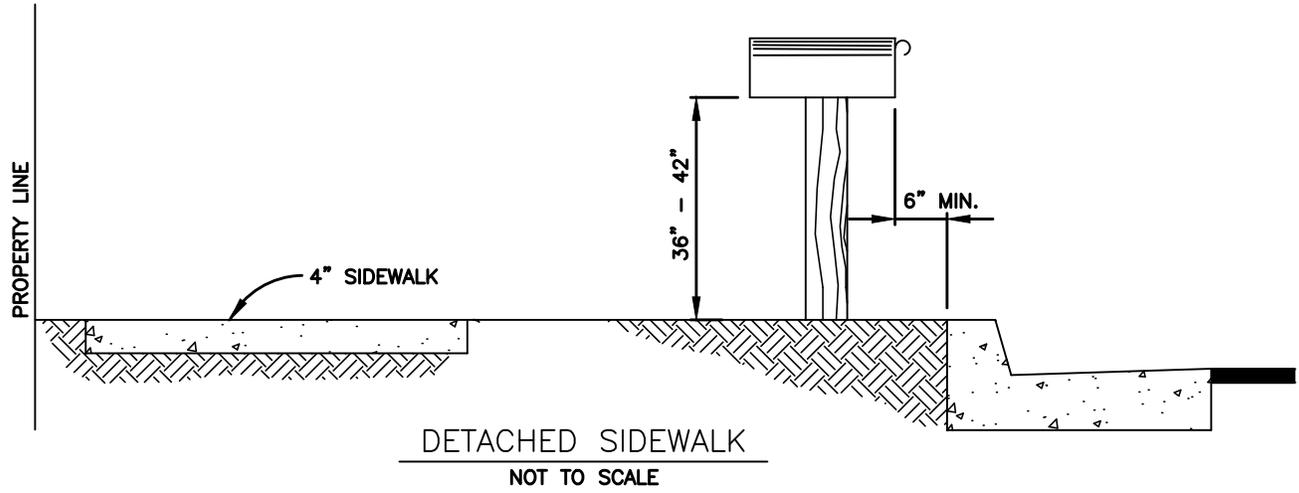
Standard Drawing for:

**TEMPORARY  
 BARRICADE**

APPROVED BY:  03/01/17  
 City Engineer R.C.E 53350 Date

Revised:

**M-3**



NOTES:

1. MAIL BOXES SHALL BE CLUSTRED IN GROUPS OF 2 MIN.
2. EACH MAIL BOX SHALL BE NUMBERED WITH THE ADDRESS OF THE RESIDENCE WHICH IT SERVES.
3. THE LOCATION OF MAIL BOXES SHALL BE DETERMINED BY THE PUBLIC WORKS DIRECTOR.
4. THE OWNERS ARE RESPONSIBLE FOR ALL MAINTENANCE & REPAIR.
5. MAIL BOXES SHALL MEET U.S. POSTAL SERVICE REQUIREMENTS.

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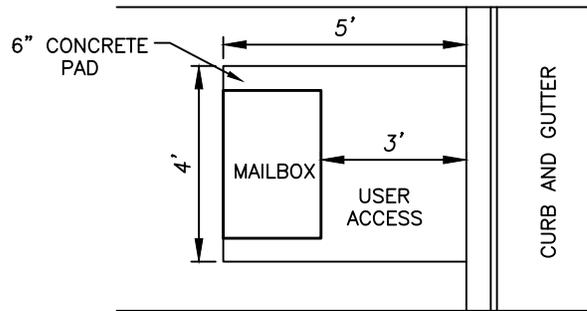
Standard Drawing for:

**MAILBOX**

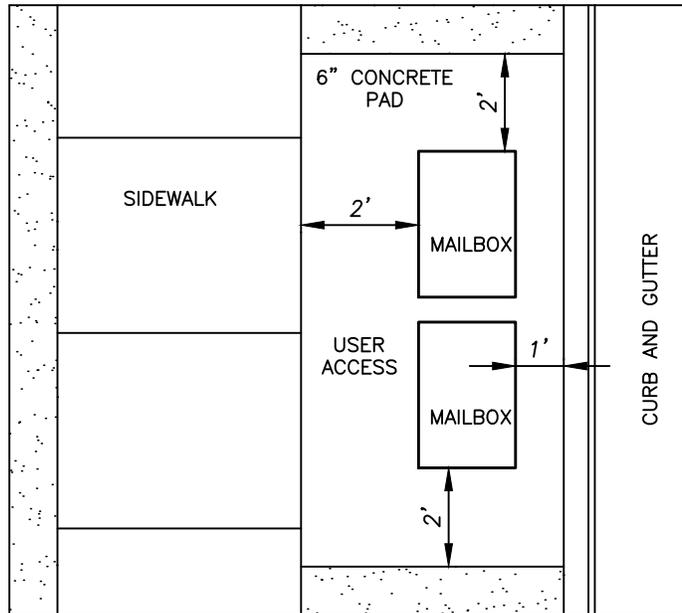
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City Engineer R.C.E 53350 Date

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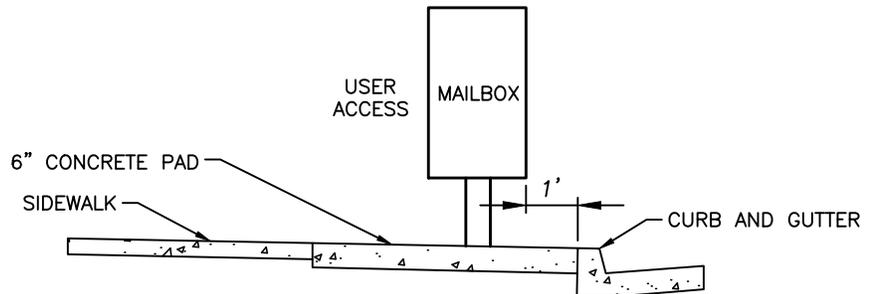
**M-4**



PLAN – WITHOUT SIDEWALK



PLAN – WITH SIDEWALK



SECTION – WITH SIDEWALK

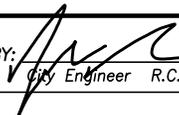
**NOTES:**

1. ALL MAILBOX CLUSTER DESIGNS SHALL BE APPROVED BY THE UNITED STATES POSTAL SERVICE PRIOR TO INSTALLATION.
2. ALL MAILBOX CONCRETE PADS SHALL HAVE 4"X6" WELDED WIRE MESH REINFORCEMENT.

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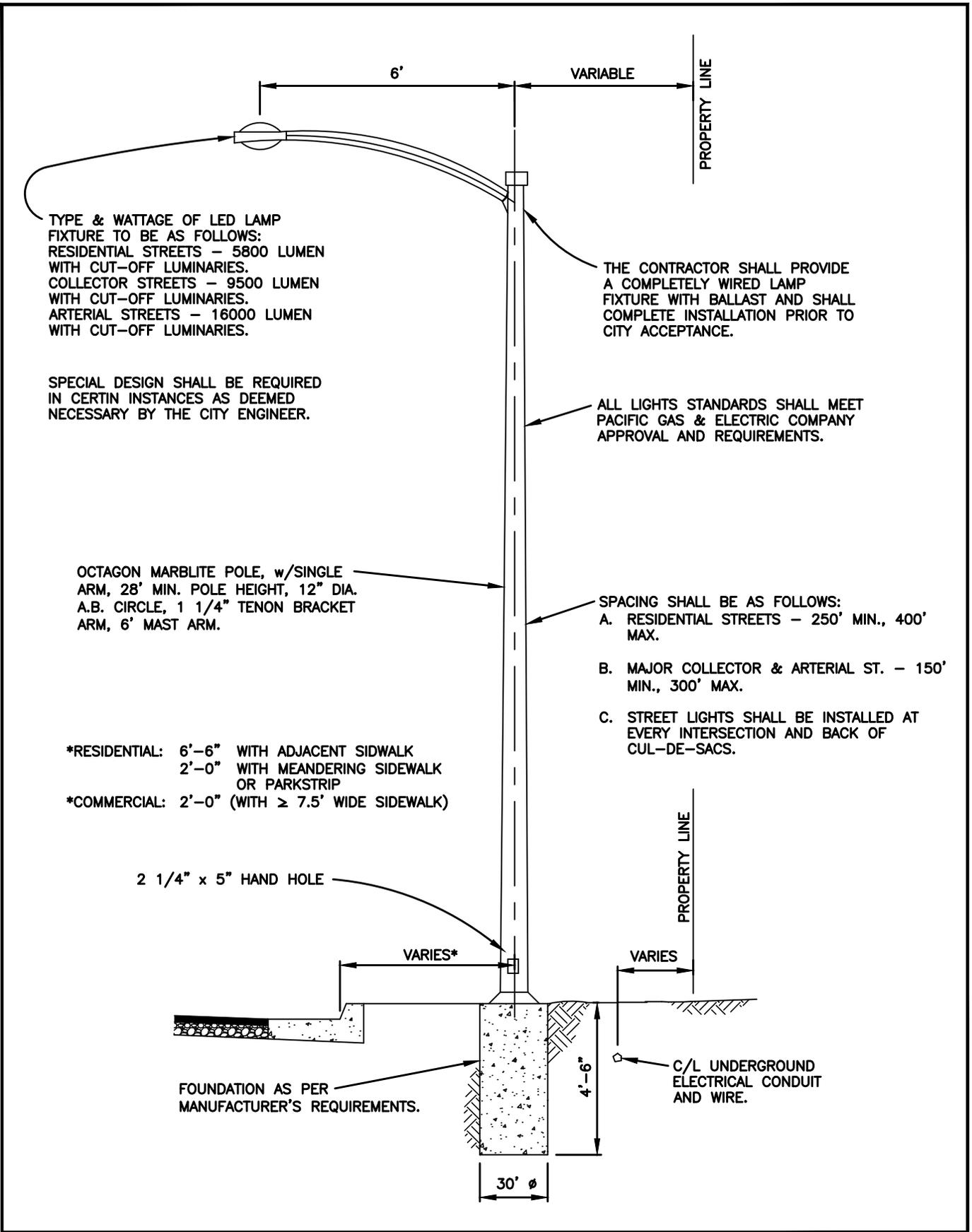
Standard Drawing for:

**MAILBOX CLUSTER**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

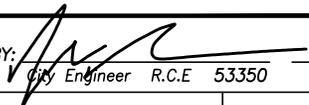
**M-4A**



CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**STREET LIGHT  
 INSTALATION**

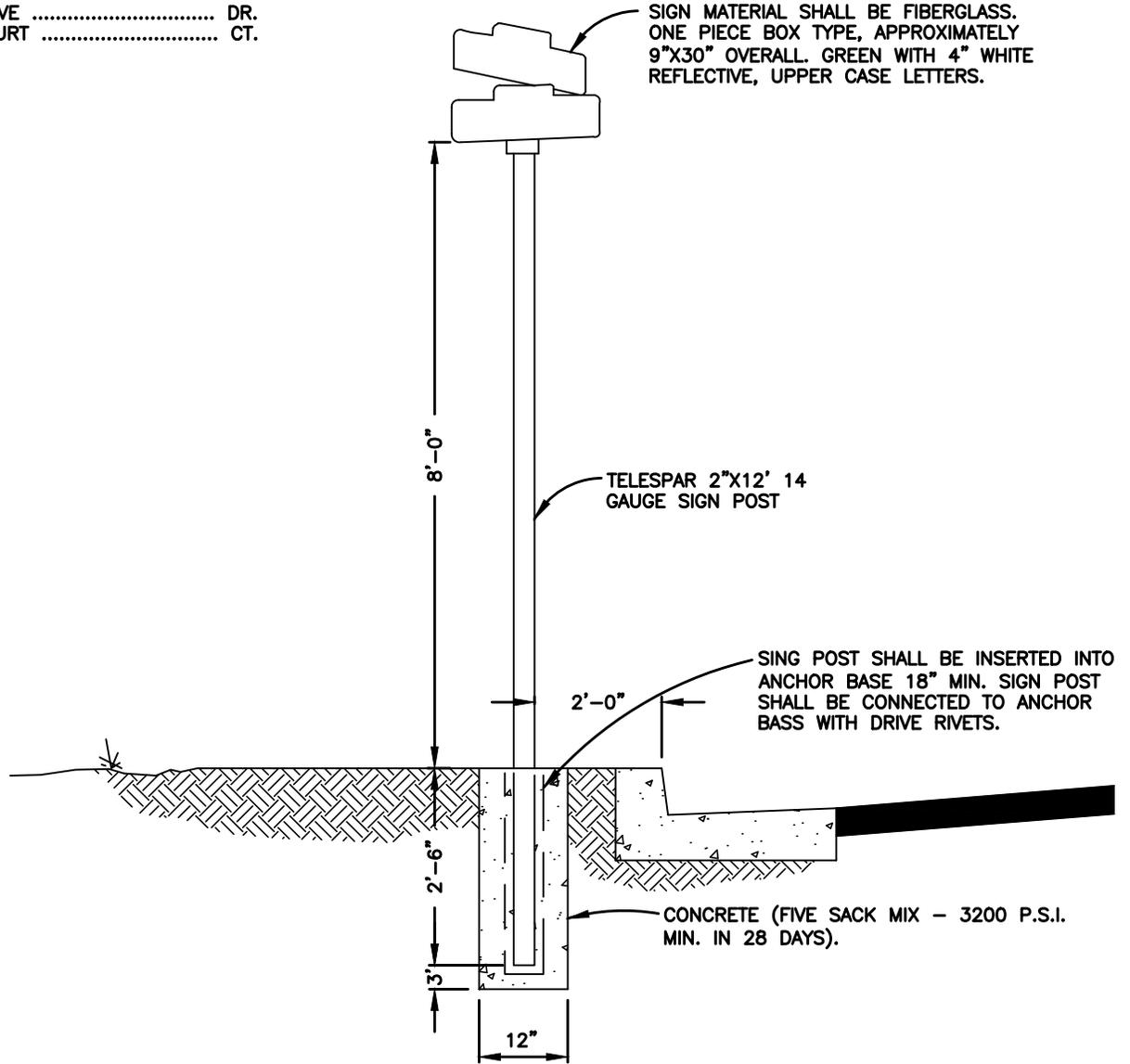
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 City Engineer R.C.E 53350 Date

Revised:

**M-5**

ALLOWABLE ABBREVIATIONS

STREET ..... ST.  
 AVENUE ..... AVE.  
 CIRCLE ..... CR.  
 DRIVE ..... DR.  
 COURT ..... CT.



NOTES:

1. STREET NAME SIGNS SHALL BE LOCATED AS DESIGNATED BY THE CITY ENGINEER.

CITY of CORCORAN • Department of Public Works

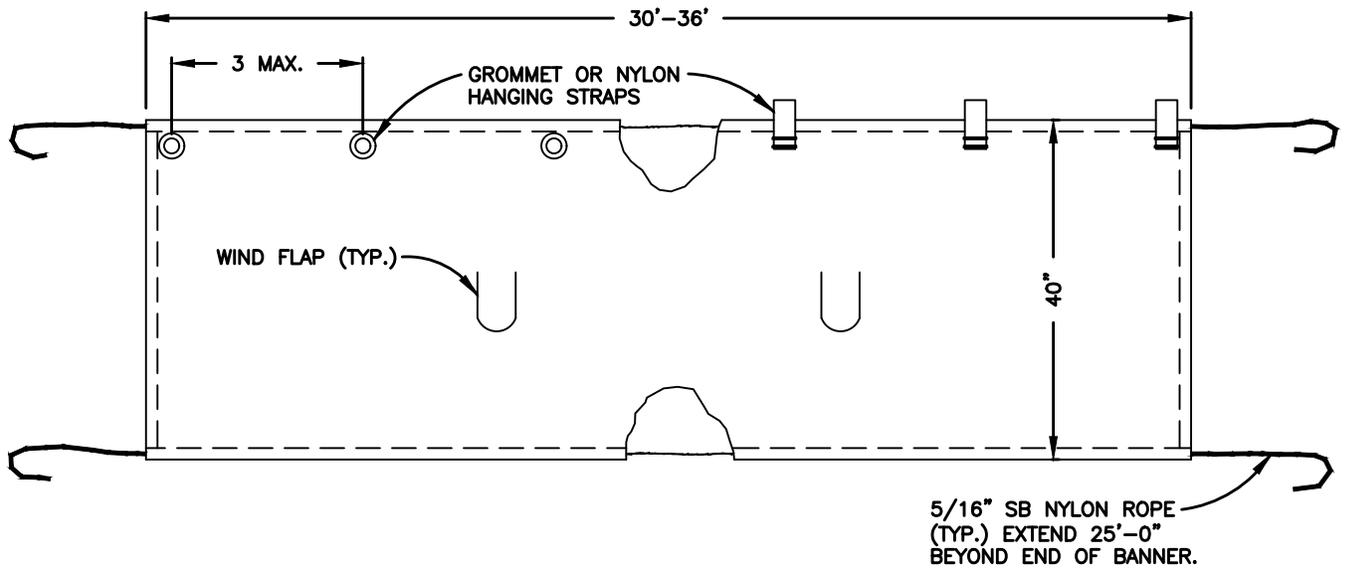
Standard Drawing for:

**STREET NAME SIGN**

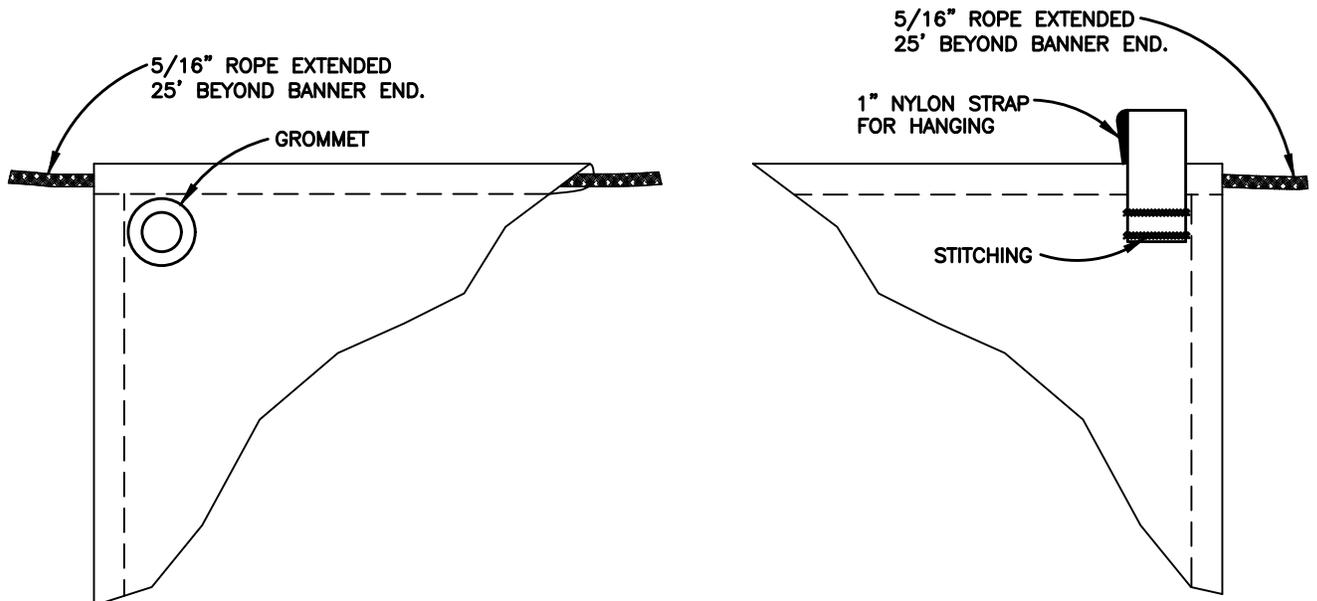
APPROVED BY:  03/01/17  
 City Engineer R.C.E 53350 Date

Revised:

**M-6**



FRONT  
NOT TO SCALE



ALTERNATIVE TO ATTACHMENT  
BY GROMMETS  
NOT TO SCALE

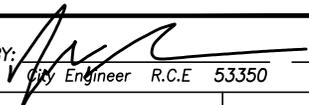
NOTES:

1. BANNER MATERIAL SHALL BE CANVAS OR APPROVED EQUAL.
2. ROPE SHALL BE SEWN INTO THE TOP AND BOTTOM EDGE OF THE BANNER LEAVING 25' OF ROPE EXTENDED BEYOND EACH END OF THE BANNER FOR TYING PURPOSES AND RUNNING ENTIRE LENGTH THRU EDGES OF BANNER.
3. EACH BANNER SHALL HAVE SUFFICIENT WIND FLAPS CUT INTO IT AS SHOWN.

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**BANNER DETAIL**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**M-7**

## REFUSE REQUIREMENTS AND STANDARDS

### 1. BIN CAPACITY REQUIREMENTS:

BIN CAPACITY REQUIREMENTS SHALL BE BASED ON FOUR APARTMENT UNITS PER ONE YARD OF BIN CAPACITY WITH TWICE A WEEK COLLECTION SERVICE. THE CITY OF CORCORAN USES ONE, TWO AND THREE YARD BINS. ANY COMBINATION OF BIN SIZES MAY BE USED TO MEET THE BIN CAPACITY REQUIREMENTS. REFUSE RATES ARE SET UP FOR UP TO SIX DAY PER WEEK COLLECTION (IF NECESSARY).

### 2. REFUSE ENCLOSURE REQUIREMENTS:

THE CITY OF CORCORAN REQUIRES THAT ALL REFUSE BINS BE PLACED IN ENCLOSURES. THE REFUSE ENCLOSURES SHALL BE CONSTRUCTED WITH CONCRETE BLOCK.

THE REFUSE ENCLOSURES SHALL BE A MINIMUM OF SIX FEET HIGH. MINIMUM INSIDE ENCLOSURE DIMENSIONS ARE AS FOLLOWS:

ONE-1 YARD BIN	-	10 FEET X 10 FEET
ONE-2 YARD BIN	-	10 FEET X 10 FEET
TWO-2 YARD BINS	-	10 FEET X 15 FEET
ONE-3 YARD BIN	-	10 FEET X 10 FEET
TWO-3 YARD BINS	-	10 FEET X 20 FEET

### 3. REFUSE ENCLOSURE ACCESS REQUIREMENTS:

ALL REFUSE ENCLOSURES SHALL BE SITUATED TO MAJOR ROADWAYS AND SHALL NOT BE SITUATED SUCH THAT PARKED VEHICLES BLOCK ACCESS. ALL MAJOR ROADWAYS WHICH MUST BE TRAVELED BY REFUSE TRUCKS SHALL NOT HAVE ANY TURNING RADIUS LESS THAN 30 FEET.

### 4. REFUSE ENCLOSURE USE REQUIREMENTS:

THE ENCLOSURE SHALL BE USED ONLY FOR PLACING REFUSE TO BE PICKED UP BY THE CITY OF CORCORAN REFUSE DIVISION. ALL OTHER USES MUST BE PROVIDED FOR IN A SEPARATE AREA.

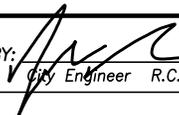
### 5. SEPARATE RECYCLABLES:

1. ALL RESIDENTIAL CUSTOMERS WITHIN THE CITY SERVICE AREA SHALL BE PROVIDED WITH ONCE A WEEK RECYCLABLES CONTAINER SERVICES BY THE AUTHORIZED AGENT IN ACCORDANCE WITH INSTRUCTIONS; RESIDENTIAL CUSTOMERS SHALL SEPARATE AND PLACE RECYCLABLES CURBSIDE, OR IN OTHER RECYCLABLES BIN CONTAINER LOCATIONS AS APPROVED AND DIRECTED BY THE CITY, FOR COLLECTION. ALL PERSONS USING THE CITY SOLID WASTE COLLECTION SERVICE OR A DULY PERMITTED COLLECTOR SHALL SEPARATE RECYCLABLES FROM ALL OTHER SOLID WASTE PRODUCED AT THEIR PREMISES.
2. AN OWNER, LANDLORD OR AGENT OF AN OWNER OR LANDLORD OF A MULTI-FAMILY RENTAL PROPERTY WITH THREE (3) OR MORE UNITS ON SINGLE WATER SERVICE, SHALL COMPLY WITH ITS SEPARATION RESPONSIBILITIES BY ESTABLISHING A COLLECTION AND STORAGE SYSTEM FOR SEPARATED RECYCLABLES AT EACH PREMISES.
3. ANY FAILURE BY A RESIDENTIAL CUSTOMER, OWNER, LANDLORD OR AGENT OF AN OWNER TO PROPERLY SEPARATE RECYCLABLES FROM OTHER WASTE AND/OR COMPLY WITH THE REQUIREMENTS OF SUBSECTION 1 OR 2, LISTED ABOVE, AS RELEVANT, MAY RESULT IN CERTAIN FINES AND/OR ADMINISTRATIVE PENALTIES, AS SET FORTH IN SECTION 4-2-12 OF THE CITY CODE. (ORD. 582, 3-23-2005)

## CITY of CORCORAN • Department of Public Works

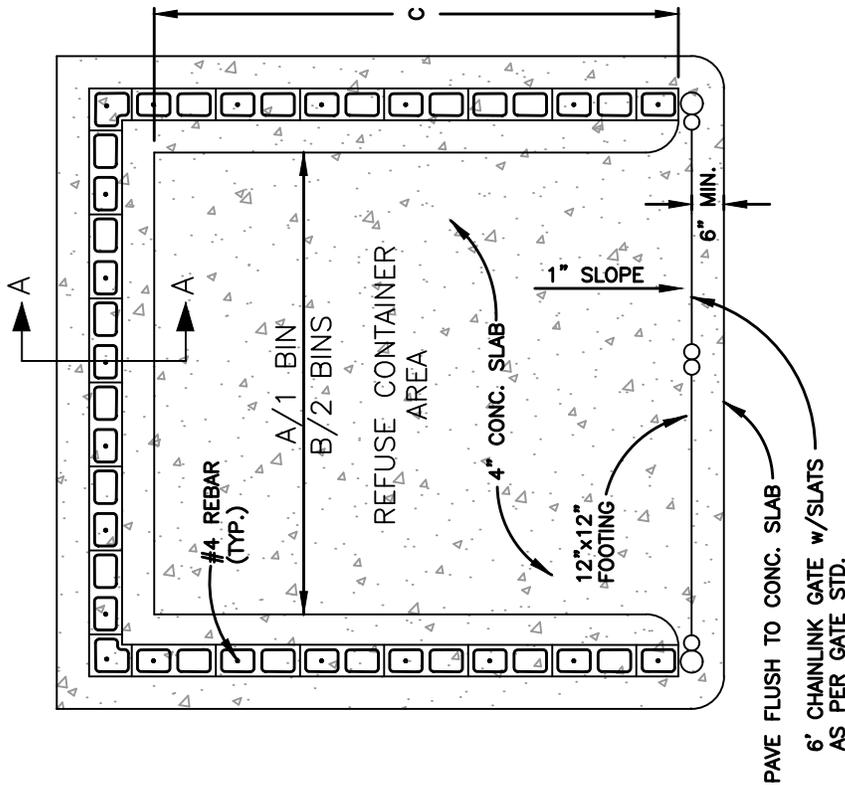
Standard Drawing for:

# REFUSE REQUIREMENTS

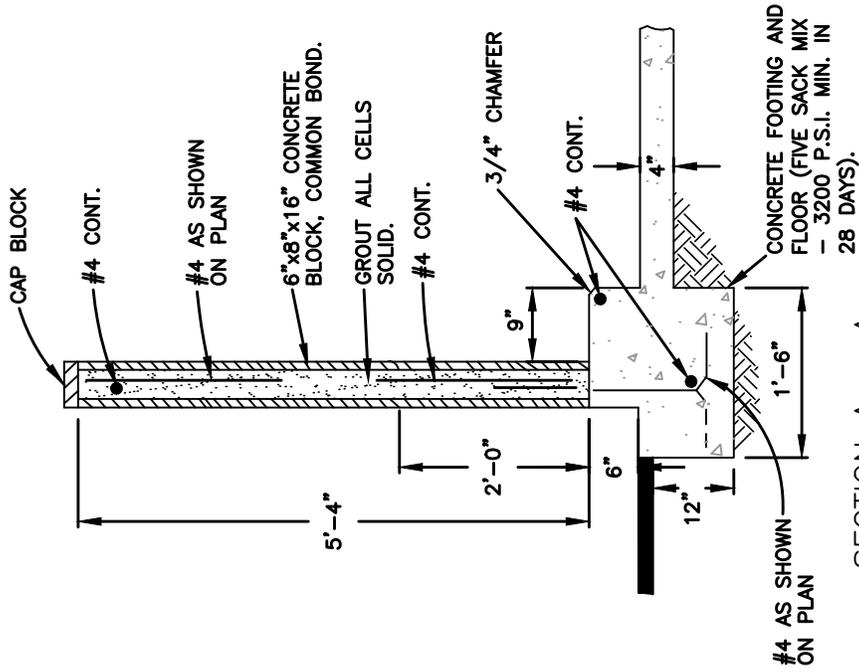
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City Engineer R.C.E 53350 Date

Revised:

M-8



PLAN  
NOT TO SCALE



SECTION A - A  
(TYP.)  
NOT TO SCALE

MIN. INSIDE ENCLOSURE DIMENSIONS

	1 YD.	2 YD.	3 YD.
A	10'	10'	10'
B	-	15'	20'
C	10'	10'	10'

**NOTE:**

1. PUBLIC WORKS RECOMMENDED ENCLOSURE.
2. CUT CORNER BLOCK ENDS AND CENTER WEB TO FORM A CONTINUOUS BOND BEAM.
3. GROUND TO BE SLOPED AWAY FROM WALL.
4. LOCATION OF BIN ENCLOSURE TO BE APPROVED BY THE CITY PUBLIC WORKS DIRECTOR.
5. A BUILDING PERMIT SHALL BE OBTAINED FROM THE CITY BUILDING DEPARTMENT.

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**TRASH BIN - BLOCK ENCLOSURE**

APPROVED BY:

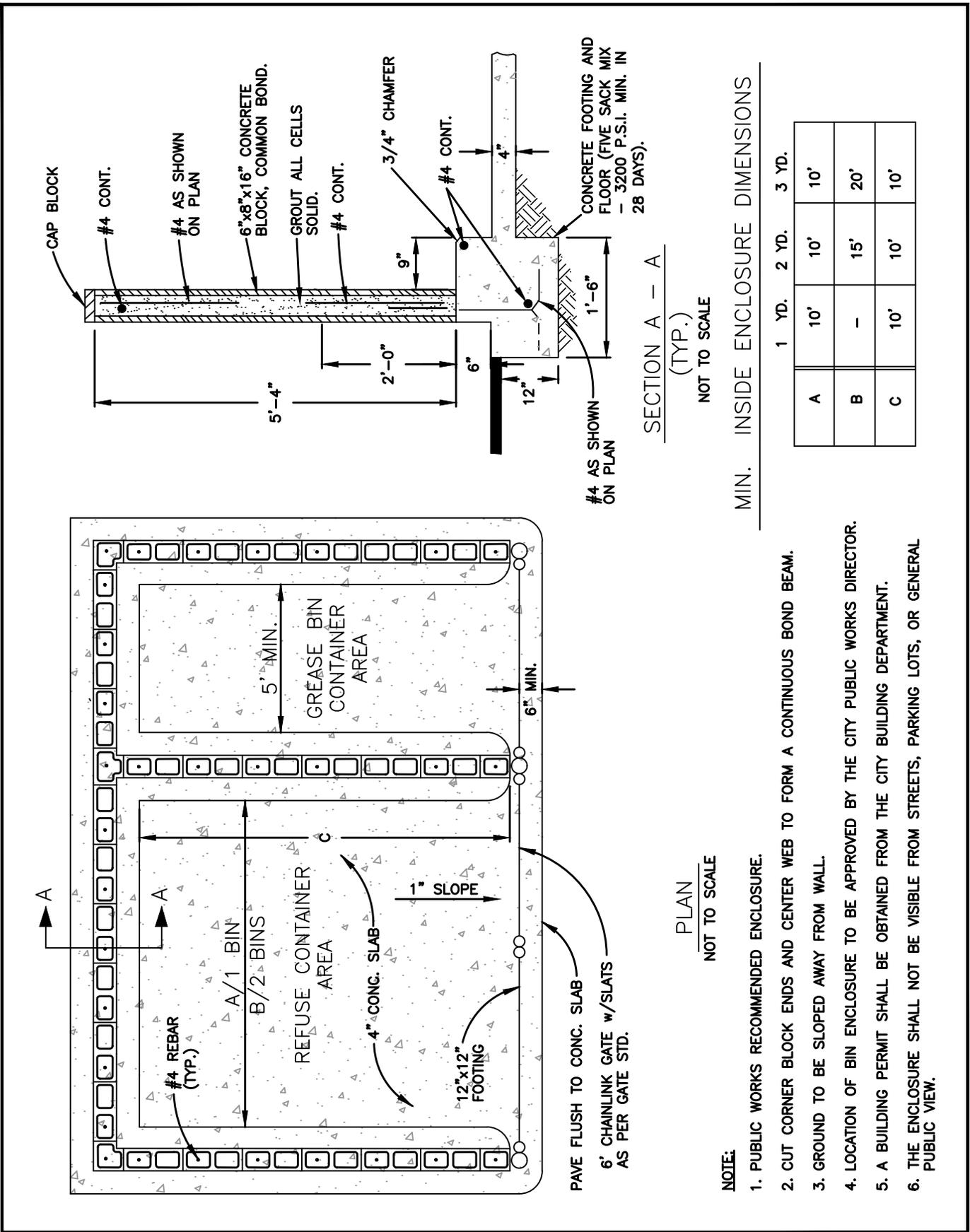
City Engineer R.C.E 53350

03/01/17

Date

Revised:

**M-9**



SECTION A - A  
(TYP.)  
NOT TO SCALE

PLAN  
NOT TO SCALE

MIN. INSIDE ENCLOSURE DIMENSIONS

	1 YD.	2 YD.	3 YD.
A	10'	10'	10'
B	-	15'	20'
C	10'	10'	10'

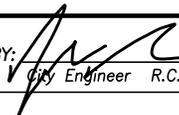
**NOTE:**

1. PUBLIC WORKS RECOMMENDED ENCLOSURE.
2. CUT CORNER BLOCK ENDS AND CENTER WEB TO FORM A CONTINUOUS BOND BEAM.
3. GROUND TO BE SLOPED AWAY FROM WALL.
4. LOCATION OF BIN ENCLOSURE TO BE APPROVED BY THE CITY PUBLIC WORKS DIRECTOR.
5. A BUILDING PERMIT SHALL BE OBTAINED FROM THE CITY BUILDING DEPARTMENT.
6. THE ENCLOSURE SHALL NOT BE VISIBLE FROM STREETS, PARKING LOTS, OR GENERAL PUBLIC VIEW.

CITY of CORCORAN • Department of Public Works

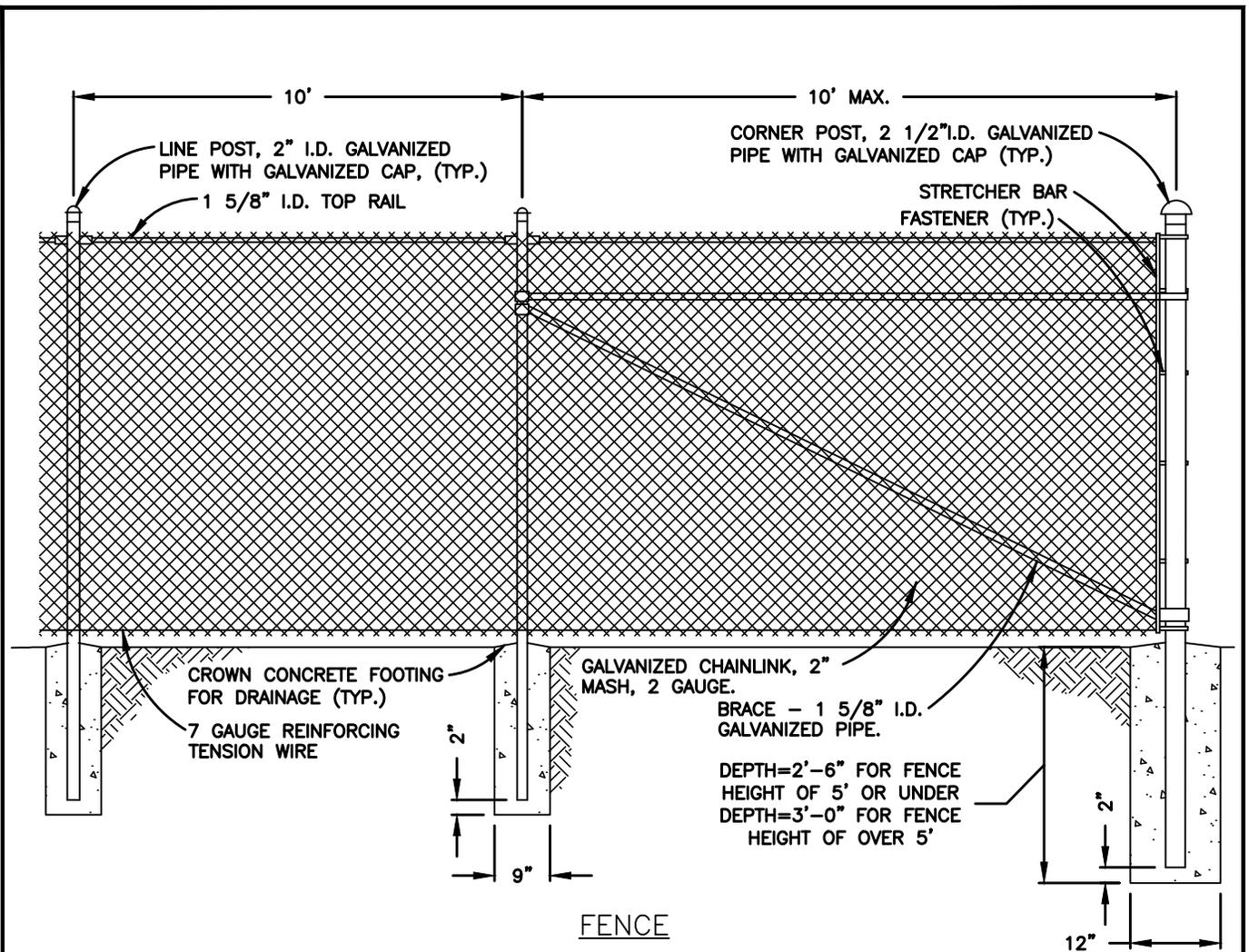
Standard Drawing for:

**TRASH BIN - BLOCK ENCLOSURE  
(FOR RESTAURANT USE)**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

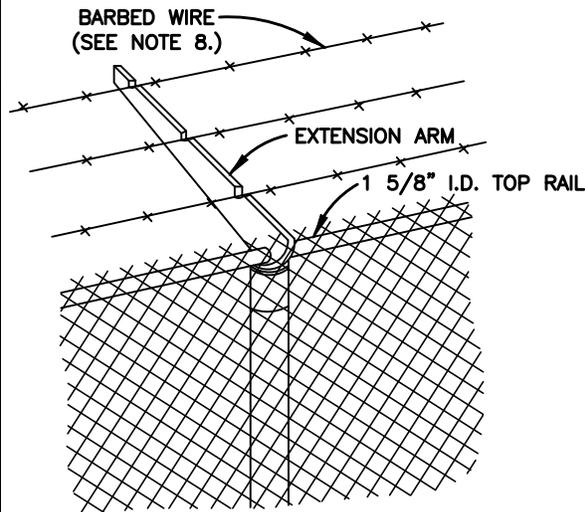
**M-9A**



**FENCE**

**NOTES:**

1. DEPTH OF FOOTINGS ARE INTO NATURAL UNDISTURBED SOIL OR TESTED OR APPROVED COMPACTED FILL.
2. CONCRETE FOOTINGS SHALL BE A MIN. FIVE SACK MIX. (3200 P.S.I. MIN. IN 28 DAYS).
3. THE FOLLOWING ITEMS SHALL BE FURNISHED & INSTALLED ONLY WHEN SHOWN ON PLANS AND/OR CALLED FOR IN THE SPECIAL PROVISIONS:
  - A. BARBED WIRE
  - B. EXTENTION ARM
  - C. TOP RAIL
  - D. VINYL SLATS
4. 9 GAUGE WIRE FASTENERS SHALL BE USED.
5. DETAIL 'A' REFERS TO ALL PUBLIC ENCLOSURES UNLESS SPECIFICALLY NOTED OTHERWISE.
6. WHEN FENCE IS AROUND A DRAINAGE BASIN A 6" w x 8" d CONCRETE CURB SHALL BE POURED UNDER FENCE, FLUSH WITH BOTTOM OF FENCE FABRIC.
7. WHERE SLATS ARE CALLED FOR ON PLANS, ALL LINE POSTS SHALL BE 2-3/8" O.D. SCH. 40 PIPE.
8. INSTALLATION OF BARBED WIRE REQUIRES CITY APPROVAL PRIOR TO INSTALLATION.



**DETAIL A**

**CITY of CORCORAN • Department of Public Works**

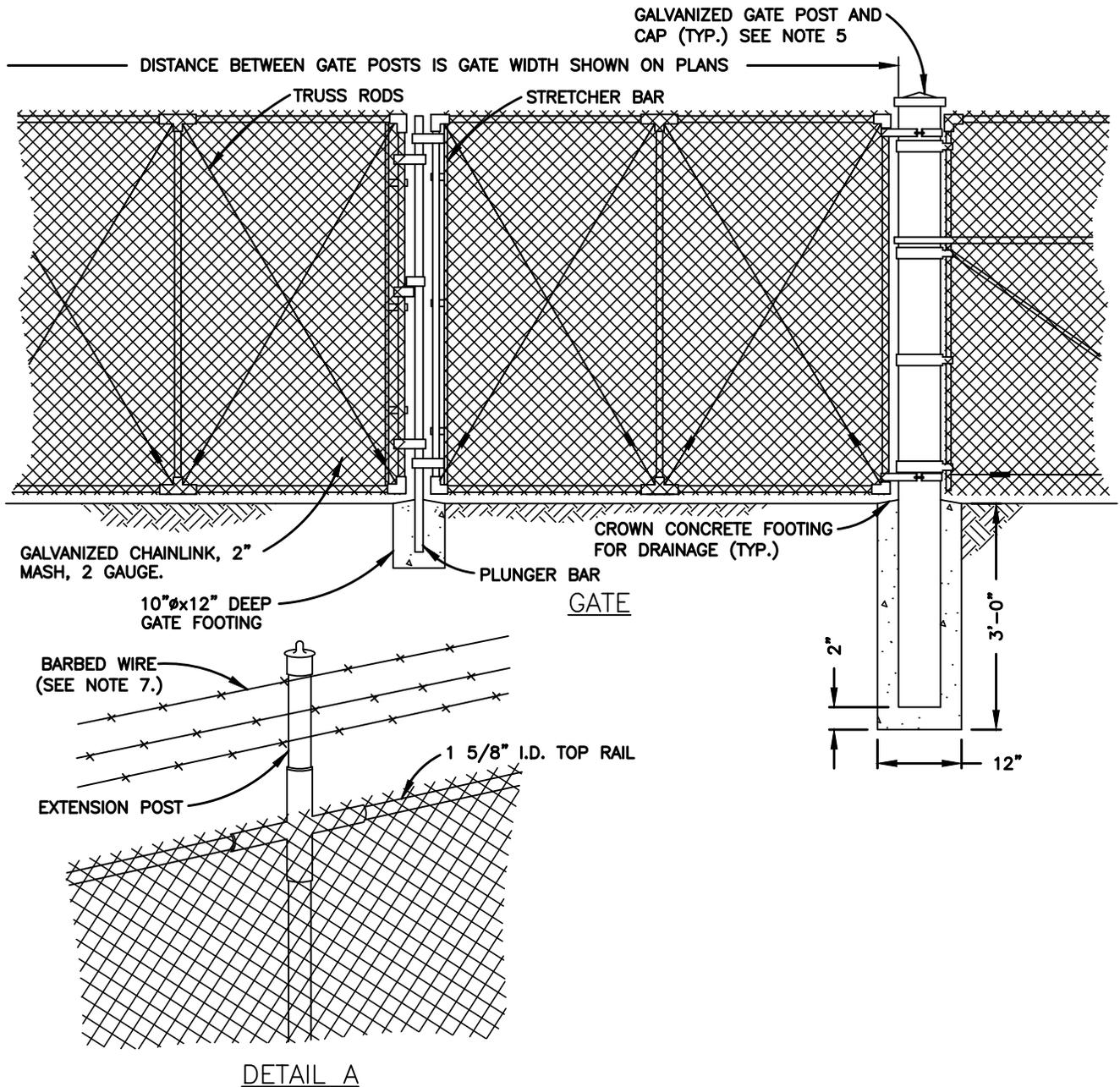
Standard Drawing for:

**CHAIN LINK FENCE  
DETAILS**

APPROVED BY: *[Signature]* 03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**M-10**



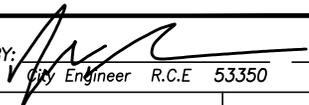
**NOTES:**

1. DEPTH OF FOOTINGS ARE INTO UNDISTURBED SOIL OR TESTED AND APPROVED COMPACTED FILL.
2. CONCRETE FOOTINGS SHALL BE A MIN. 5 SACK MIX AND TEST TO A MIN. OF 3200 P.S.I. IN 28 DAYS.
3. THE FOLLOWING ITEMS SHALL BE FURNISHED AND INSTALLED ONLY WHEN SHOWN ON PLANS AND/OR CALLED FOR IN THE SPECIAL PROVISIONS:
  - A. BARBED WIRE
  - B. EXTENSION ARM
4. 12 GAUGE WIRE FASTENERS SHALL BE USED.
5. GATE POSTS - USE 2 1/2" I.D. PIPE FOR DOUBLE GATE WIDTHS UP TO 12' AND 4" I.D. PIPE FOR DOUBLE GATE WIDTHS 12' TO 24'.
6. DETAIL 'A' REFERS TO ALL PUBLIC ENCLOSURES UNLESS SPECIFICALLY NOTED OTHERWISE.
7. INSTALLATION OF BARBED WIRE REQUIRES CITY APPROVAL PRIOR TO INSTALLATION.

**CITY of CORCORAN • Department of Public Works**

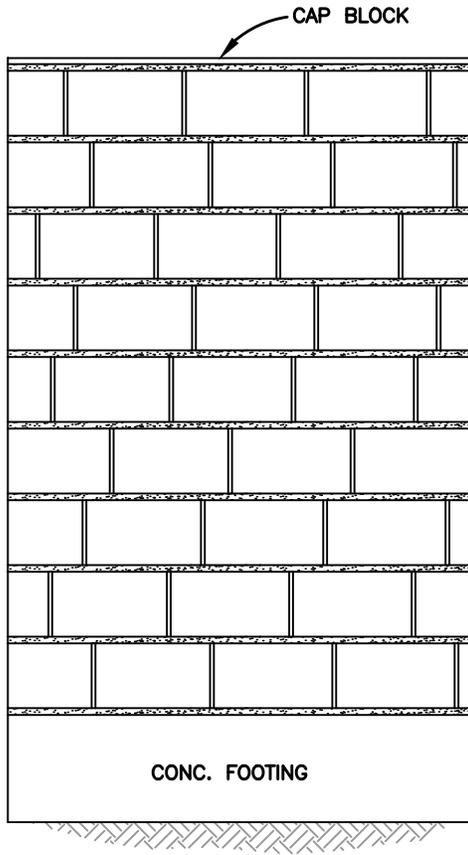
Standard Drawing for:

**CHAIN LINK GATE  
DETAILS**

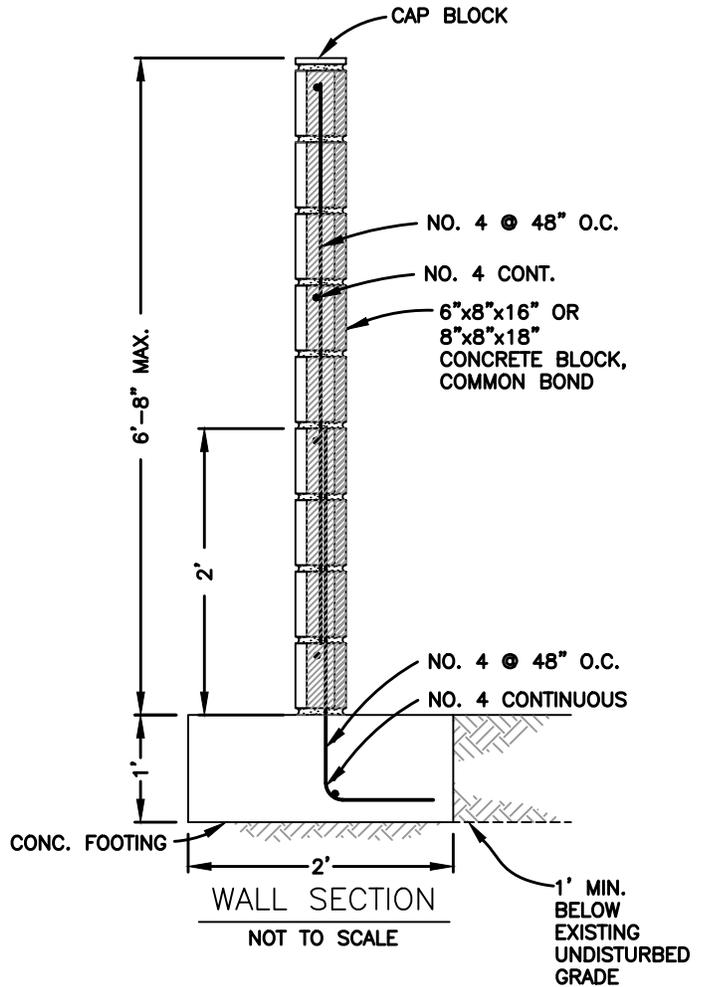
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Revised:

**M-11**



ELEVATION VIEW  
NOT TO SCALE



WALL SECTION  
NOT TO SCALE

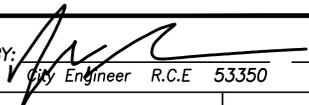
**NOTE:**

1. LATEST EDITION OF THE CBC SHALL BE COMPLIED WITH.
2. WALL THICKNESS SHALL BE EITHER 6" OR 8".
3. GROUT TO BE PLACED AT LOCATION OF VERTICAL REINFORCING BARS.
4. MORTAR SHALL CONSIST OF PORTLAND CEMENT AND SAND MEETING TYPE "S" REQUIREMENTS AS PER LATEST ADOPTED BUILDING CODE.
5. A BUILDING PERMIT IS REQUIRED FOR THE INSTALLATION OF A BLOCK FENCE.
6. CONCRETE SHALL BE MINIMUM FIVE SACK MIX. (3200 P.S.I. MIN. IN 28 DAYS).

CITY of CORCORAN • Department of Public Works

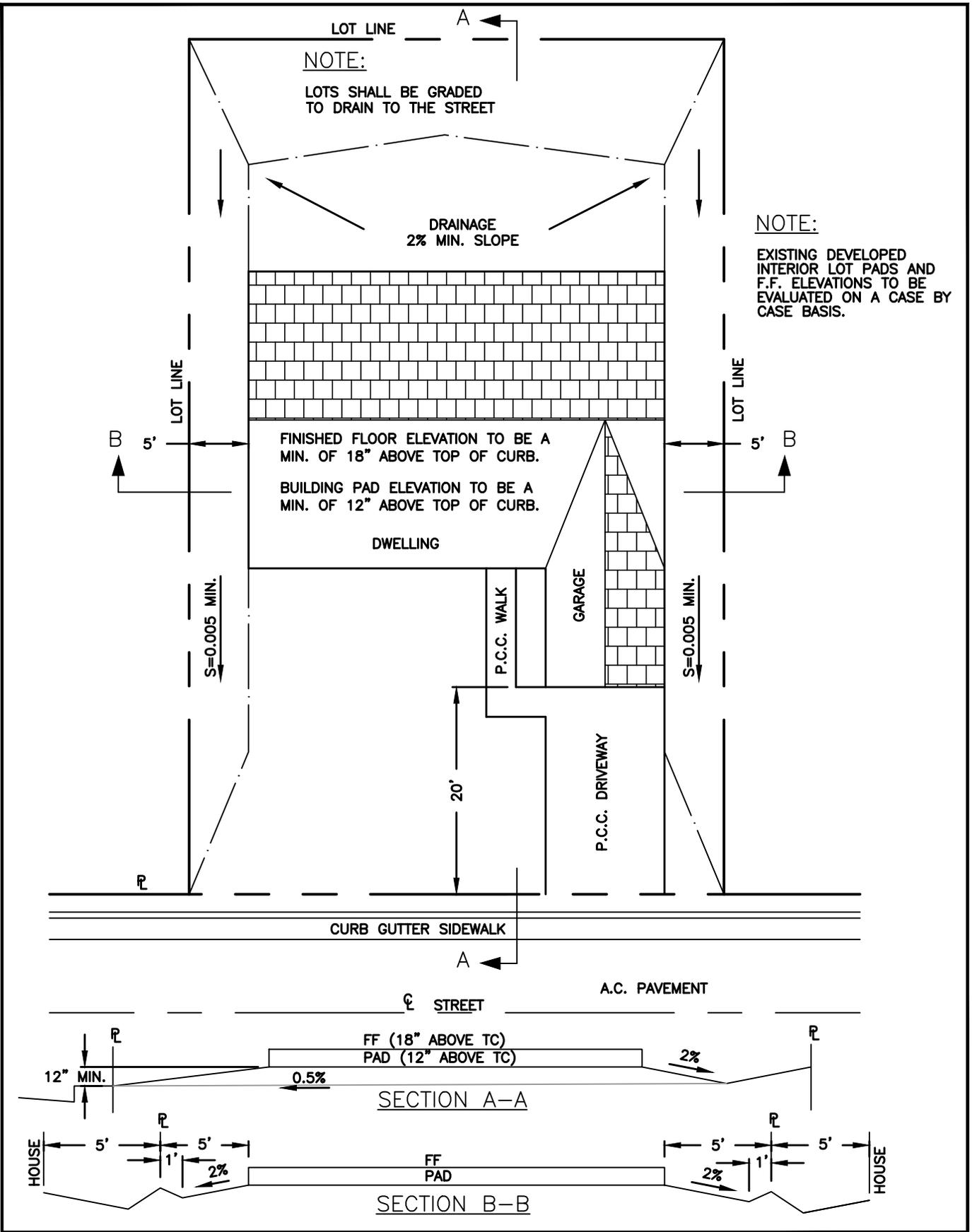
Standard Drawing for:

**NON-DECORATIVE  
BLOCK FENCE**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

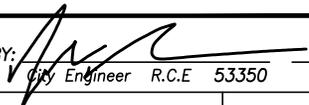
**M-12**



CITY of CORCORAN • Department of Public Works

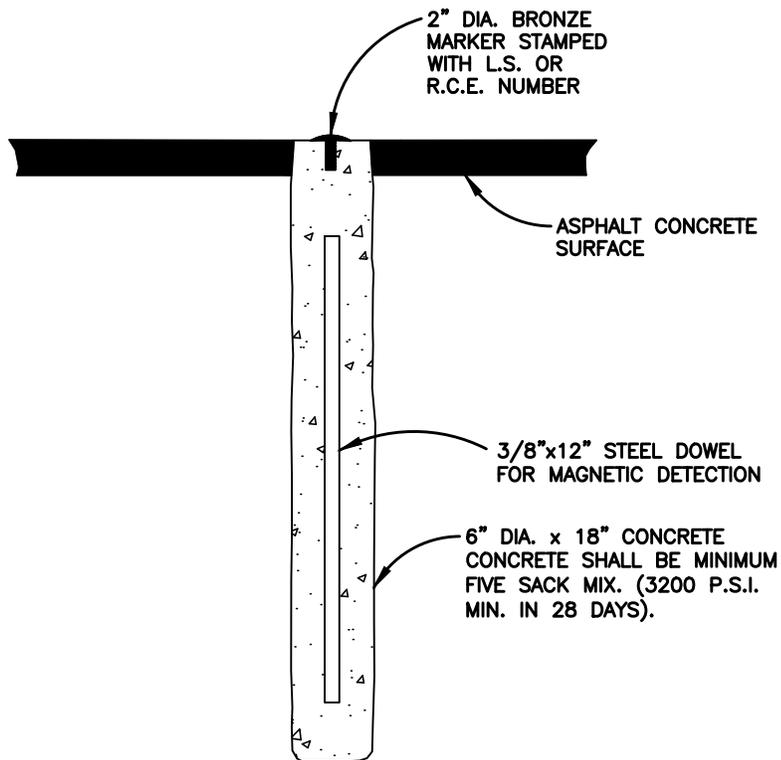
Standard Drawing for:

**TYPICAL LOT DRAINAGE**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**M-13**



STREET MONUMENTS  
NOT TO SCALE

NOTES:

1. STATE LAW REQUIRES ALL CORNERS TO BE TAGGED WITH LAND SURVEYORS OR REGISTERED CIVIL ENGINEERS NUMBER.
2. STREET MONUMENTS SHALL BE USED TO LOCATE ALL ANGLE AND CURVE POINTS ON THE CENTERLINES OF ASPHALT CONCRETE SURFACED SUBDIVISION STREETS.
3. ADDITIONAL MONUMENTS MAY BE REQUIRED AT THE DISCRETION OF THE CITY ENGINEER.

CITY of CORCORAN • Department of Public Works

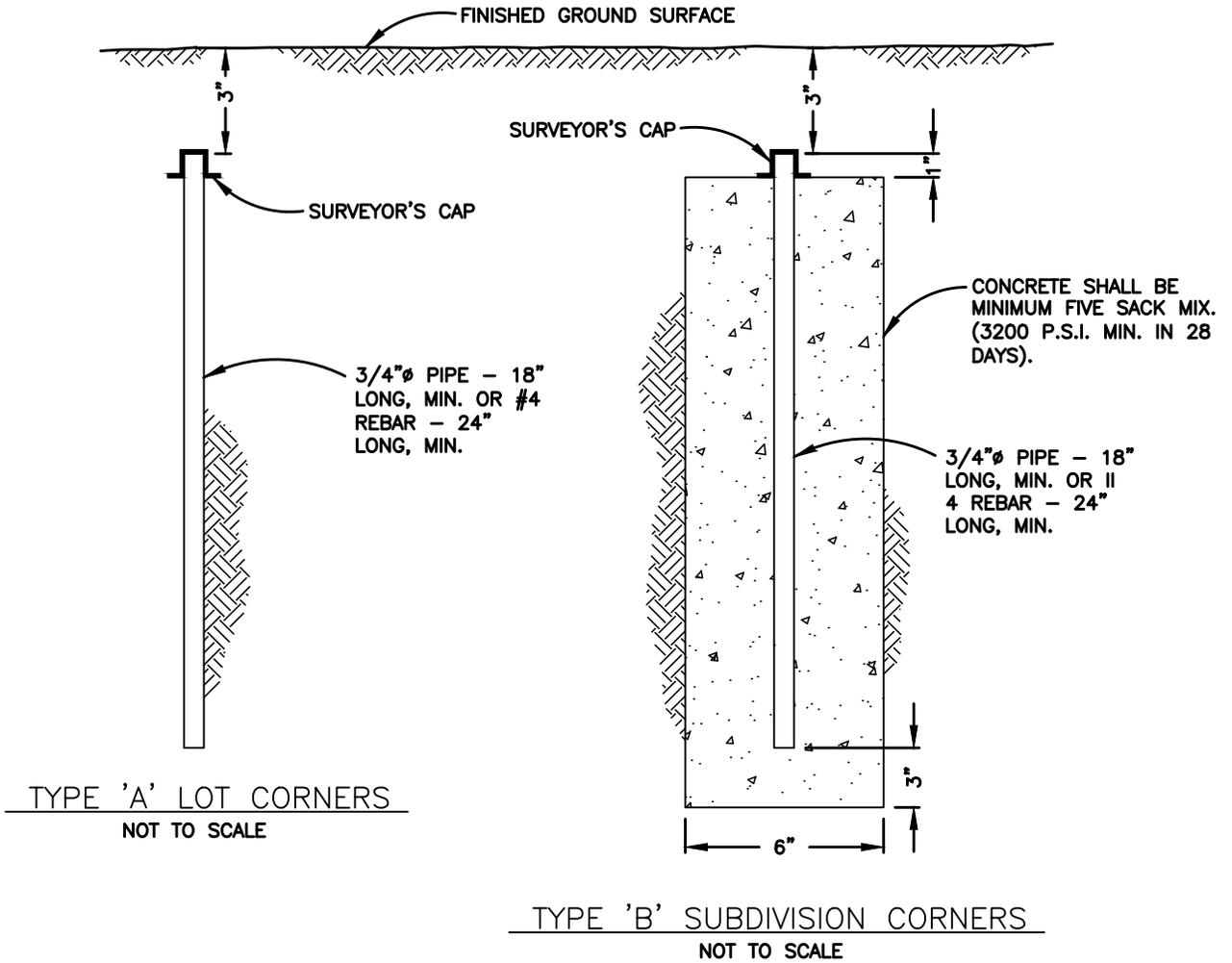
Standard Drawing for:

**STREET MONUMENT  
DETAIL**

APPROVED BY:  03/01/17  
City Engineer R.C.E. 53350 Date

Revised:

**M-14**



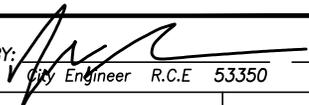
**NOTES:**

1. STATE LAW REQUIRES ALL CORNERS TO BE TAGGED WITH LAND SURVEYORS OR REGISTERED CIVIL ENGINEERS NUMBER.
2. ALL SUBDIVISION LOT CORNERS SHALL BE TYPE 'A'.
3. REGULAR-SHAPED SUBDIVISIONS SHALL HAVE BLOCK CORNERS MARKED TYPE 'B' IRREGULAR-SHAPED SUBDIVISIONS SHALL HAVE ALL ANGLE AND CURVE POINTS MARKED WITH TYPE 'B'.

**CITY of CORCORAN • Department of Public Works**

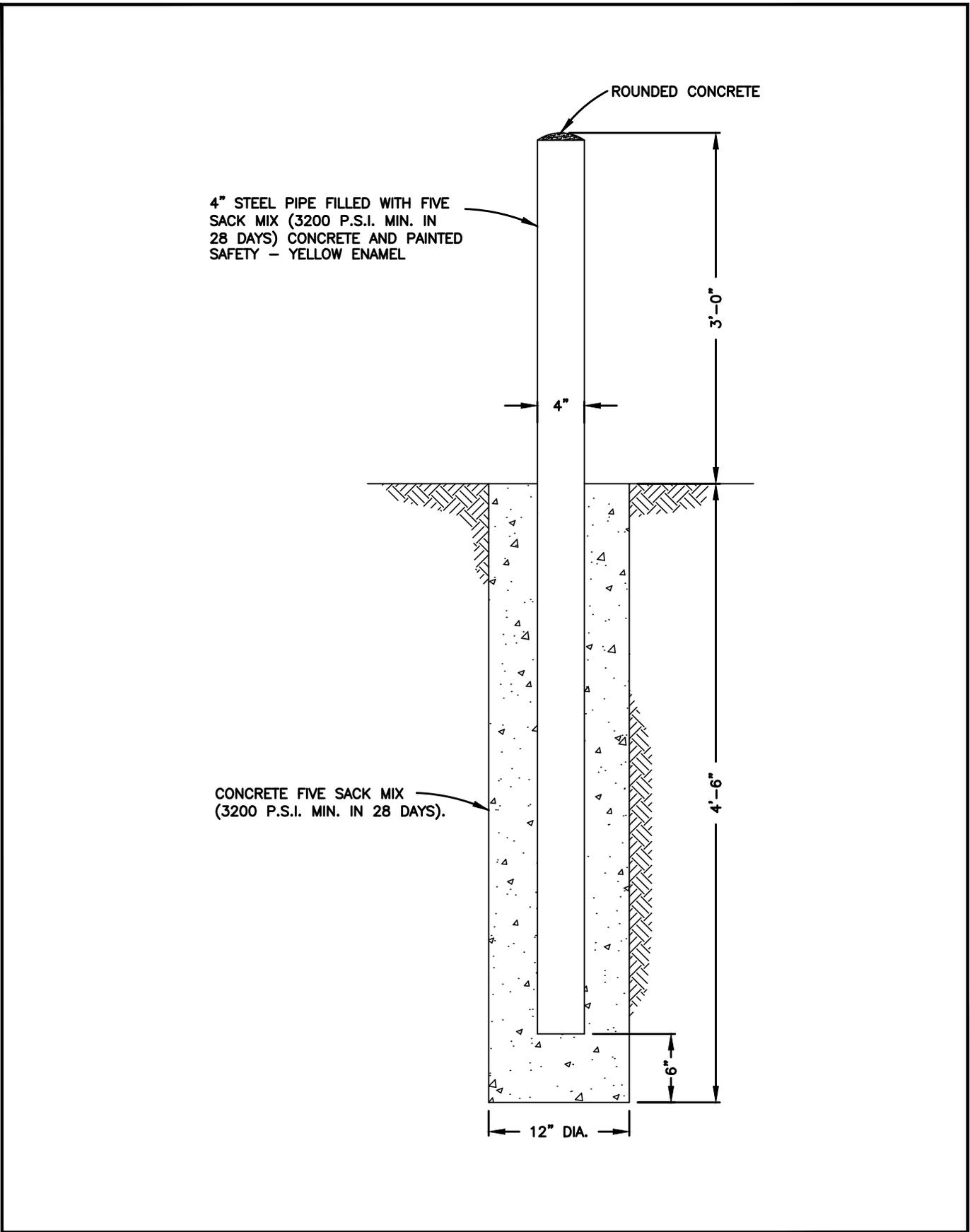
Standard Drawing for:

**PROPERTY MONUMENT  
DETAIL**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

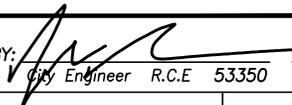
**M-15**



CITY of CORCORAN • Department of Public Works

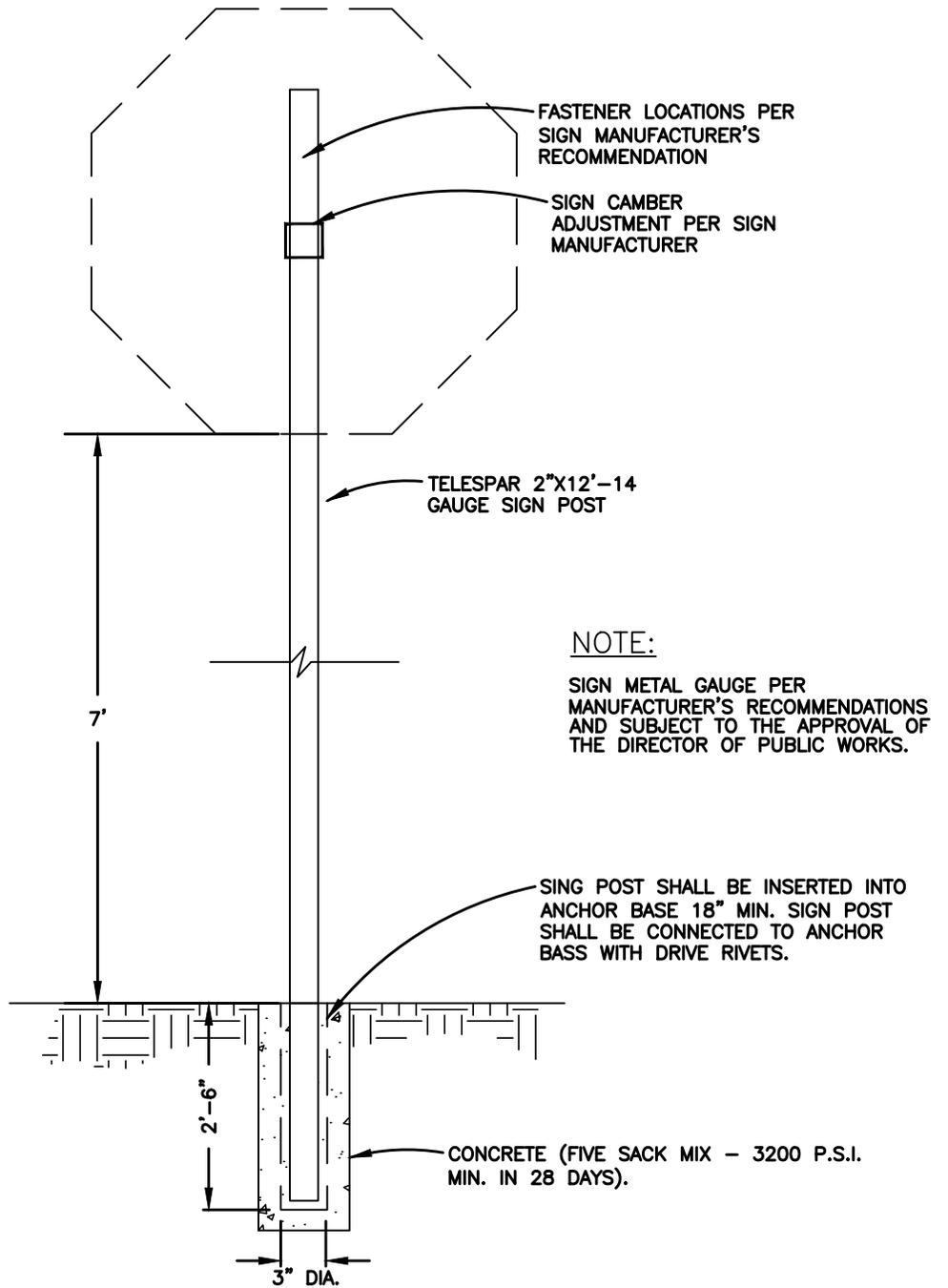
Standard Drawing for:

**PROTECTION POST**

APPROVED BY:  03/01/17  
 City Engineer R.C.E 53350 Date

Revised:

**M-16**



**NOTE:**  
 SIGN METAL GAUGE PER MANUFACTURER'S RECOMMENDATIONS AND SUBJECT TO THE APPROVAL OF THE DIRECTOR OF PUBLIC WORKS.

SIGN POST SHALL BE INSERTED INTO ANCHOR BASE 18" MIN. SIGN POST SHALL BE CONNECTED TO ANCHOR BASS WITH DRIVE RIVETS.

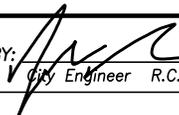
CONCRETE (FIVE SACK MIX - 3200 P.S.I. MIN. IN 28 DAYS).

TRAFFIC SIGNS  
 NOT TO SCALE

CITY of CORCORAN • Department of Public Works

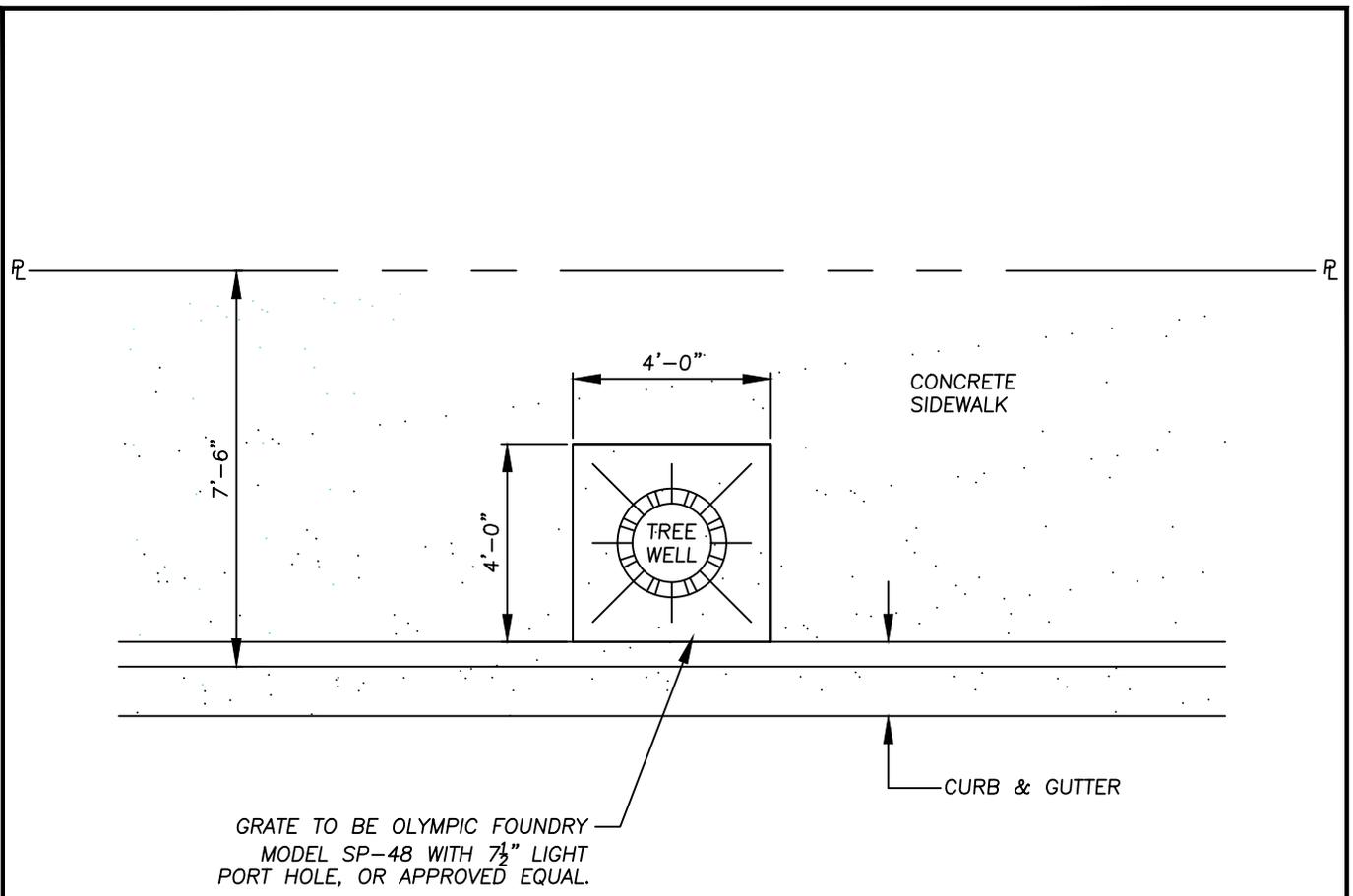
Standard Drawing for:

**STREET AND TRAFFIC  
 SIGNAL DESIGN**

APPROVED BY:  **03/01/17**  
 City Engineer R.C.E 53350 Date

Revised:

**M-17**



PLAN

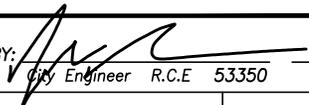
NOTES:

1. SPACING AND LOCATION TO BE DESIGNATED BY THE CITY ENGINEER.
2. ANY TREES IN PARKSTRIPS MUST BE APPROVED BY THE PARKS SUPERINTENDENT DURING THE SITE PLAN REVIEW PROCESS.

CITY of CORCORAN • Department of Public Works

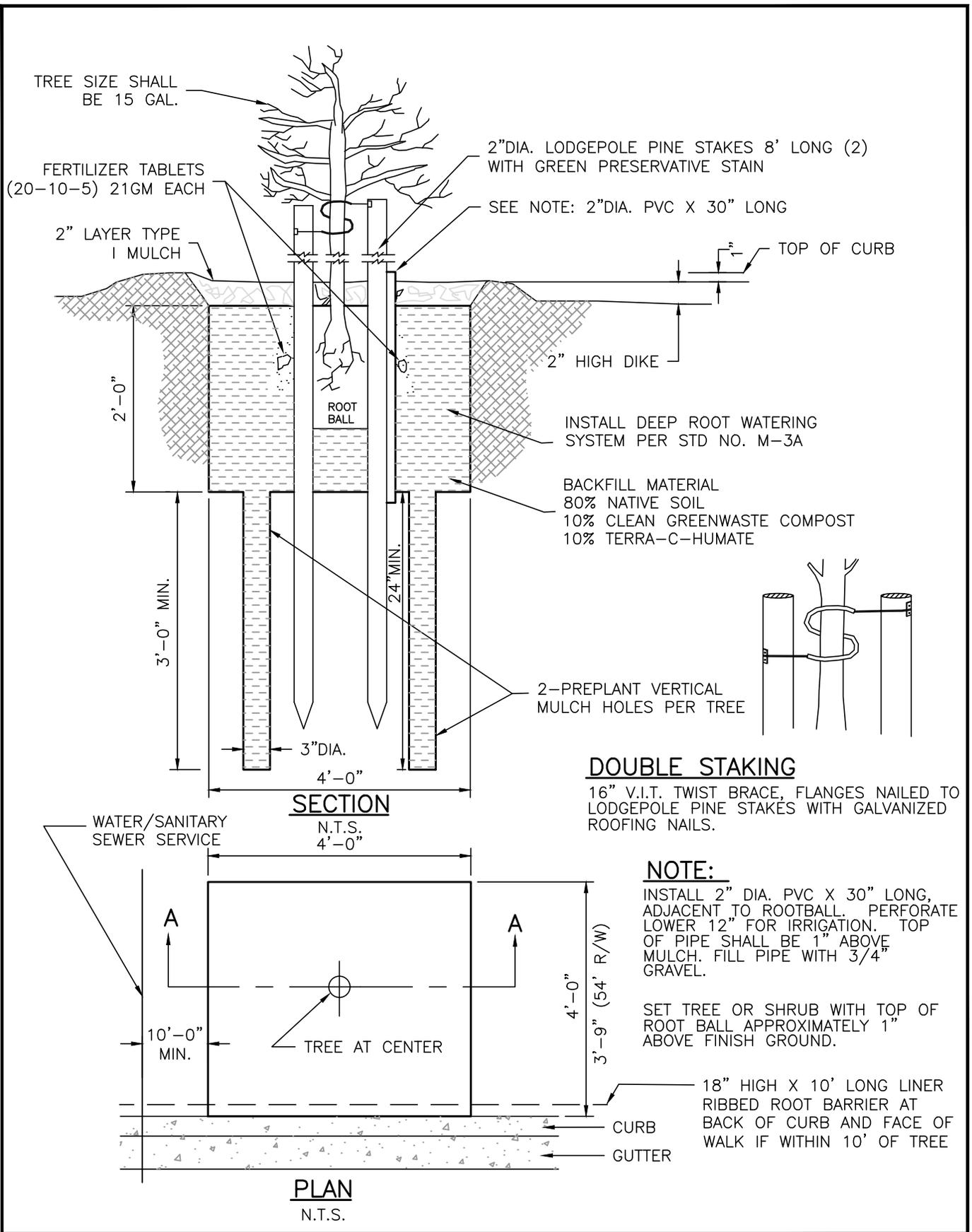
Standard Drawing for:

**48 IN. TREE WELL**

APPROVED BY:  **03/01/17**  
City Engineer R.C.E 53350 Date

Revised:

**M-18**



CITY of CORCORAN • Department of Public Works

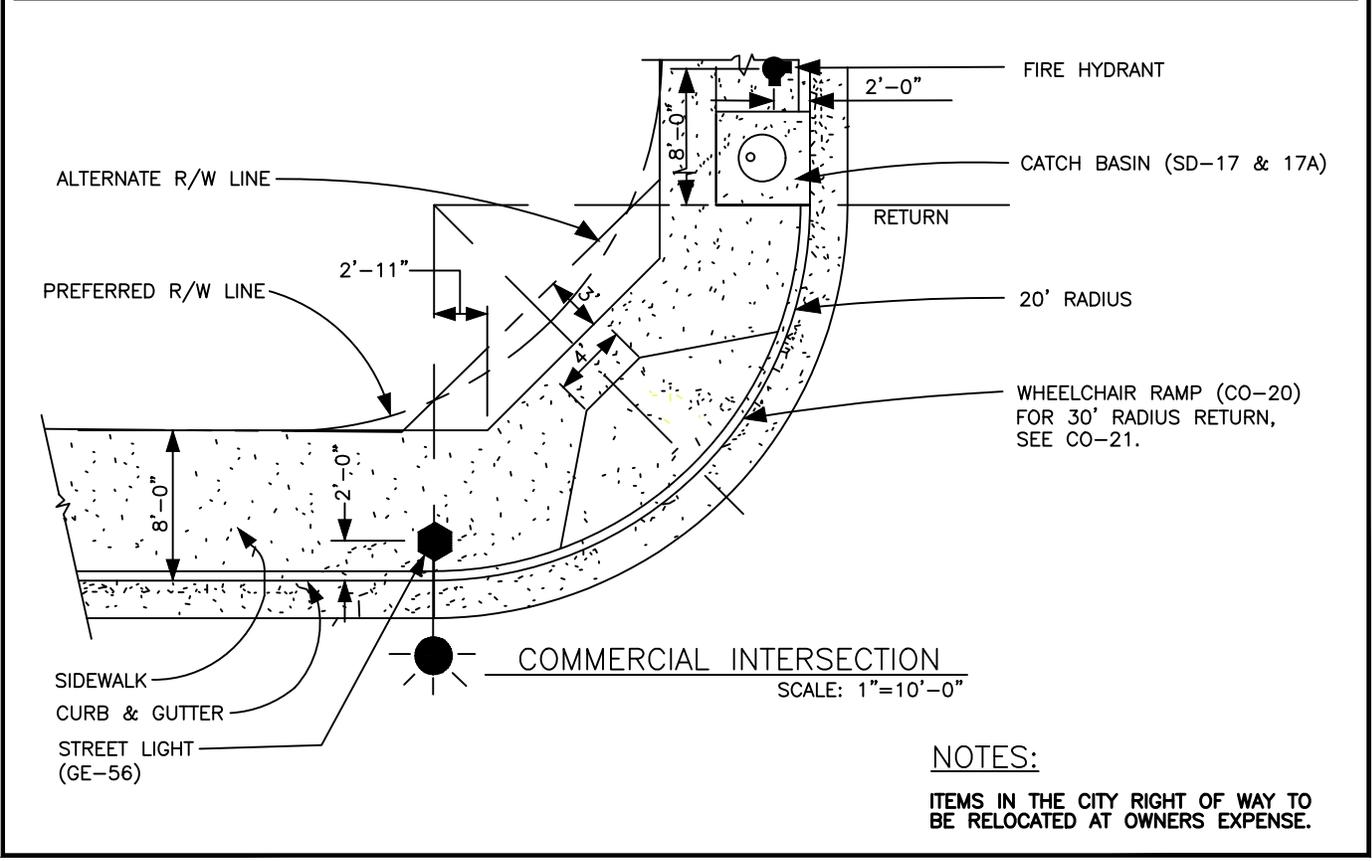
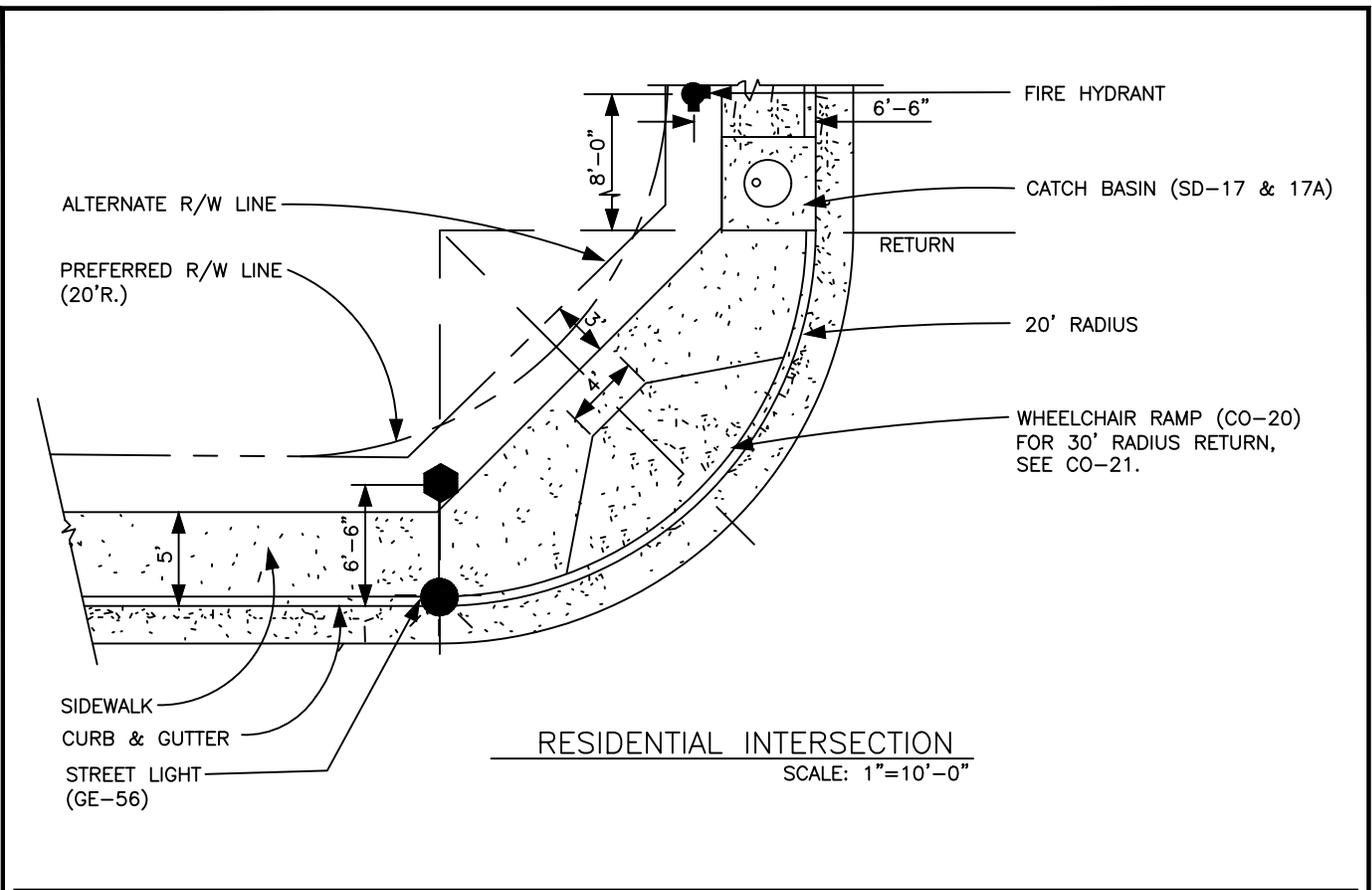
Standard Drawing for:

**STANDARD TREE WELL**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**M-19**



**NOTES:**  
ITEMS IN THE CITY RIGHT OF WAY TO BE RELOCATED AT OWNERS EXPENSE.

<b>CITY of CORCORAN • Department of Public Works</b>	
Standard Drawing for:  <h2 style="text-align: center;">STREET LIGHT AND FIRE HYDRANT LOCATION DETAIL</h2>	APPROVED BY: <small>City Engineer R.C.E 53350</small> <b>03/01/17</b> <small>Date</small> <hr/> Revised: _____
<b>M-20</b>	

STREET CRITERIA

IN ORDER TO CLARIFY OUR REQUIREMENTS FOR THE COMPACTION OF STREET SUBGRADE AND BASE MATERIALS, THE FOLLOWING CRITERIA SHALL APPLY:

MAXIMUM DENSITY – OPTIMUM MOISTURE RELATIONSHIPS, (COMPACTION TESTS), WILL BE DETERMINED IN ACCORDANCE WITH TEST METHOD NO. CALIF. 216 OR 231

SUBGRADE SHALL BE:

1. COMPACTED TO A RELATIVE COMPACTION OF 95 PERCENT FOR ALL SOIL MATERIAL, (COHESIVE, NON-FREE DRAINING MATERIAL) AND NON-COHESIVE, FREE DRAINING MATERIAL.
2. COMPACTED TO A RELATIVE COMPACTION OF 95 PERCENT FOR GRANULAR MATERIAL, (NON-COHESIVE, FREE DRAINING MATERIAL).

AGGREGATE BASE SHALL BE COMPACTED TO A 95 PERCENT RELATIVE COMPACTION.

ASPHALT CONCRETE PAVEMENT SHALL BE COMPACTED TO A 95 PERCENT RELATIVE COMPACTION.

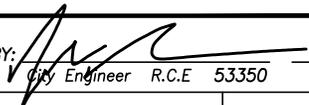
CLASS I, II, AND III BACKFILL FOR TRENCHES SHALL BE COMPACTED TO 95 PERCENT, RELATIVE COMPACTION WITHIN TOP 3 FEET IN STREET SECTION, AND 90 PERCENT, RELATIVE COMPACTION IN REMAINDER OF TRENCH.

THE REQUIREMENTS FOR MAXIMUM DENSITY–OPTIMUM MOISTURE RELATIONSHIPS FOR ALL OTHER FILLS AS SPECIFIED IN THE GRADING ORDINANCE SHALL APPLY TO THOSE FILLS PLACED ON PRIVATE PROPERTIES OUTSIDE OF STREET RIGHTS-OF-WAY.

CITY of CORCORAN • Department of Public Works

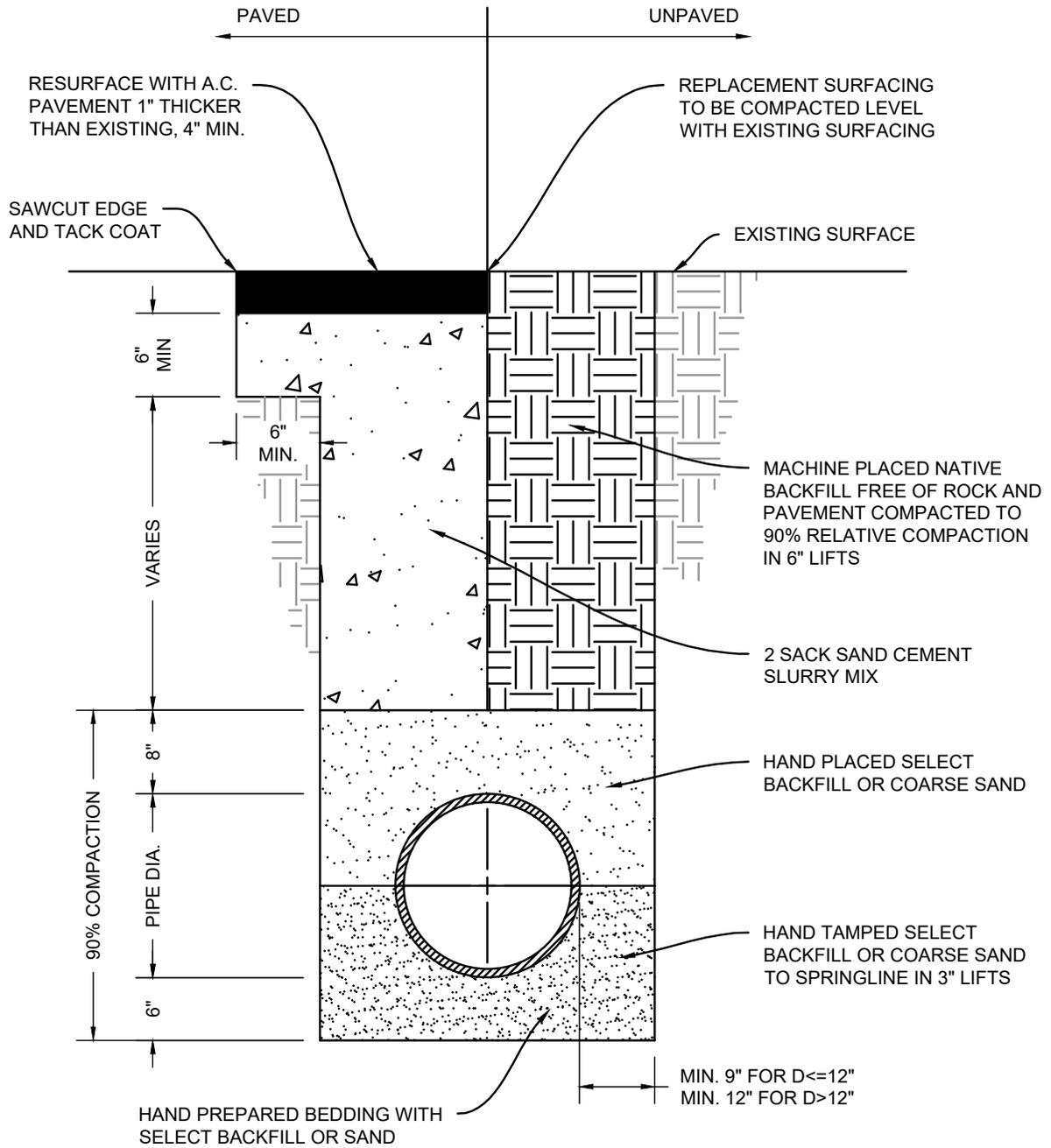
Standard Drawing for:

**STREET CRITERIA**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**ST-1**



HAND PREPARED BEDDING WITH SELECT BACKFILL OR SAND

SEWER MAIN

UP THRU 8" PIPE - 36" COVER  
 UP THRU 12" PIPE - 42" COVER  
 OVER 12" PIPE - 48" COVER  
 COVER SHOWN IS MINIMUM.

WATER MAIN

UP THRU 6" PIPE - 36" COVER  
 UP THRU 8" PIPE - 42" COVER  
 OVER 10" PIPE - 48" COVER  
 COVER SHOWN IS MINIMUM.

NOTE:

MUST PROVIDE COMPACTION TEST FOR ALL SOIL BACKFILL UNLESS USING 2 SACK SAND CEMENT SLURRY.

CITY OF CORCORAN • DEPARTMENT OF PUBLIC WORKS

STANDARD DRAWING FOR:

TRENCH BACKFILL

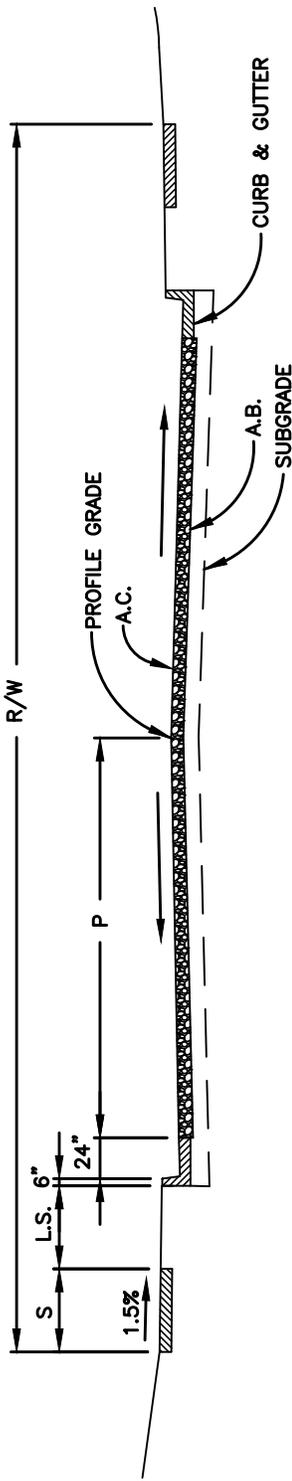
APPROVED BY:

*[Signature]*  
 ORFIL MUNIZ, P.E. 88165,  
 CITY ENGINEER

08/17/23  
 Date

REVISED:

**ST-2**



STREET SECTION TABLE

TYPE OF STREET	R/W	T.I.	A.C. (MIN)	A.B. (MIN.)
ARTERIAL	116'-124'	7.5	3"	8"
COLLECTOR	96'-106'	6	3"	6"
MINOR COLLECTOR/LOCAL	56'-60'	5	2"	4"
LOCAL CUL-DE-SAC	56'-60'	5	2"	4"

**LEGEND**

- R/W -- -- -- RIGHT-OF-WAY -- INCREASE IN "P" FROM THE VALUES GIVEN IN THE ABOVE TABLE WILL REQUIRE A CORRESPONDING INCREASE IN R/W.
- A.C. & A.B. -- MIN. GIVEN. AN INCREASED THICKNESS WILL BE REQUIRED AS FOLLOWS:
  1. BASED UPON TRAFFIC INDEX CALCULATIONS APPROVED BY THE CITY OF CORCORAN.
  2. TESTS SHALL BE TAKEN AT EVERY INTERSECTION OR EVERY 500' WHICH EVER IS LESS FOR "R" VALUES AND RESULTS SHALL BE SUBMITTED WITH DESIGN CALCULATIONS.
- P -- -- -- PAVED WIDTH -- 1/2 ST.
- L.S. -- -- -- LANDSCAPE AREA
- S -- -- -- SIDEWALK -- A FULL WIDTH SIDEWALK WILL BE REQUIRED ON COMMERCIAL STREETS DESIGNATED BY THE CITY ENGINEER AS PEDESTRIAN ORIENTED
- T.I. -- -- -- TRAFFIC INDEX -- A CONSTANT USED IN THE DESIGN OF FLEXIBLE PAVEMENT BASED ON THE ESTIMATED VOLUME ON TRUCK TRAFFIC (EWL) T.I. VALUES SPECIFIED ARE MINIMUMS ALLOWED -- IN NO CASE WILL A STREET SECTION DESIGN BE APPROVED USING T.I. VALUES LESS THAN THE MINIMUM LISTED ABOVE.

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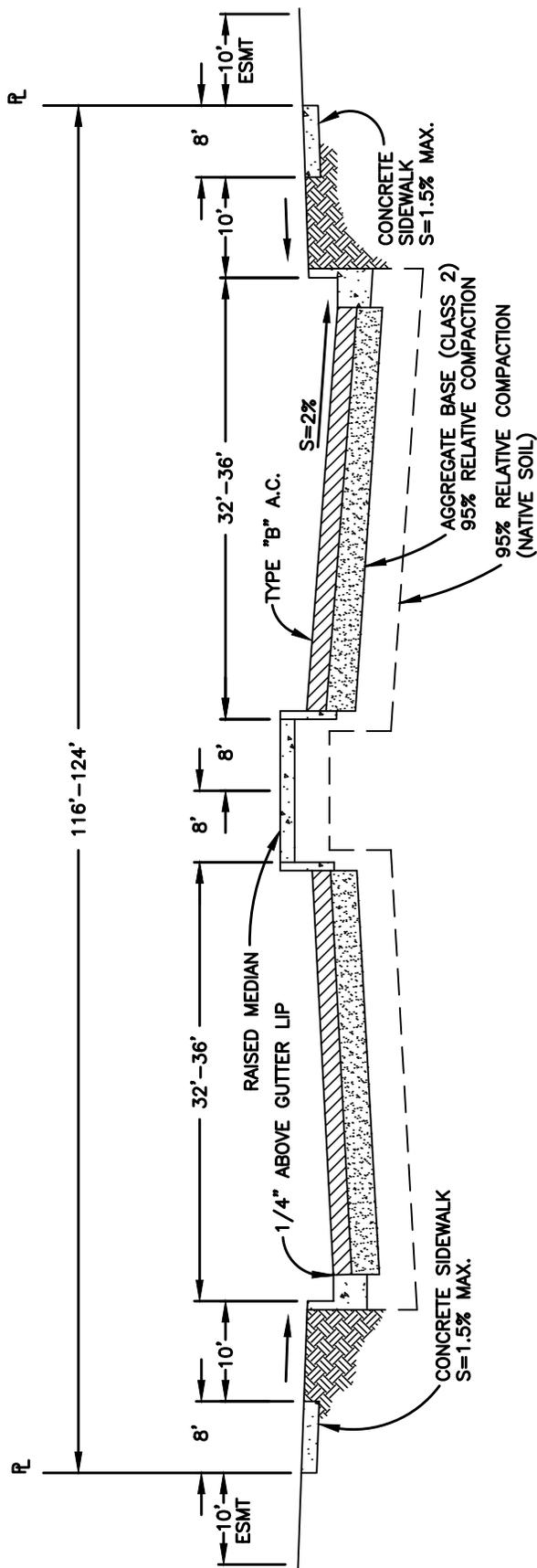
Standard Drawing for:

**STREET SUMMARY SHEET**

APPROVED BY:  03/01/17  
 City Engineer R.C.E 53350 Date

Revised:

**ST-3**



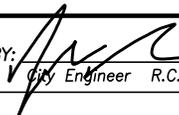
**NOTES:**

1. MEDIAN STRIP FINISH SHALL BE AS DIRECTED BY THE CITY ENGINEER.
2. SEE STANDARDS FOR STRUCTURAL SECTION REQUIREMENTS.
3. ASPHALT CONCRETE LEVELING COURSES SHALL BE CONSTRUCTED WITH 3/4" MAXIMUM, MEDIUM GRADED MATERIAL. FINISH COURSE SHALL BE CONSTRUCTED WITH 3/4" MAXIMUM, MEDIUM GRADED MATERIAL, OR AS APPROVED BY THE CITY ENGINEER.
4. ASPHALT CONCRETE SHALL BE SPREAD AND COMPACTED IN LAYERS NOT TO EXCEED 0.25 FEET COMPACTED THICKNESS.

CITY of CORCORAN • Department of Public Works

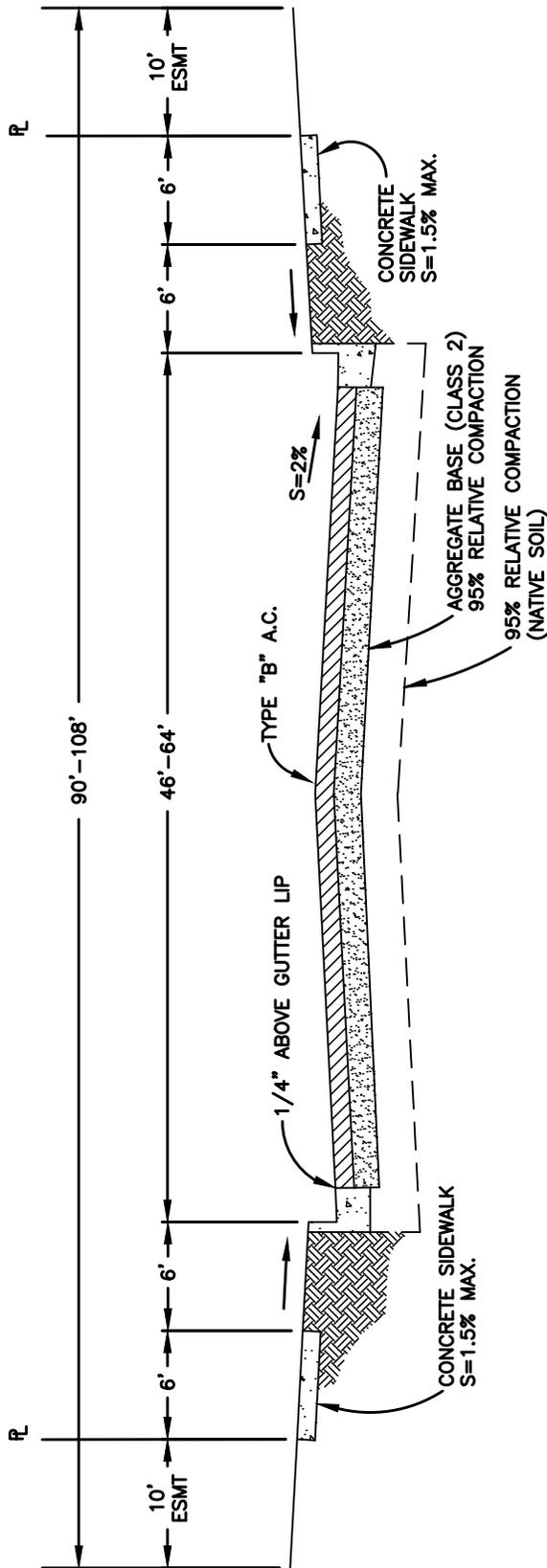
Standard Drawing for:

**ARTERIAL STREET**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**ST-4**



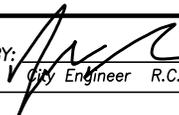
**NOTES:**

1. SEE STANDARDS FOR STRUCTURAL SECTION REQUIREMENTS.
2. ASPHALT CONCRETE LEVELING COURSES SHALL BE CONSTRUCTED WITH 3/4" MAXIMUM, MEDIUM GRADED MATERIAL. FINISH COURSE SHALL BE CONSTRUCTED WITH 1/2" MAXIMUM, MEDIUM GRADED MATERIAL, OR AS APPROVED BY THE CITY ENGINEER.
3. ASPHALT CONCRETE SHALL BE SPREAD AND COMPACTED IN LAYERS NOT TO EXCEED 0.25 FEET COMPACTED THICKNESS.

CITY of CORCORAN • Department of Public Works

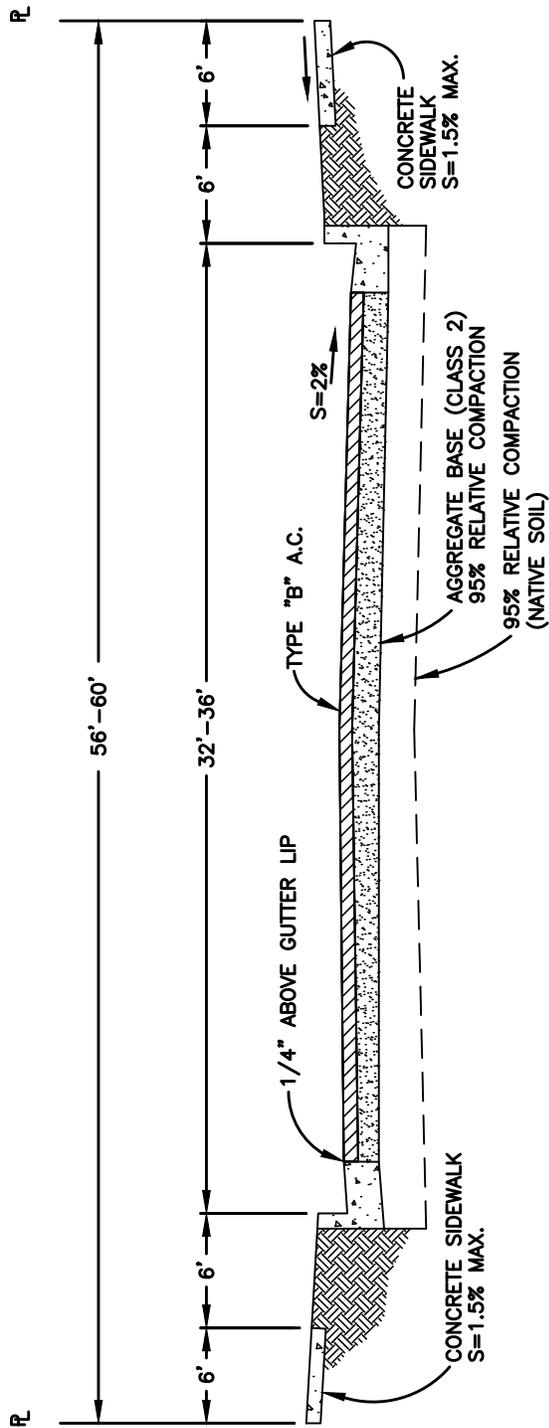
Standard Drawing for:

**COLLECTOR STREET**

APPROVED BY:  03/01/17  
 City Engineer R.C.E 53350 Date

Revised:

**ST-5**



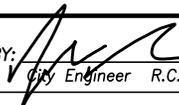
**NOTE:**

1. SEE STANDARDS FOR STRUCTURAL SECTION REQUIREMENTS.
2. ASPHALT CONCRETE LEVELING COURSES SHALL BE CONSTRUCTED WITH 3/4" MAXIMUM, MEDIUM GRADED MATERIAL. FINISH COURSE SHALL BE CONSTRUCTED WITH 1/2" MAXIMUM, MEDIUM GRADED MATERIAL, OR AS APPROVED BY THE CITY ENGINEER.
3. ASPHALT CONCRETE SHALL BE SPREAD AND COMPACTED IN LAYERS NOT TO EXCEED 0.25 FEET COMPACTED THICKNESS.

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**MINOR COLLECTOR - LOCAL STREET**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**ST-6**

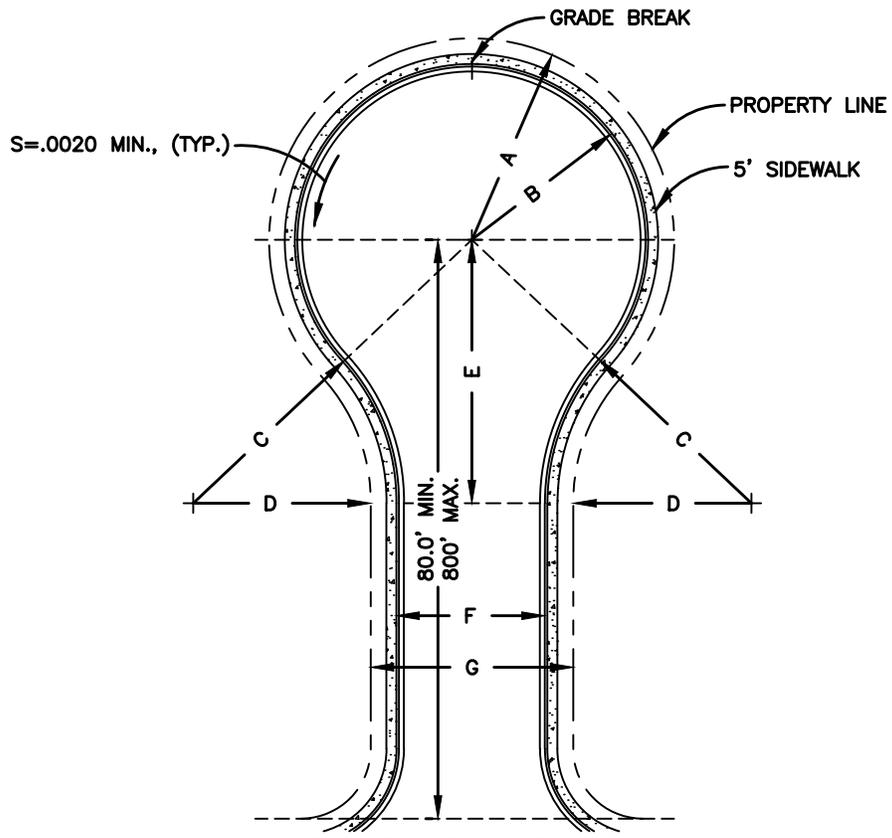
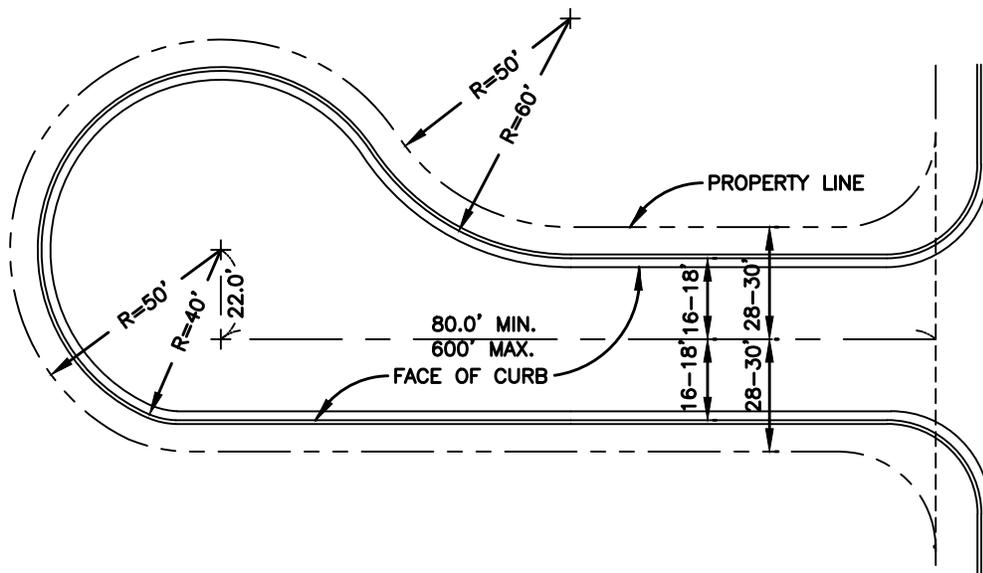


TABLE OF DIMENSIONS

TYPE	A	B	C	D	E	F	G
60' R/W	50'	40'	60'	50'	60'	36'	60'
56' R/W	50'	40'	60'	50'	62.58'	32'	56'



CITY of CORCORAN • Department of Public Works

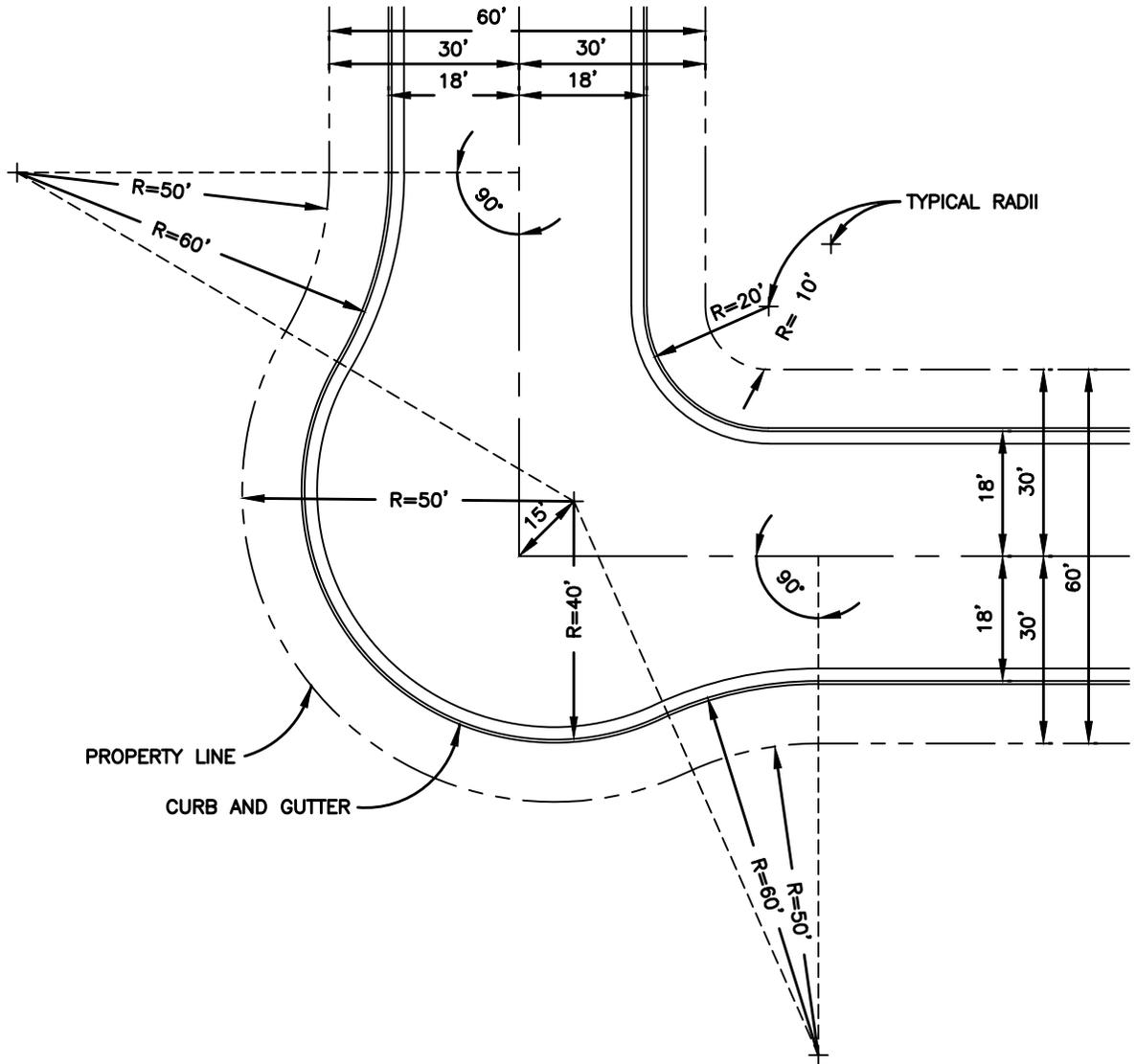
Standard Drawing for:

**TYPICAL CUL-DE-SAC**

APPROVED BY:  **03/01/17**  
 City Engineer R.C.E 53350 Date

Revised:

**ST-7**



CITY of CORCORAN • Department of Public Works

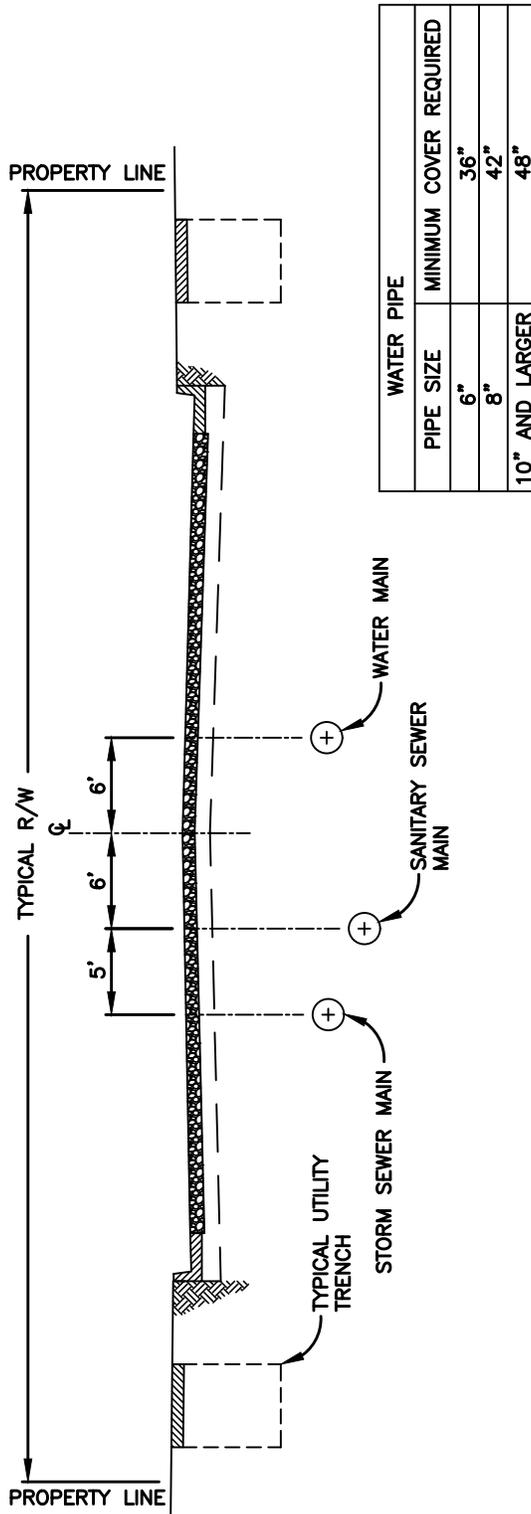
Standard Drawing for:

**TYPICAL STREET BULB**

APPROVED BY:  03/01/17  
 City Engineer R.C.E 53350 Date

Revised:

**ST-8**



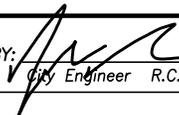
**NOTES:**

1. SEWER MAINS AND STORM SEWER MAINS SHALL BE PLACED SOUTH OR EAST OF THE CENTERLINE OF THE STREET.
2. WATER MAINS SHALL BE PLACED TO THE NORTH OR THE WEST OF THE CENTERLINE OF THE STREET.
3. WATER SERVICES SHALL BE PLACED 2' FROM THE PROPERTY LINE.
4. SEWER LATERALS SHALL BE CENTERED ON LOT.
5. THERE SHALL BE 12' MIN. BETWEEN WATER MAINS AND SEWER MAINS.
6. BACKFILL IN PARKWAY OR STREET INSTALLATIONS SHALL BE AS REQUIRED BY THE CITY ENGINEER.
7. NORMAL LOCATION OF UTILITY TRENCH SHALL BE UNDER THE SIDEWALK AREA.
8. WHERE PRACTICAL, LOCATE UTILITY BOXES AT ALTERNATE LOT LINES FROM WATER METER BOXES TO MINIMIZE CONFLICTS.
9. AT NO TIME WILL THE SANITARY SEWER LINES BE ALLOWED IN A TRENCH OCCUPIED BY PRIMARY OF SECONDARY POWER.
10. EACH UTILITY COMPANY MAY HAVE ONE OR MORE CONDUITS OR CONDUCTORS IN A TRENCH.
11. DEPTHS TO VARY ACCORDING TO CONDUIT SIZES AND LATERAL INTERFERENCE.
12. ALL CONDUCTORS OR CONDUITS SHALL HAVE 3" MIN. CLEARANCE FROM TRENCH SIDEWALL.

CITY of CORCORAN • Department of Public Works

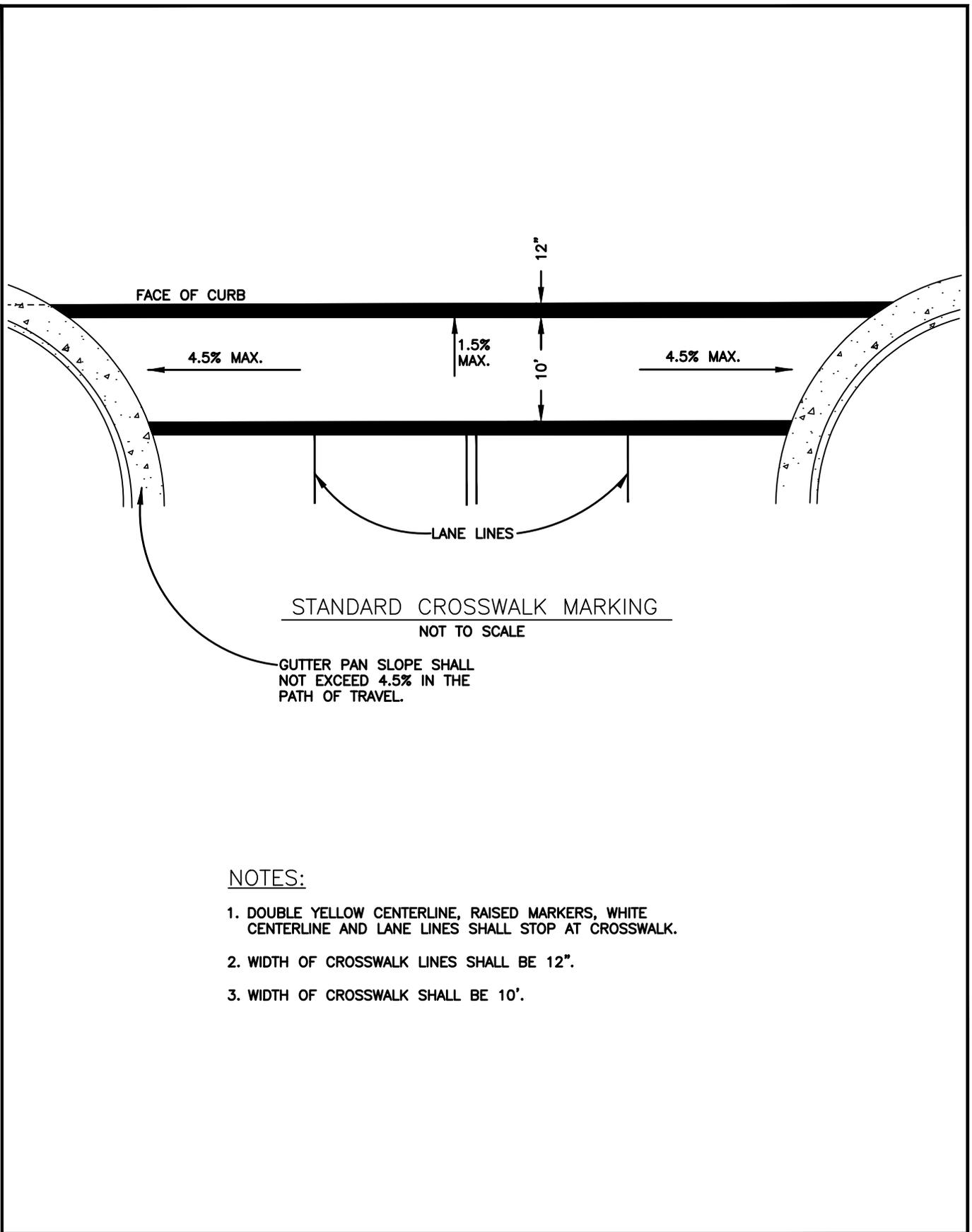
Standard Drawing for:

**UTILITY LOCATIONS**

APPROVED BY:  03/01/17  
 City Engineer R.C.E 53350 Date

Revised:

**ST-9**



NOTES:

1. DOUBLE YELLOW CENTERLINE, RAISED MARKERS, WHITE CENTERLINE AND LANE LINES SHALL STOP AT CROSSWALK.
2. WIDTH OF CROSSWALK LINES SHALL BE 12".
3. WIDTH OF CROSSWALK SHALL BE 10'.

CITY of CORCORAN • Department of Public Works

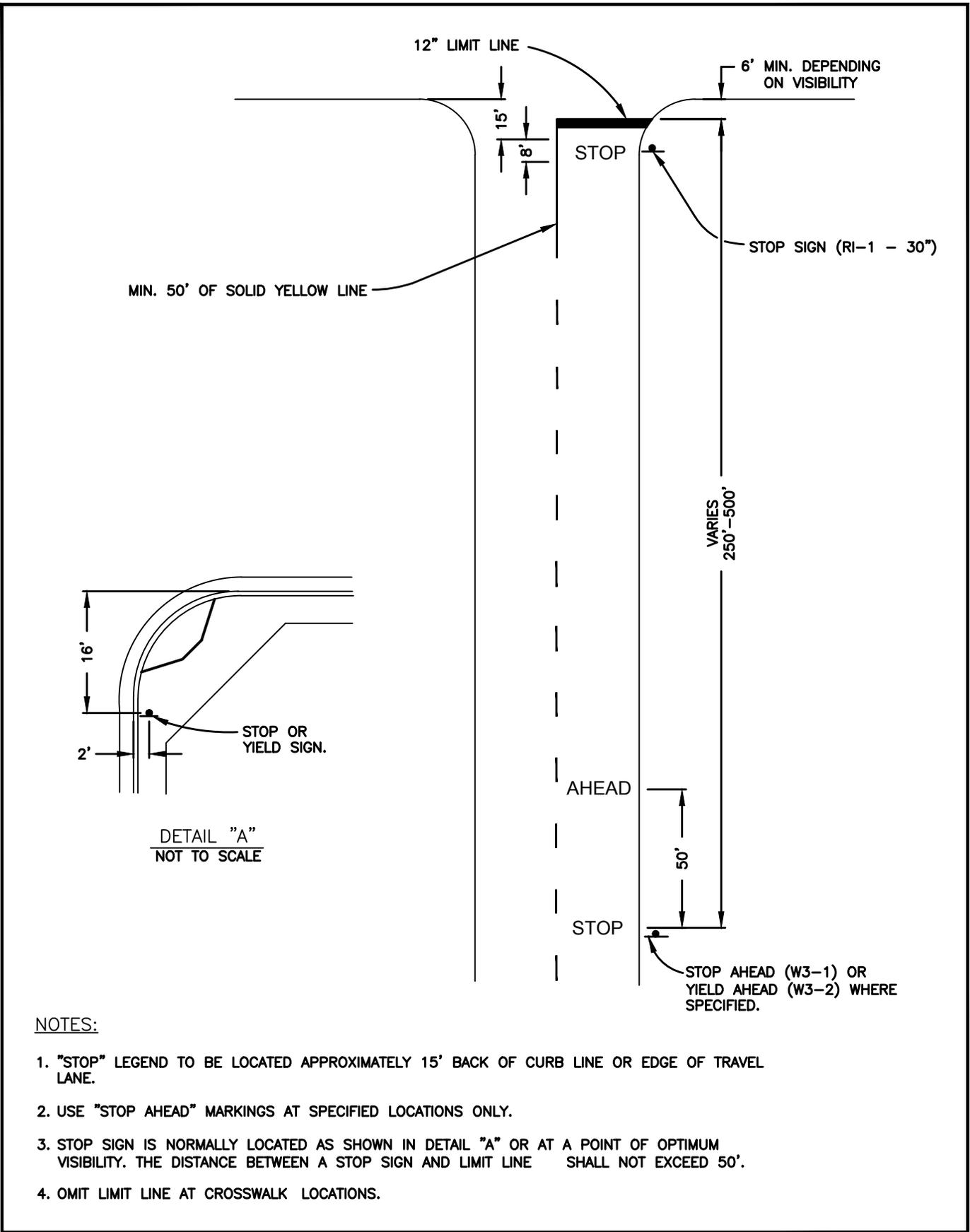
Standard Drawing for:

**CROSSWALK DETAIL**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**ST-10**



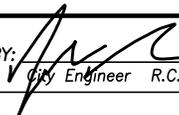
**NOTES:**

1. "STOP" LEGEND TO BE LOCATED APPROXIMATELY 15' BACK OF CURB LINE OR EDGE OF TRAVEL LANE.
2. USE "STOP AHEAD" MARKINGS AT SPECIFIED LOCATIONS ONLY.
3. STOP SIGN IS NORMALLY LOCATED AS SHOWN IN DETAIL "A" OR AT A POINT OF OPTIMUM VISIBILITY. THE DISTANCE BETWEEN A STOP SIGN AND LIMIT LINE SHALL NOT EXCEED 50'.
4. OMIT LIMIT LINE AT CROSSWALK LOCATIONS.

**CITY of CORCORAN • Department of Public Works**

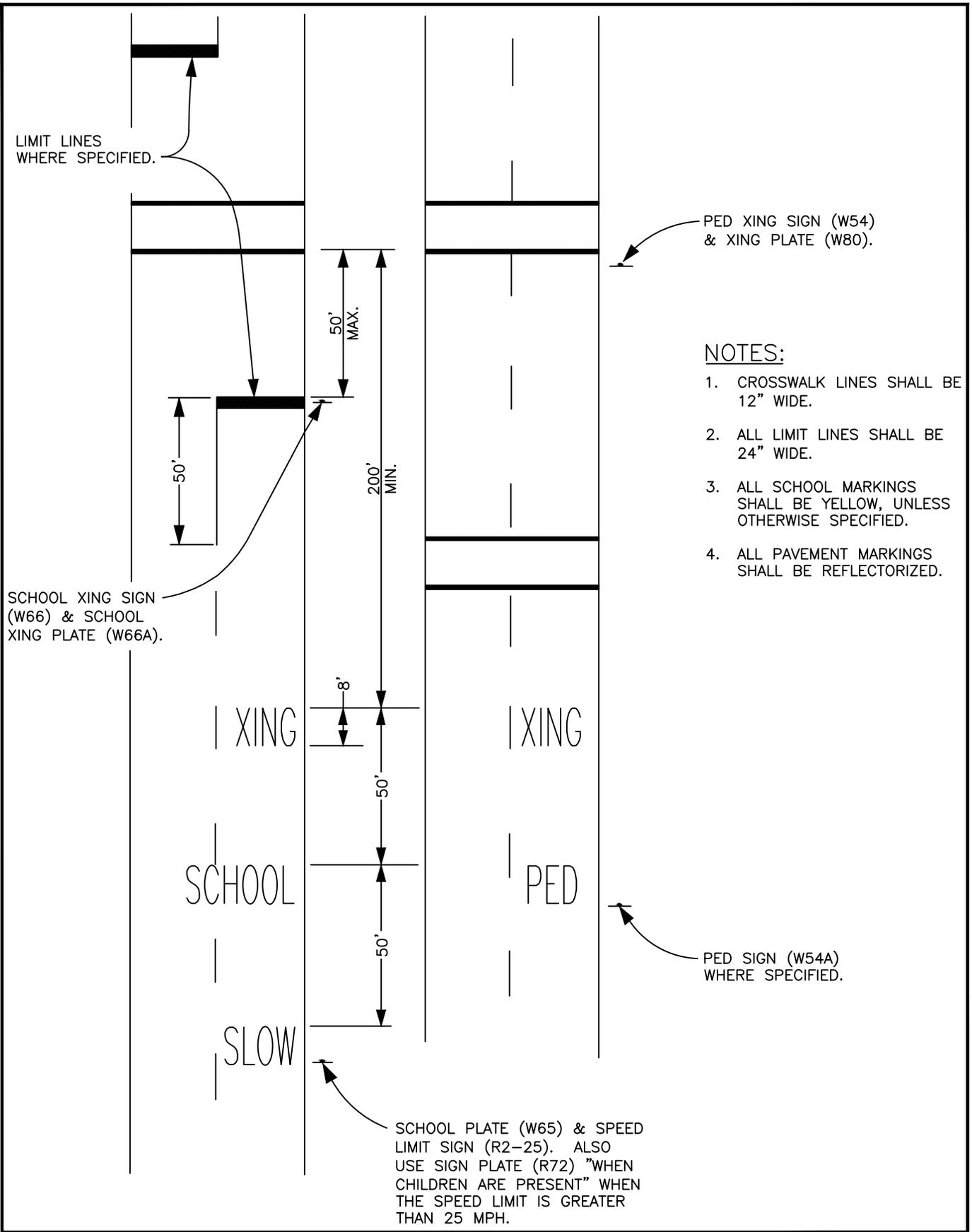
Standard Drawing for:

**STOP & YIELD  
PAVEMENT MARKINGS**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**ST-11**



**NOTES:**

1. CROSSWALK LINES SHALL BE 12" WIDE.
2. ALL LIMIT LINES SHALL BE 24" WIDE.
3. ALL SCHOOL MARKINGS SHALL BE YELLOW, UNLESS OTHERWISE SPECIFIED.
4. ALL PAVEMENT MARKINGS SHALL BE REFLECTORIZED.

**CITY of CORCORAN • Department of Public Works**

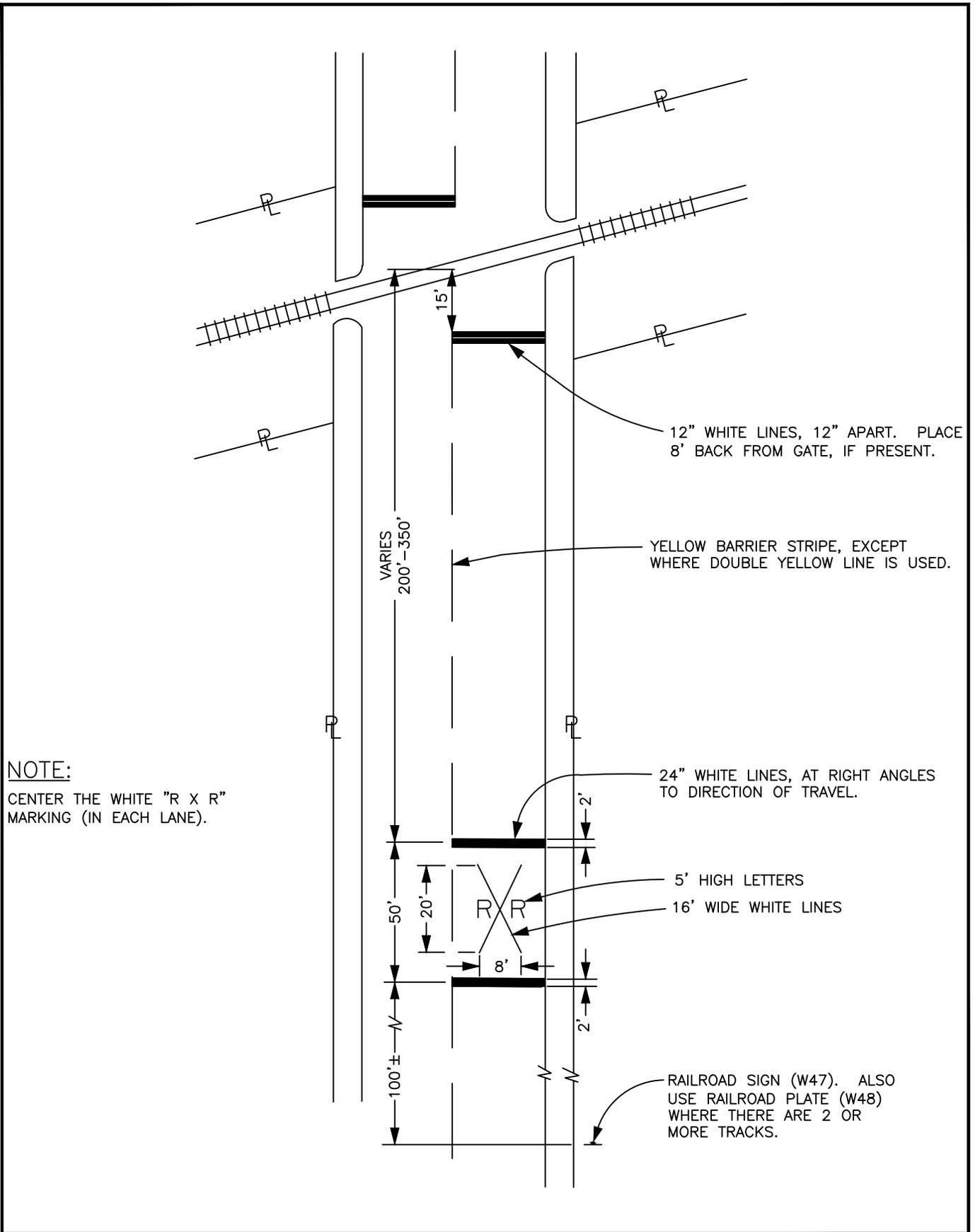
Standard Drawing for:

**SCHOOL X-ING AND PEDESTRIAN X-ING PAVEMENT MARKINGS**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**ST-12**



CITY of CORCORAN • Department of Public Works

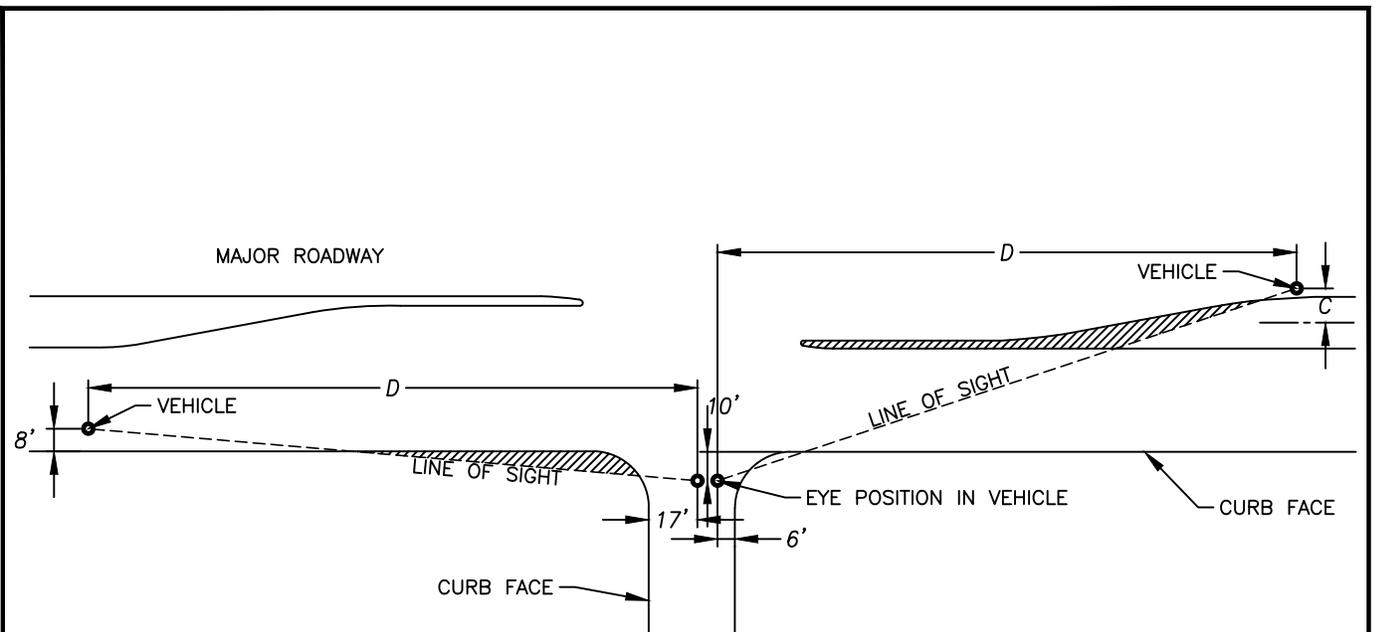
Standard Drawing for:

**RAILROAD X-ING  
PAVEMANT MARKINGS**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**ST-13**



**NOTES:**

1. HATCHED AREAS SHALL BE LIMITED TO VEGETATION THAT WILL NOT EXCEED 30" IN HEIGHT ABOVE PAVEMENT.
2. DISTANCE "C" - 2' WITH CENTERLINE OR  $\frac{1}{2}$  MEDIAN WIDTH PLUS 3'.
3. SEE STREET SPACING STANDARD IN LANDSCAPE DESIGN MANUAL FOR TREE PLACEMENT GUIDELINES.

	LOCAL	COLLECTOR	ARTERIAL	HIGH SPEED ARTERIAL
	DESIGN SPEED (M.P.H.)			
	30	40	50	60
D	205'	308'	431'	590'

**CITY of CORCORAN • Department of Public Works**

Standard Drawing for:

**SIGHT DISTANCE**

APPROVED BY:  **03/01/17**  
City Engineer R.C.E 53350 Date

Revised:

**ST-14**

1. REASON FOR CURB AND GUTTER:

CURB AND GUTTER IS REQUIRED TO PROTECT THE EDGE OF THE PAVEMENT, TO CHANNEL STORM DRAINAGE TO COLLECTION POINTS, TO DEFINE THE RIGHT-OF-WAY FOR VEHICULAR TRAFFIC, TO MAKE BETTER USE OF THE CITY'S STREET SWEEPING PROGRAM, AND TO PROTECT PEDESTRIAN SIDEWALK TRAFFIC. IT IS OUR OPINION THAT CURB AND GUTTER REDUCES THE CITY'S STREET MAINTENANCE COSTS. A SECONDARY BENEFIT IS THAT IT ALSO ESTABLISHES REFERENCE POINTS FOR PROPERTY LINES AND SHOWS WHERE UNDERGROUND UTILITIES ARE STUBBED OUT.

2. REASON FOR SIDEWALKS

THE PRINCIPAL REASON FOR A SIDEWALK IS PEDESTRIAN SAFETY. THE SIDEWALK GIVES THE PEDESTRIAN A PLACE TO WALK OUTSIDE OF THE VEHICULAR TRAVEL LANES. THE CITY'S EXPERIENCE HAS BEEN THAT GRADED OR GRAVELED AREAS HAVE NOT BEEN A SATISFACTORY REPLACEMENT FOR SIDEWALKS AS PROPERTY OWNERS' PLANTS, VEGETATION, LANDSCAPING, OR FENCES FORCE PEOPLE INTO THE STREET.

3. REQUIREMENT BY PERMIT

GENERALLY, CURB, GUTTER AND SIDEWALK ARE REQUIRED FOR ALL NEW DEVELOPMENT IN THE CITY. THESE REQUIREMENTS OCCUR IN SEVERAL AREAS UNDER CITY PROCEDURES:

A. BUILDING PERMITS

PURSUANT TO TITLE 10 OF THE CORCORAN CITY CODE, ANY PERSON OBTAINING A BUILDING PERMIT IS REQUIRED TO CONSTRUCT CURB, GUTTER AND SIDEWALK ALONG ALL PUBLIC STREET FRONTAGE ADJACENT TO THE LOT. IN ADDITION, THE DEVELOPER IS REQUIRED TO PAVE BETWEEN THE EDGE OF THE EXISTING ROAD AND THE GUTTER.

IN SITUATIONS WHERE IT IS NOT POSSIBLE TO SET GRADES FOR THE IMPROVEMENTS, THE PROPERTY OWNER MUST SIGN A "DEFERRED IMPROVEMENT AGREEMENT" THAT ESSENTIALLY PROVIDES THAT CURB, GUTTER AND SIDEWALK WILL BE INSTALLED UPON DEMAND OF THE CITY. THIS AGREEMENT ALSO AMOUNTS TO AN AUTOMATIC "YES" VOTE IN ANY FUTURE ASSESSMENT DISTRICT FOR STREET IMPROVEMENTS.

B. SUBDIVISIONS

IT IS THE GENERAL POLICY OF THE CITY THAT CURB, GUTTER AND SIDEWALK ARE REQUIRED AS A CONDITION OF APPROVAL OF ANY SUBDIVISION. SIDEWALKS SHALL BE REQUIRED ON BOTH SIDES OF THE STREET UNLESS IT CAN BE SHOWN THROUGH DESIGN OR LOT SIZE (IN EXCESS OF ONE-HALF (1/2) ACRE) THAT ELIMINATION WILL NOT JEOPARDIZE THE PUBLIC SAFETY.

FOR SUBDIVISION BY FINAL MAP, CURB, GUTTER AND SIDEWALK IS REQUIRED AS A CONDITION TO RECORDING THE FINAL MAP. FOR SUBDIVISIONS BY PARCEL MAP, THEY ARE REQUIRED AT THE TIME OF DEVELOPMENT. IT SHOULD BE NOTED THAT THE CITY DOES NOT PERMIT A RESIDENTIAL LOT TO BE CREATED WITHOUT FRONTAGE ON A PUBLIC STREET.

C. USE PERMITS, SITE PLAN REVIEWS, AND VARIANCES

CURB, GUTTER AND SIDEWALK ARE ALSO REQUIRED AS A CONDITION OF APPROVAL OF USE PERMITS, SITE PLAN REVIEWS, AND VARIANCES. IN ADDITION, THE DEVELOPER IS ALSO REQUIRED TO PAVE BETWEEN THE EDGE OF EXISTING PAVEMENT AND THE GUTTER. GENERALLY THE CONDITIONS ON THESE APPLICATIONS ARE MORE OF AN INFORMATION ITEM SINCE MOST WOULD FALL UNDER THE BUILDING PERMIT REQUIREMENTS.

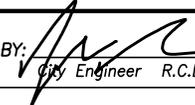
D. PLANNED DEVELOPMENT AND MOBILE HOME PARKS

PLANNED DEVELOPMENT AND MOBILE HOME PARKS ARE TREATED SOMEWHAT DIFFERENTLY IN THAT CURB, GUTTER AND SIDEWALK ARE ONLY REQUIRED ON THE PUBLIC STREET FRONTAGES. INTERNAL ROADS WITHIN THE PROJECTS ARE PRIVATE, THUS NO SIDEWALK REQUIREMENT IS MADE. USUALLY PLANNED DEVELOPMENTS HAVE THEIR OWN INTERNAL WALKWAY SYSTEM.

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**CONCRETE CRITERIA**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**C-1**

4. ADDITIONAL NOTES:

- A. CROSS SECTIONAL AREA OF CURB AND GUTTER – 1.64 sq. ft.
- B. CURB AND GUTTER – 1.64 L.F. PER cu. yd. OF CONCRETE.
- C. AN APPROXIMATE 4" FLOW LINE SHALL BE TROWELED SMOOTH.
- D. ALL BROOMING SHALL BE PARALLEL TO THE DIRECTION OF FLOW.
- E. 1/2" PER-MOLDED JOINT FILLER SHALL BE INSTALLED IN EXPANSION JOINTS AT REGULAR INTERVALS NOT EXCEEDING 60', AT THE BEGINNING AND END OF ALL CURB RETURNS AND ALL DRIVEWAYS AND SHALL BE HELD FIRMLY IN PLACE PRIOR TO PLACING CONCRETE. WEAKENED PLANE JOINTS SHALL BE PLACED AT 10' INTERVALS.
- F. IF SLIP FORM EQUIPMENT IS USED, CONTROL JOINTS SHALL BE SAW CUT TO A DEPTH OF 2" AT LOCATIONS AS SPECIFIED IN NOTE E. THE TOP 3/4" OF THE CONTROL JOINT SHALL BE FILLED WITH A POLYURETHANE SEALANT (SILAFLEXT – 1A OR EQUAL).
- G. ALL WORK TO BE DONE AND ALL MATERIALS SUPPLIED SHALL CONFORM TO THE "STANDARD SPECIFICATIONS, DEPARTMENT OF TRANSPORTATION, STATE OF CALIFORNIA" LATEST EDITION.
- H. HUNT PROCESS CURING COMPOUND SHALL BE APPLIED TO FRESH CONCRETE BY ROLLING, BRUSHING OR SPRAYING.
- I. ALL CONCRETE SHALL BE A MINIMUM 5 SACK MIX AND TEST TO A MINIMUM OF 3200 P.S.I. IN 28 DAYS.

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

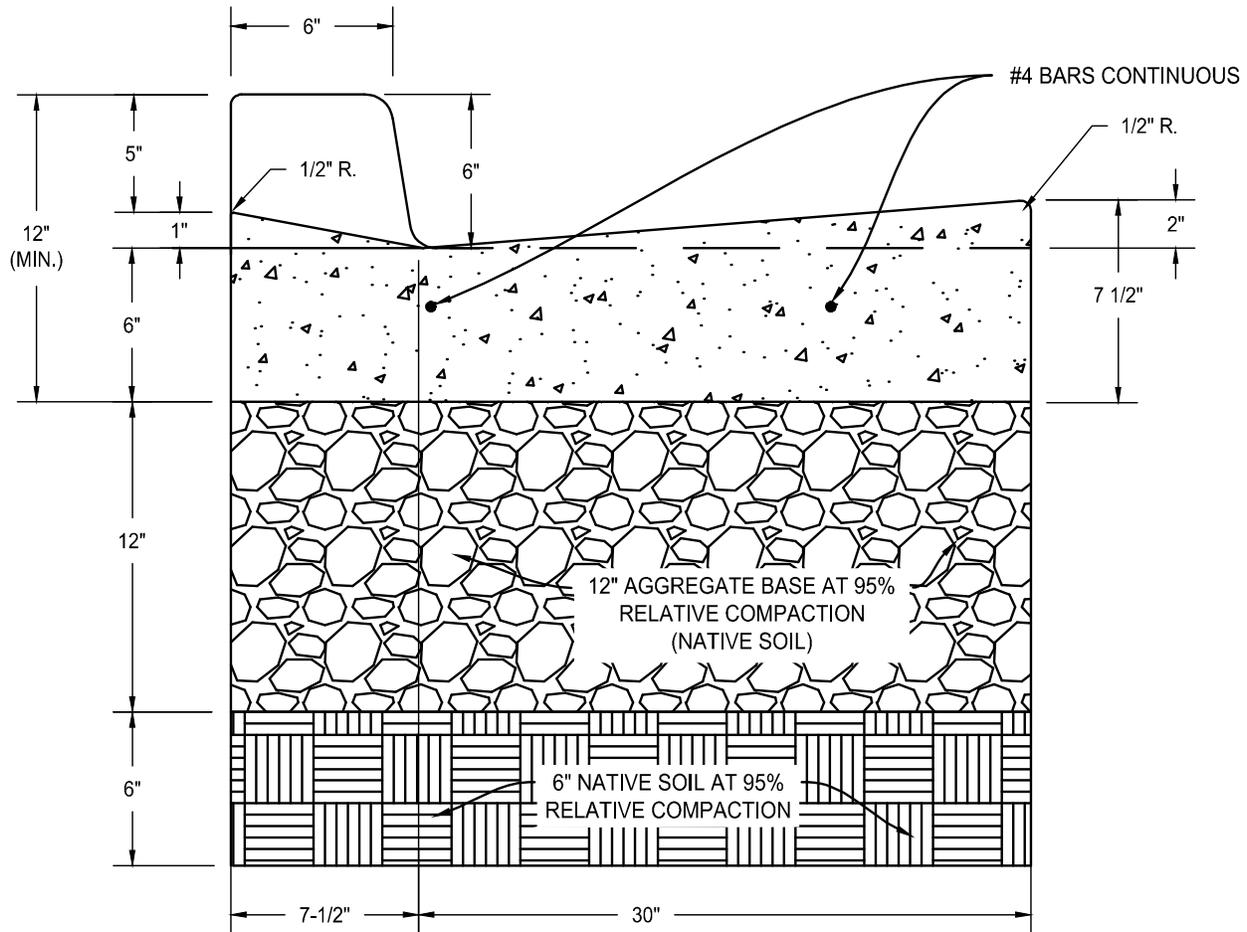
**CONCRETE CRITERIA**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**C-1A**





DEPRESSED CURB

NOTES:

1. ALL CONCRETE SHALL BE CLASS 3 CONCRETE.
2. A WEAKENED PLANE JOINT OR COLD JOINT SHALL BE INSTALLED AT THE END OF CURB RETURNS AND AT THE CENTERLINE OF PROPOSED DRIVE APPROACHES.
3. WHERE ADA ACCESSIBLE PATH CROSSES GUTTER PAN, SLOPE IN THE DIRECTION OF TRAVEL SHALL BE 3% MINIMUM AND 4.5% MAXIMUM.
4. WOOD FORMS SHALL HAVE A NORMAL THICKNESS OF 2", EXCEPT ON CURVE CONSTRUCTION WHERE THE THICKNESS SHALL BE DETERMINED BY THE CITY INSPECTOR.
5. MINIMUM GRADE FOR CURB & GUTTER SHALL NEVER BE LESS THAN .0015 SLOPE, EXCEPT CURVE PORTIONS OF CUL- DE-SAC STREETS WHICH SHALL HAVE .0020 SLOPE MINIMUM.
6. 1" x 2" LINE & GRADE STAKES ARE TO BE SET 3' FROM FACE OF CURB. STAKE SPACING SHALL BE 25' MAX. FOR A SLOPE OF .0015 AND 50' MAX. FOR SLOPES OF .0020 OR MORE ( OR EQUIVALENT CONTROLS).
7. SEE CONCRETE NOTES AND CITY STANDARD SPECIFICATIONS.
8. FILL AND GRADE AREA BEHIND CURB AS DIRECTED BY THE CITY OF CORCORAN.

**CITY OF CORCORAN • DEPARTMENT OF PUBLIC WORKS**

STANDARD DRAWING FOR:

**CURB AND GUTTER -  
DEPRESSED CURB**

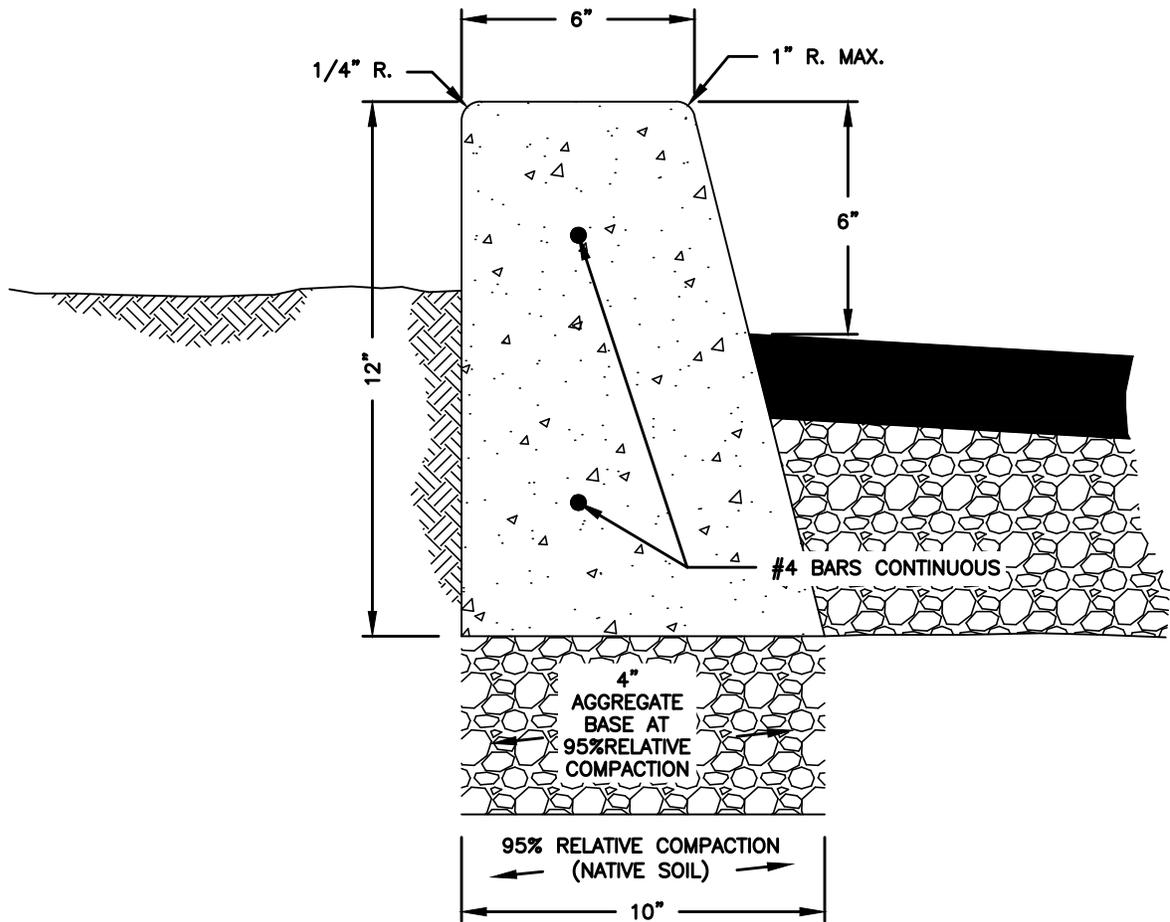
APPROVED BY:

*[Signature]*  
ORFIL MUNIZ, P.E. 88165,  
CITY ENGINEER

1/17/2024  
Date

REVISED:

**C-2A**



NOTES:

1. FORMS TO REMAIN FOR A MIN. OF 24 HOURS.
2. WOOD FORMS SHALL HAVE A NORMAL THICKNESS OF 2", EXCEPT ON CURVE CONSTRUCTION WHERE THE THICKNESS SHALL BE DETERMINED BY THE CITY INSPECTOR.
3. MINIMUM GRADE FOR CURB & GUTTER SHALL NEVER BE LESS THAN .0015 SLOPE, EXCEPT CURVE PORTIONS OF CUL-DE-SAC STREETS WHICH SHALL HAVE .0020 SLOPE MINIMUM.
4. 1"x2" LINE & GRADE STAKES ARE TO BE SET 3' FROM FACE OF CURB. STAKE SPACING SHALL BE 25' MAX. FOR A SLOPE OF .0015 AND 50' MAX. FOR SLOPES OF .0020 OR MORE (OR EQUIVALENT CONTROLS).
5. SEE CONCRETE NOTES AND CITY STANDARD SPECIFICATIONS.

CITY of CORCORAN • Department of Public Works

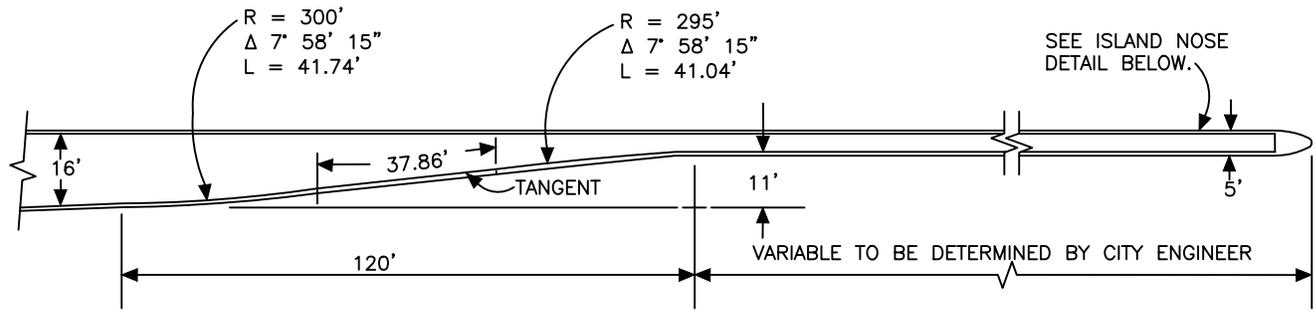
Standard Drawing for:

**MEDIAN CURB**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

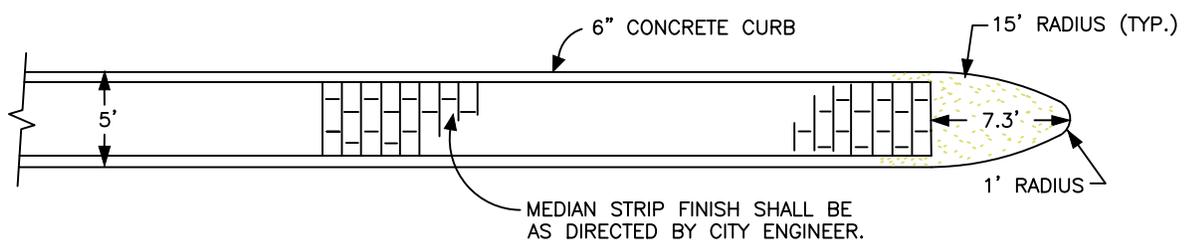
Revised:

**C-3**



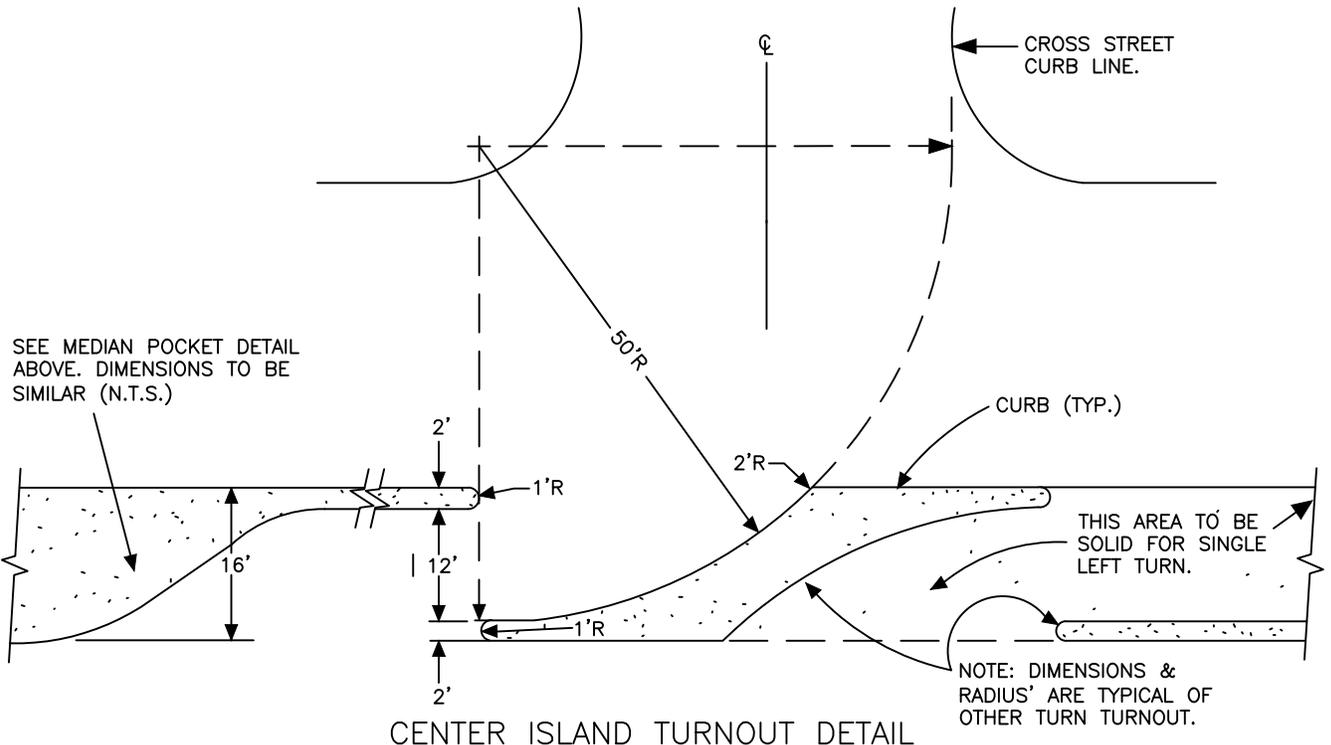
MEDIAN POCKET DETAIL

SCALE: 1"=40'



ISLAND NOSE DETAIL

SCALE: 1"=10'



CENTER ISLAND TURNOUT DETAIL

SCALE: 1"=20'

CITY of CORCORAN • Department of Public Works

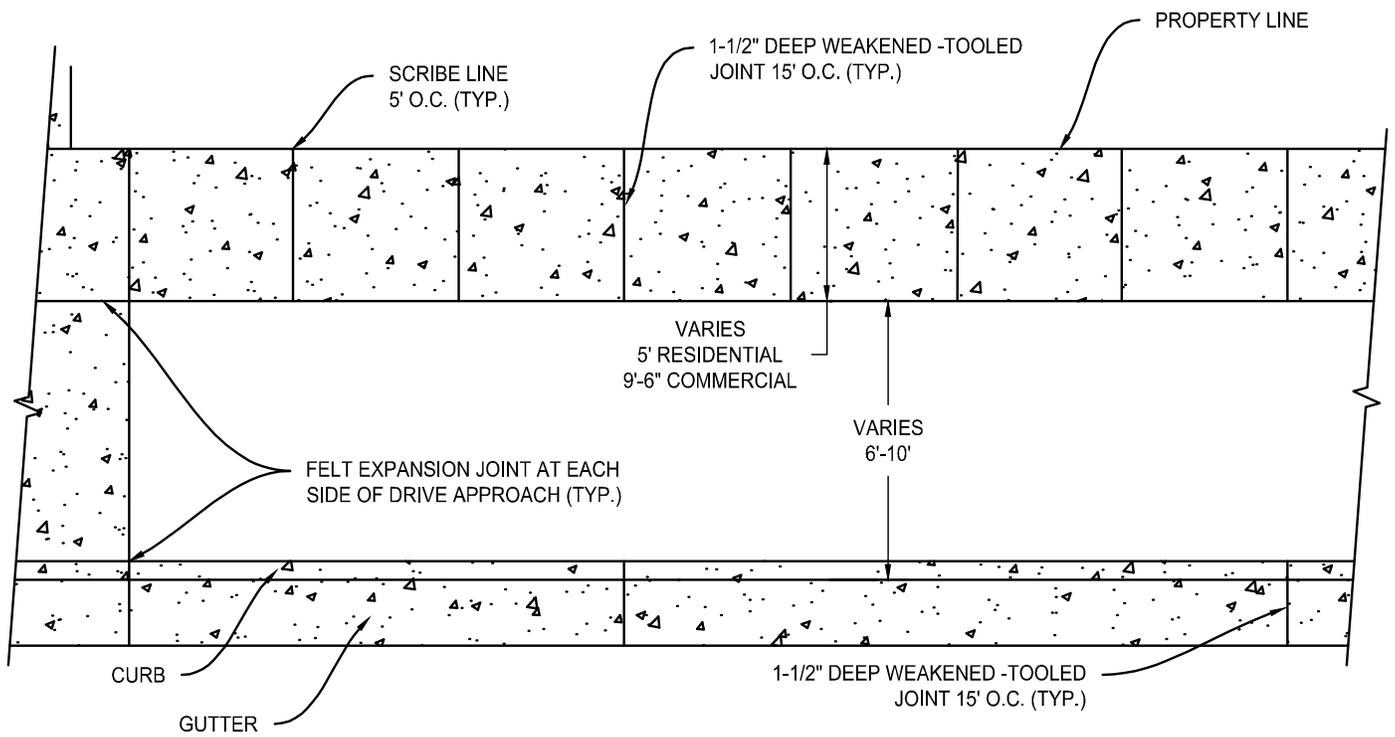
Standard Drawing for:

MEDIAN DETAILS

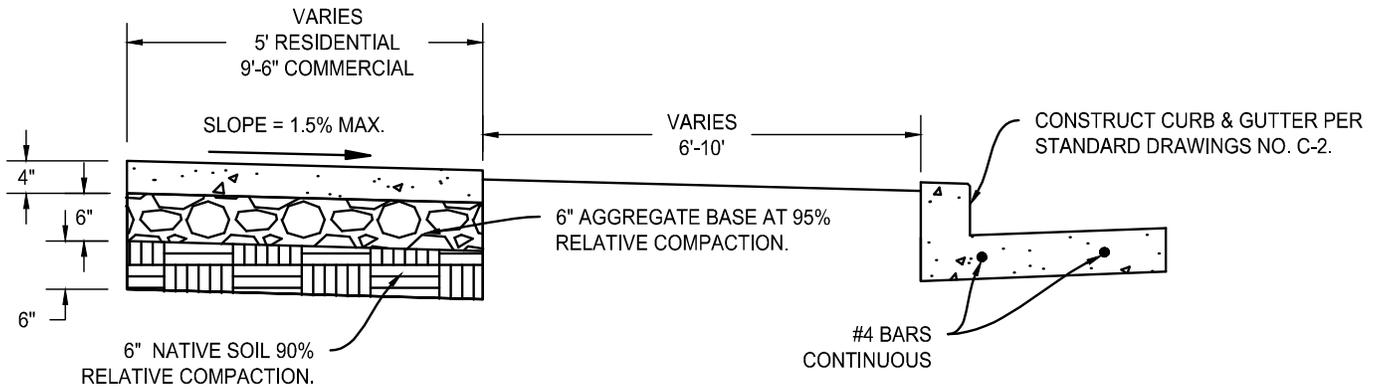
APPROVED BY: *[Signature]* 03/01/17  
 City Engineer R.C.E 53350 Date

Revised:

C-3A



CURB, GUTTER AND SIDEWALK SCRIBE LINE DETAIL  
NOT TO SCALE



CURB, GUTTER AND SIDEWALK CROSS - SECTION  
NOT TO SCALE

NOTES:

1. FORMS TO REMAIN FOR A MIN. OF 24 HOURS.
2. WOOD FORMS SHALL HAVE NOMINAL THICKNESS OF 2", EXCEPT ON CURVE CONSTRUCTION WHERE THE THICKNESS SHALL BE DETERMINED BY THE CITY ENGINEER.
3. SEE CONCRETE NOTES (C-1) AND CITY STANDARD SPECIFICATIONS.
4. EXPANSION JOINTS SHALL BE INSTALLED WITHIN CURVILINEAR SIDEWALKS AT MIN. 60 FEET O.C.
5. CONCRETE SHALL BE MINIMUM FIVE SACK MIX. (3200 P.S.I. MIN. IN 28 DAYS).
6. ALL SIDEWALKS AND RAMPS SHALL BE CURED IN ACCORDANCE WITH THE PROVISIONS OUTLINED IN THE LATEST CALTRANS STANDARD SPECIFICATIONS.

**CITY OF CORCORAN • DEPARTMENT OF PUBLIC WORKS**

STANDARD DRAWING FOR:

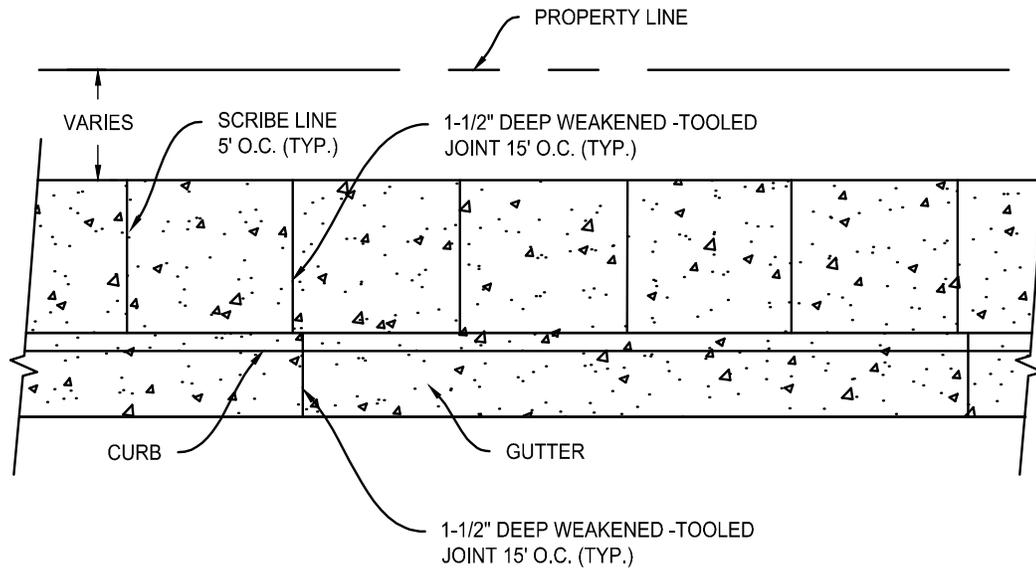
**PARKWAY SIDEWALK  
DETAIL**

APPROVED BY:   
ORFIL MUNIZ, P.E. 88165,  
CITY ENGINEER

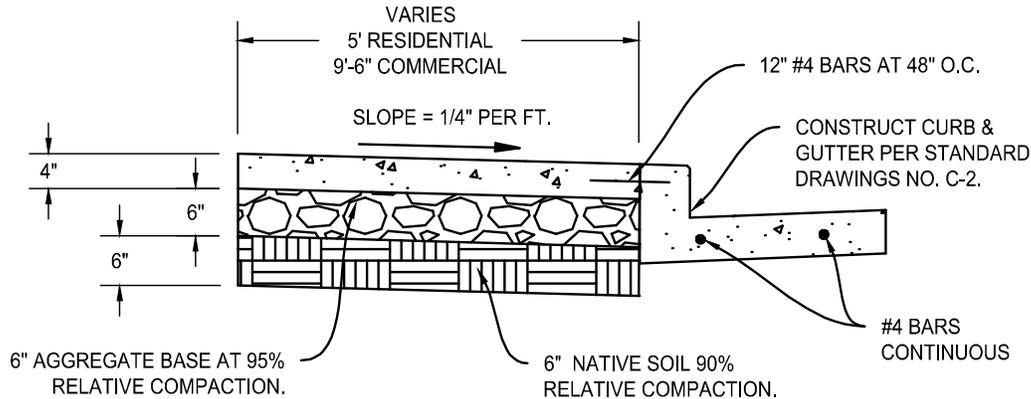
1/17/2024  
Date

REVISED:

**C-4**



CURB, GUTTER AND SIDEWALK SCRIBE LINE DETAIL  
NOT TO SCALE



CURB, GUTTER AND SIDEWALK CROSS - SECTION  
NOT TO SCALE

NOTES:

1. FORMS TO REMAIN FOR A MIN. OF 24 HOURS.
2. WOOD FORMS SHALL HAVE NOMINAL THICKNESS OF 2", EXCEPT ON CURVE CONSTRUCTION WHERE THE THICKNESS SHALL BE DETERMINED BY THE CITY ENGINEER.
3. SEE CONCRETE NOTES (C-1) AND CITY STANDARD SPECIFICATIONS.
4. EXPANSION JOINTS SHALL BE INSTALLED WITHIN CURVILINEAR SIDEWALKS AT MIN. 60 FEET O.C.
5. CONCRETE SHALL BE MINIMUM FIVE SACK MIX. (3200 P.S.I. MIN. IN 28 DAYS).
6. ALL SIDEWALKS AND RAMPS SHALL BE CURED IN ACCORDANCE WITH THE PROVISIONS OUTLINED IN THE LATEST CALTRANS STANDARD SPECIFICATIONS.

**CITY OF CORCORAN • DEPARTMENT OF PUBLIC WORKS**

STANDARD DRAWING FOR:

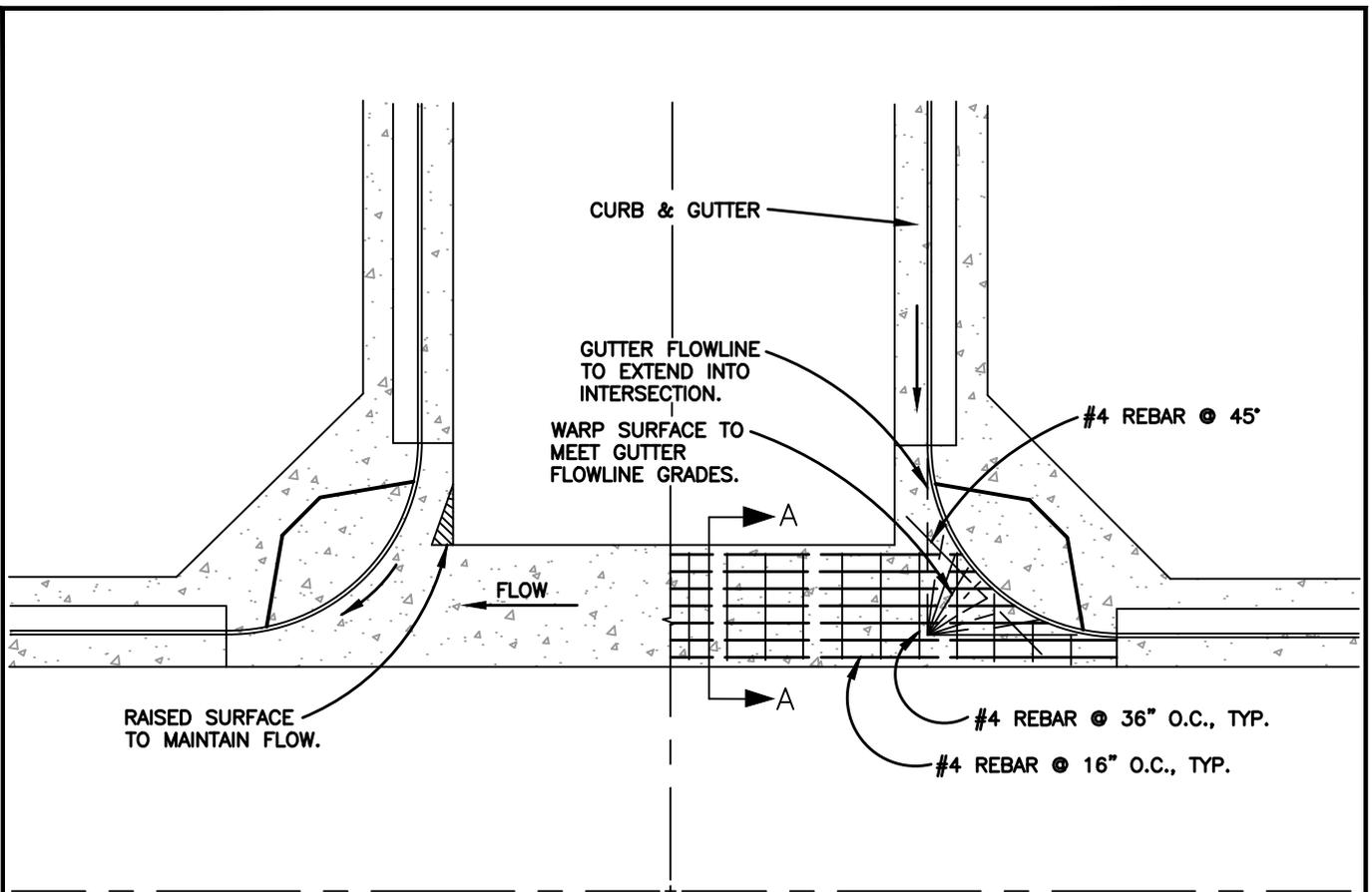
**ADJACENT SIDEWALK  
DETAIL**

APPROVED BY:   
ORFIL MUNIZ, P.E. 88165,  
CITY ENGINEER

1/17/2024  
Date

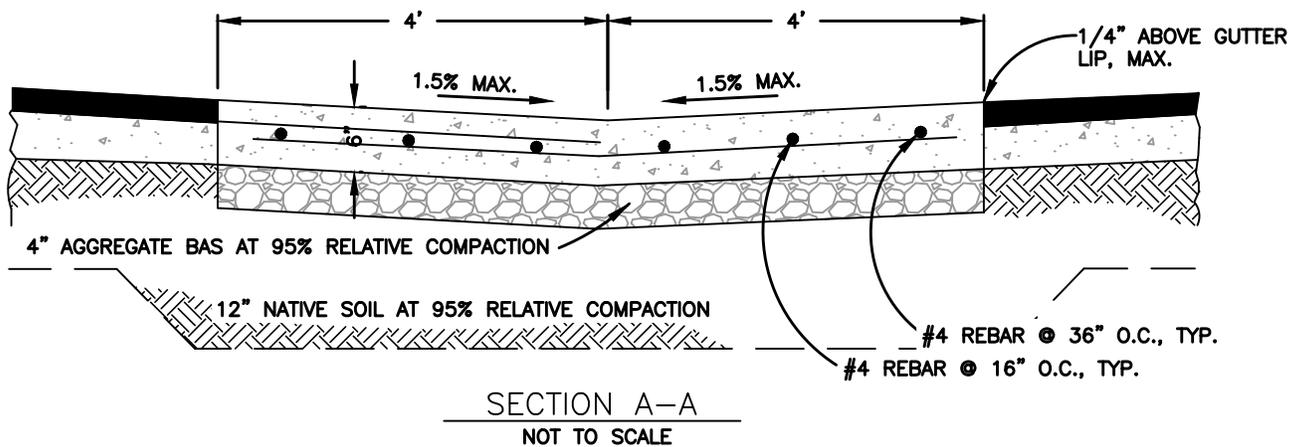
REVISED:

**C-4A**



**NOTE:**

THE VALLEY GUTTER SHALL HAVE A MIN. NET SLOPE OF 0.0030 IN THE DIRECTION OF FLOW.



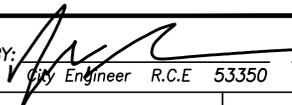
**NOTES:**

1. SEE CONCRETE NOTES AND CITY STANDARD SPECIFICATIONS.
2. CONCRETE SHALL BE MINIMUM FIVE SACK MIX. (3200 P.S.I. MIN. IN 28 DAYS).

**CITY of CORCORAN • Department of Public Works**

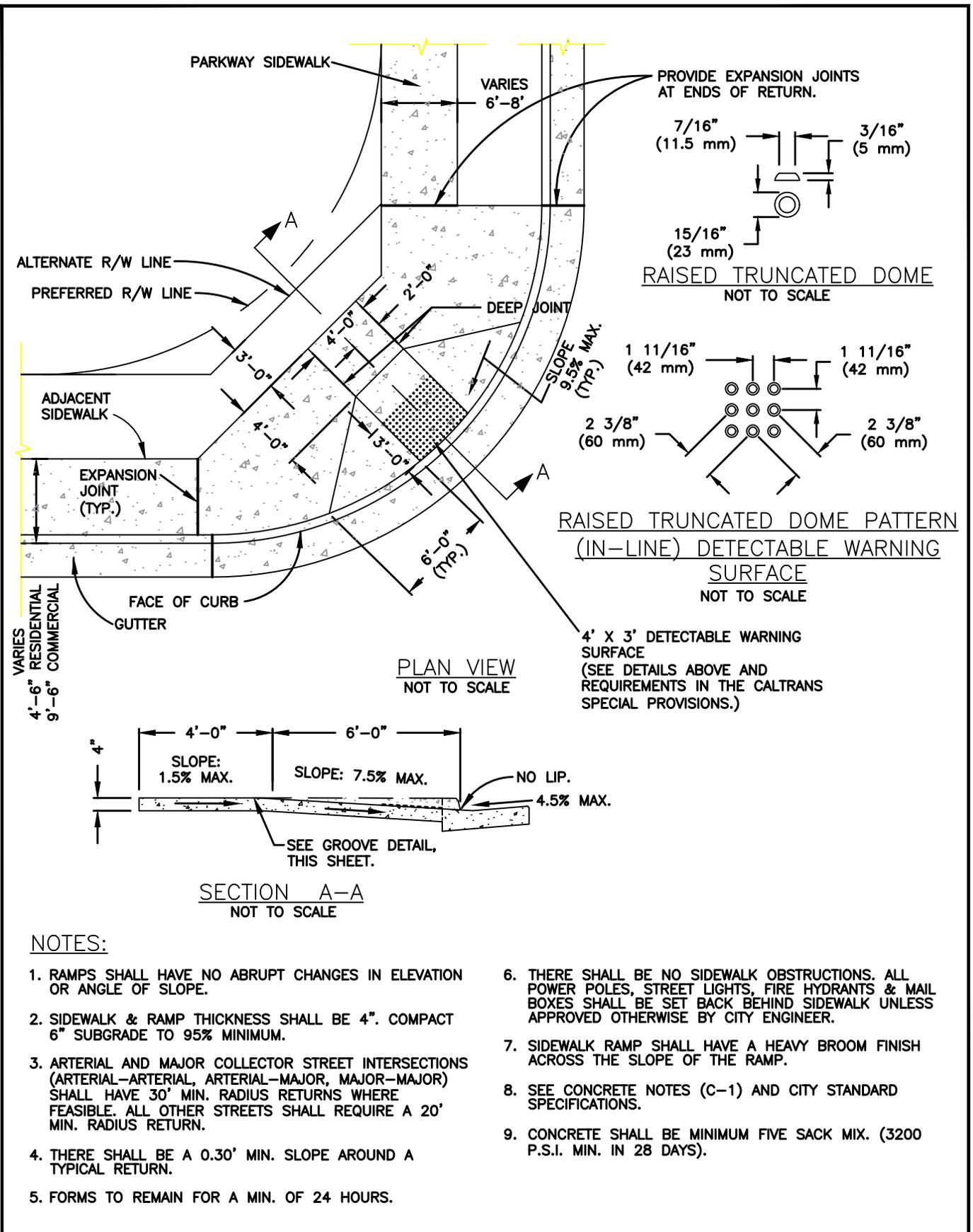
Standard Drawing for:

**VALLEY GUTTER**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**C-5**



CITY of CORCORAN • Department of Public Works

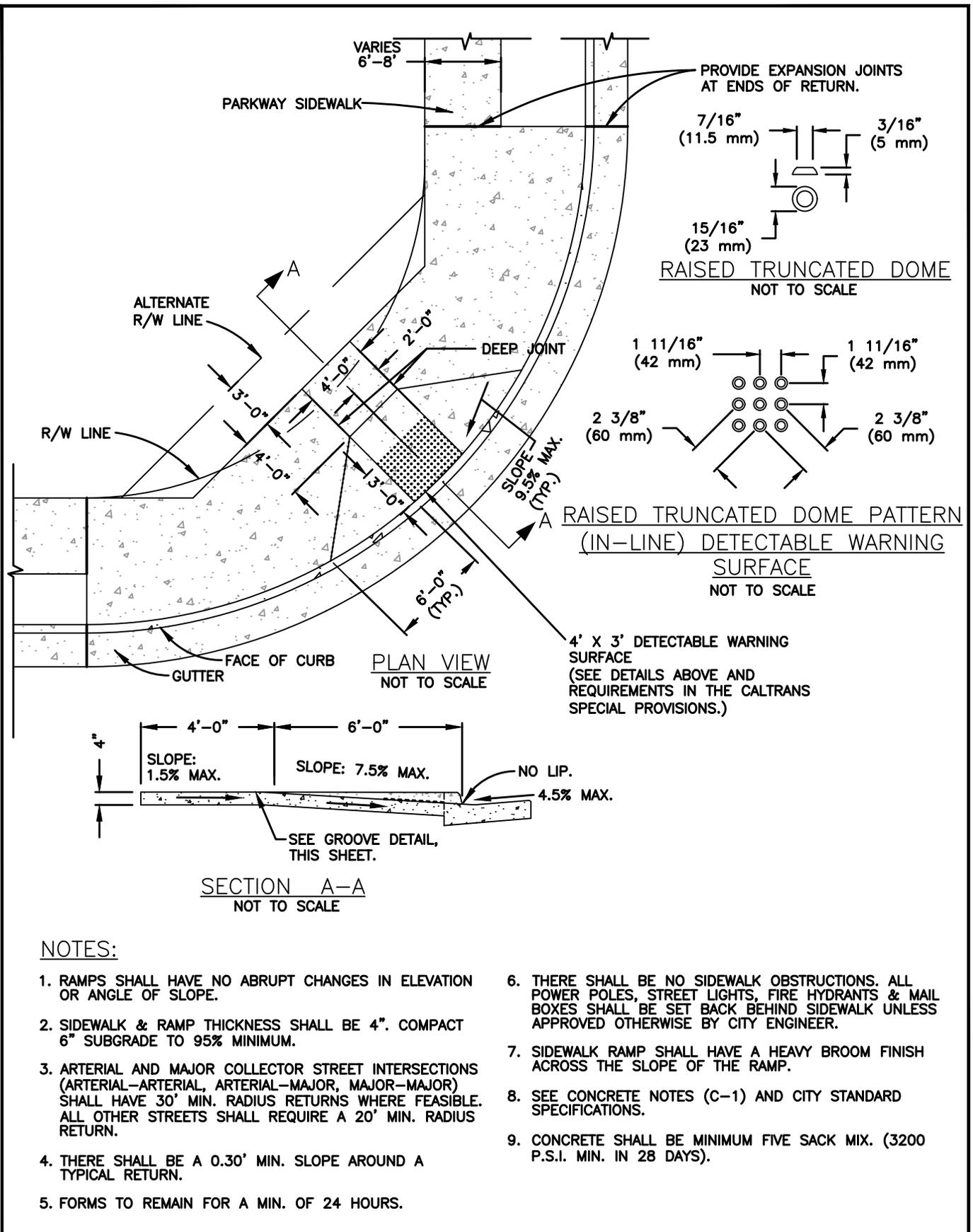
Standard Drawing for:

**WHEELCHAIR RAMP DETAIL**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

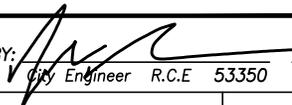
**C-6**



CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**WHEELCHAIR RAMP DETAIL  
(30 FOOT RADIUS RETURN)**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**C-6A**



## DRIVE WAY STANDARDS AND CRITERIA

### 1. DRIVEWAYS – GENERAL

ALL DRIVEWAY APPROACHES, HEREINAFTER CALLED DRIVEWAYS, IN CITY RIGHT-OF-WAY SHALL BE CONSTRUCTED IN CONFORMANCE WITH CITY SPECIFICATIONS AND STANDARD DRIVEWAY DRAWINGS OR AS MODIFIED FOR SPECIAL SITUATIONS DESCRIBED HEREIN.

- A. A RESIDENTIAL DRIVEWAY APRON SHALL BE CONSTRUCTED BETWEEN THE CURB AND THE PROPERTY LINE WITH PORTLAND CEMENT CONCRETE PER DRIVEWAY STANDARDS.
- B. A COMMERCIAL DRIVEWAY APRON TO A PARKING LOT OR "DRIVE-IN" BUSINESS SHALL BE CONSTRUCTED BETWEEN THE CURB AND THE PROPERTY LINE WITH AN APPROVED PORTLAND CEMENT CONCRETE STRUCTURAL SECTION.
- C. AN INDUSTRIAL DRIVEWAY APRON SHALL BE CONSTRUCTED BETWEEN THE CURB AND THE PROPERTY LINE WITH AN APPROVED PORTLAND CEMENT CONCRETE STRUCTURAL SECTION BASED ON THE AMOUNT OF TRUCK TRAFFIC (T) AND THE ABILITY OF THE SOIL (R-VALUE) TO WITHSTAND TRUCK WHEEL LOADS.
- D. IN ALL CASES ABOVE, IT SHALL BE THE RESPONSIBILITY OF THE ABUTTING PROPERTY OWNER TO MAINTAIN THE DRIVEWAY APRON IN A SAFE AND SUITABLE CONDITION FOR THE TRAFFIC TO BE CARRIED, WHETHER PEDESTRIAN OR VEHICULAR.

### 2. COMMERCIAL – INDUSTRIAL HIGH VOLUME DRIVEWAYS

COMMERCIAL AND INDUSTRIAL DRIVEWAYS THAT SERVE A SUBSTANTIAL NUMBER OF VEHICLES OR TRUCKS SHALL HAVE DIMENSIONS, SIGHT DISTANCE, GEOMETRICS, SPACING, ETC., DETERMINED BY THE CITY ENGINEER.

### 3. ONE-WAY DRIVEWAYS

ONE WAY ENTRANCE OR EXIT DRIVEWAYS SHALL CONFORM TO THE CITY STANDARD FOR COMMERCIAL DRIVEWAYS OR AS MODIFIED BY THE CITY ENGINEER FOR SPECIAL SITUATIONS.

### 4. AMOUNT OF FRONTAGE ALLOWED FOR DRIVEWAYS

NOT MORE THAN 40 PERCENT OF RESIDENTIAL OR 40 PERCENT OF COMMERCIAL FRONTAGE OF ANY PARCEL MAY BE DEVOTED TO DRIVEWAYS.

### 5. DRIVEWAY WIDTH "W"

THE WIDTH OF DRIVEWAYS SHALL BE MEASURED BETWEEN THE BOTTOM POINTS OF THE DRIVEWAY.

### 6. MINIMUM WIDTH "W"

- A. THE MINIMUM WIDTH OF DRIVEWAYS FOR ONE AND TWO FAMILY RESIDENCES SHALL BE 12 FEET.
- B. THE MINIMUM WIDTH OF ALL OTHER DRIVEWAYS SHALL PROVIDE FOR THE SAFE, EFFICIENT AND ECONOMICAL MOVEMENT OF TRAFFIC AND SHOULD BE APPROXIMATELY 24 FEET.

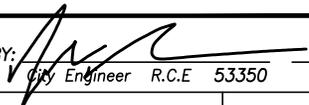
### 7. MAXIMUM WIDTH "W"

- A. THE MAXIMUM WIDTH FOR A RESIDENTIAL DRIVEWAY SHALL BE 20 FEET, UNLESS APPROVED OTHERWISE BY THE CITY ENGINEER.
- B. THE MAXIMUM WIDTH OF ALL COMMERCIAL DRIVEWAYS SHALL BE 35 FEET EXCEPT THIS MAY BE INCREASED BY THE CITY ENGINEER.
- C. IN THE CASE OF A DRIVEWAY LOCATED ADJACENT TO AN ALLEY, IF APPROVED BY THE CITY ENGINEER, THE DRIVEWAY APRON MAY BE COMBINED WITH THE ALLEY BUT THE TOTAL COMBINED WIDTH SHALL NOT EXCEED 40 FEET.
- D. THE DRIVEWAY WIDTH MAY BE MODIFIED BY THE CITY ENGINEER TO FACILITATE TURNING MOVEMENT WHERE CURB LANES ARE USED.

## CITY of CORCORAN • Department of Public Works

Standard Drawing for:

### DRIVEWAY STANDARDS AND CRITERIA

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

C-8

DRIVE WAY STANDARDS AND CRITERIA (CONT.)

**8. DISTANCE BETWEEN DRIVEWAYS**

- A. NO DRIVEWAY SHALL BE LOCATED CLOSER THAN FIVE FEET (AT TOP OF APRON) FROM A SIDE PROPERTY LINE.
- B. THE MINIMUM LENGTH OF FULL HEIGHT CURB BETWEEN DRIVEWAYS ON ADJACENT LOTS SHALL BE TEN FEET EXCEPT AS ALLOWED BY SPECIFIC ZONING ORDINANCE.
- C. NO DRIVEWAY SHALL BE LOCATED CLOSER THAN SIX FEET FROM AN EXISTING OR FUTURE ALLEY ENTRANCE EXCEPT AS PROVIDED ELSEWHERE IN THESE STANDARDS.
- D. WHERE TWO OR MORE DRIVEWAYS ARE CONSTRUCTED ON THE SAME LOT, THE MINIMUM LENGTH OF FULL HEIGHT CURB BETWEEN DRIVEWAYS SHALL BE 26 FEET. WHERE PRACTICAL TO PROVIDE PARKING, THE TOTAL LENGTH OF FULL HEIGHT CURB BETWEEN DRIVEWAYS SHALL BE IN MULTIPLES OF 24 FEET.

**9. DRIVEWAY GRADE (SLOPE)**

THE MINIMUM GRADE FOR DRIVEWAYS SHALL BE 2 PERCENT.

**10. DRIVEWAY DISTANCE FROM UTILITY OR SAFETY DEVICES**

NO DRIVEWAY SHALL BE LOCATED CLOSER THAN FIVE FEET FROM A FIRE HYDRANT, TRAFFIC SIGNAL, STREET LIGHT STANDARDS, UTILITY POLE OR GUY WIRE.

**11. UTILITY RELOCATION**

RELOCATION OF UTILITY COMPANY'S FACILITIES OR OTHER PUBLIC IMPROVEMENTS IN ORDER TO ACCOMODATE A DRIVEWAY SHALL BE ACCOMPLISHED WITHOUT COST TO THE CITY.

**12. SIGNAL AND ELECTRICAL CONDUIT**

WHERE TRAFFIC SIGNAL OR HIGHWAY LIGHTING IS PLANNED OR ANTICIPATED, A MINIMUM OF ONE 2-INCH PVC-P & C TC-6 CONDUIT SHALL BE PLACED UNDER ANY NEW DRIVEWAY APRON AND EXTEND A MINIMUM OF ONE FOOT BEYOND THE ENDS OF THE DRIVEWAY. THE CONDUIT SHALL BE PLACED BEHIND AND 24" BELOW THE TOP OF THE CURB.

**13. REMOVAL OF EXISTING DRIVEWAYS**

WHEN DRIVEWAY CONSTRUCTION IS TO TAKE PLACE ON A PARCEL, ANY ABANDONED DRIVEWAYS SHALL BE REMOVED AND REPLACED WITH STANDARD CURB, GUTTER AND SIDEWALKS CONCURRENTLY WITH THE NEW CONSTRUCTION AND WITHOUT COST TO THE CITY.

**14. MODIFICATION**

THE ABOVE STANDARDS MAY BE MODIFIED BY THE CITY ENGINEER FOR HARDSHIP CONDITIONS OR WHERE NECESSARY TO PROVIDE FOR THE SAFE AND EFFECIENT MOVEMENT OF TRAFFIC OR TO ACHIEVE APPROPRIATE DRAINAGE.

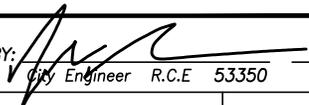
**15. SPECIFICATIONS**

ALL WORK TO BE DONE AND ALL MATERIALS SUPPLIED SHALL CONFORM TO "STANDARDS SPECIFICATIONS" LATEST EDITION AND CITY OF CORCORAN STANDARD CONSTRUCTION PLANS.

**CITY of CORCORAN • Department of Public Works**

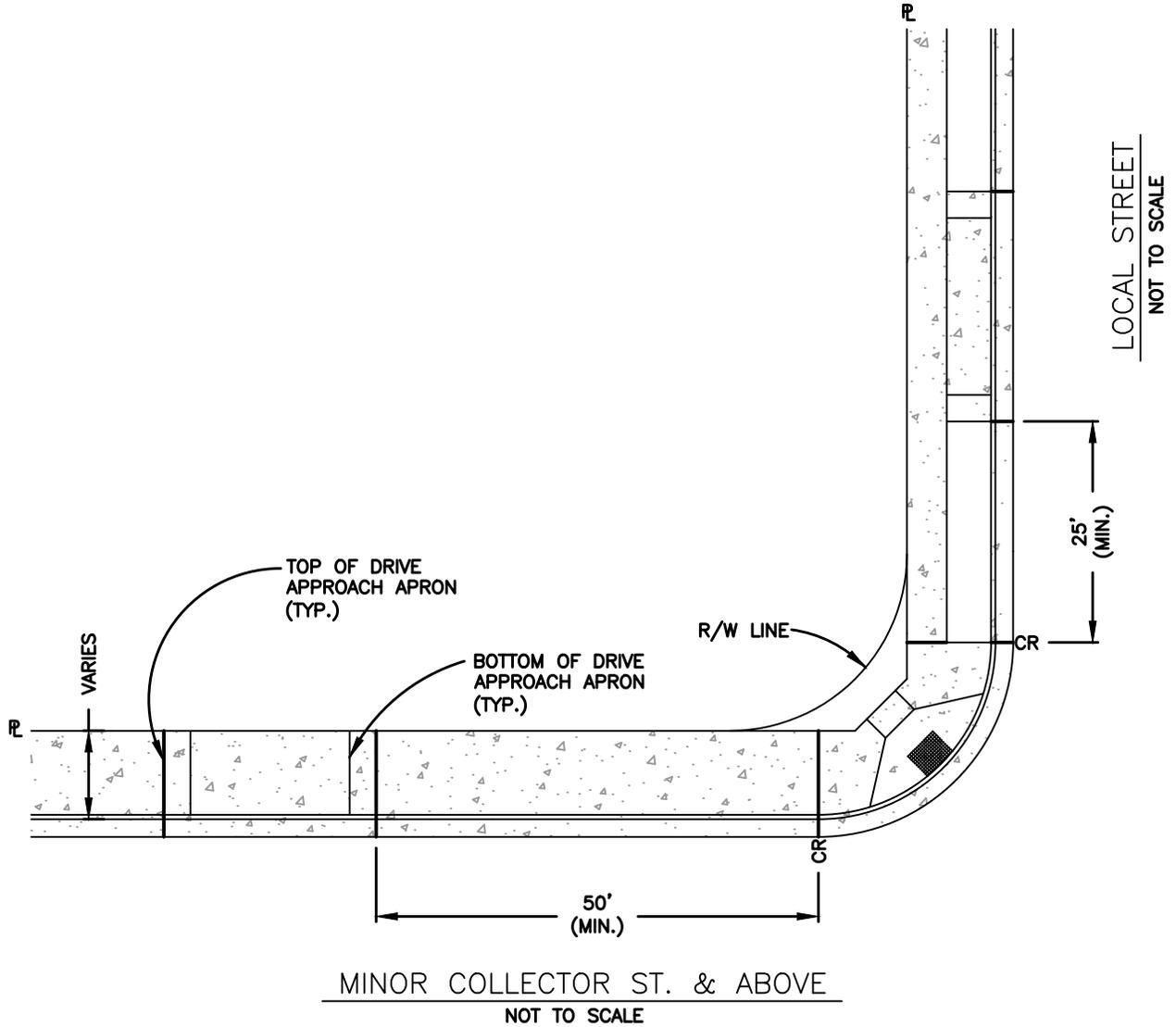
Standard Drawing for:

**DRIVEWAY STANDARDS  
AND CRITERIA**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**C-8A**



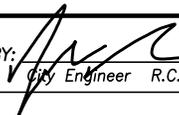
NOTES:

1. NO PORTION OF ANY DRIVEWAY SHALL BE PERMITTED WITHIN SHOWN DISTANCES FROM THE END OF CURB RETURN.
2. ON ALL INTERSECTIONS WHERE CHANNELIZATION AND/OR COMPOUND CURVES ARE TO EXIST, THE DRIVEWAY LOCATION SHALL BE SUBJECT TO APPROVAL OF THE CITY ENGINEER.

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

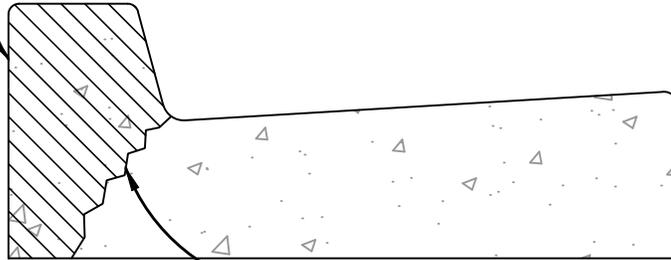
**DRIVE LOCATIONS**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**C-9**

PORTION OF CURB  
TO BE REMOVED



CUT LINE  
(SEE NOTE BELOW)

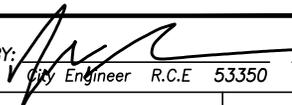
NOTE:

WHEN REMOVING CURB FOR NEW DRIVEWAY APPROACHES, CUT EXISTING CURB AS SHOWN ABOVE AT TOP OF APPROACH FOR NEW DRIVEWAY.

CITY of CORCORAN • Department of Public Works

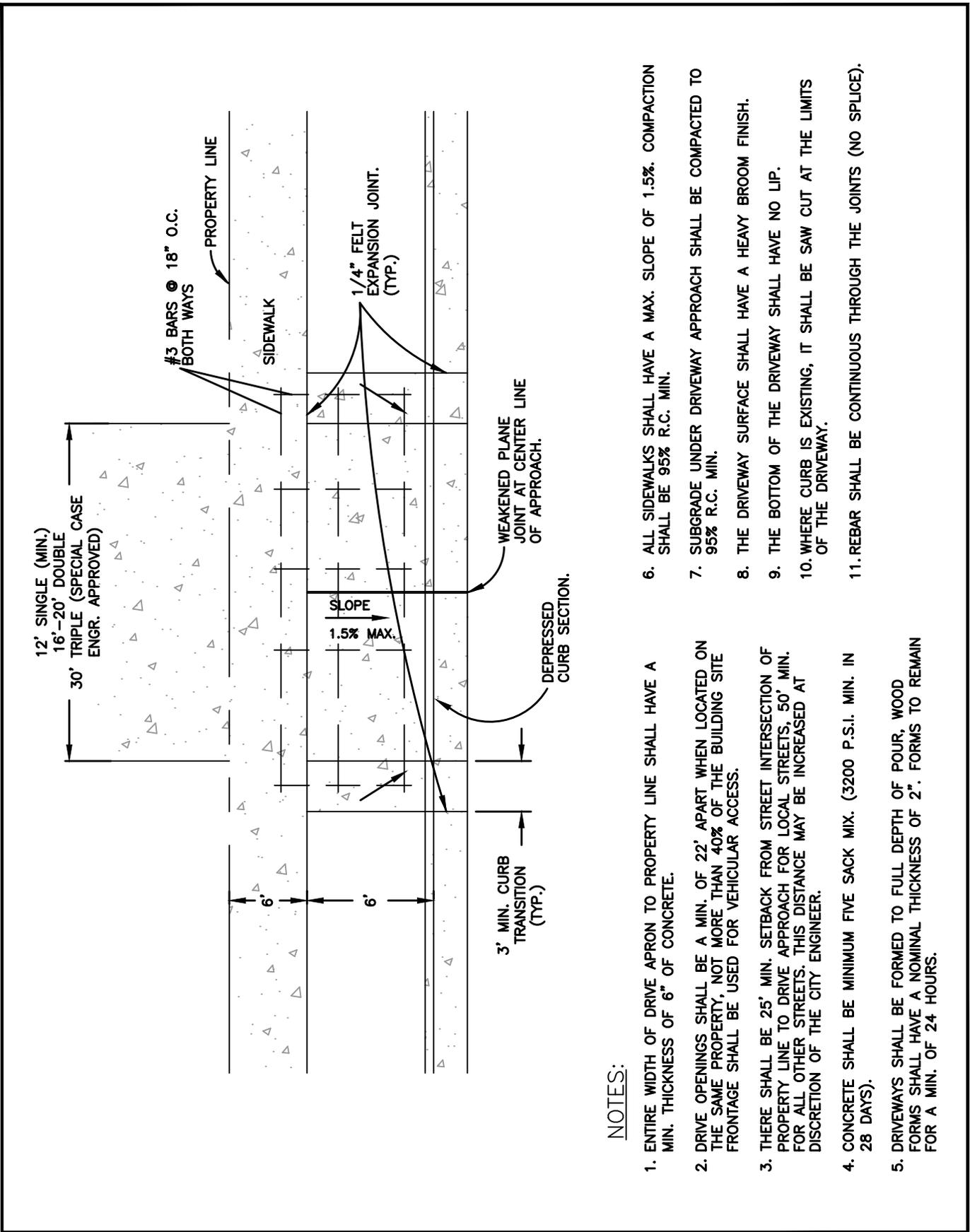
Standard Drawing for:

**CURB REMOVAL AT  
NEW DRIVE**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**C-10**



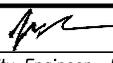
**NOTES:**

1. ENTIRE WIDTH OF DRIVE APRON TO PROPERTY LINE SHALL HAVE A MIN. THICKNESS OF 6" OF CONCRETE.
2. DRIVE OPENINGS SHALL BE A MIN. OF 22' APART WHEN LOCATED ON THE SAME PROPERTY, NOT MORE THAN 40% OF THE BUILDING SITE FRONTAGE SHALL BE USED FOR VEHICULAR ACCESS.
3. THERE SHALL BE 25' MIN. SETBACK FROM STREET INTERSECTION OF PROPERTY LINE TO DRIVE APPROACH FOR LOCAL STREETS, 50' MIN. FOR ALL OTHER STREETS. THIS DISTANCE MAY BE INCREASED AT DISCRETION OF THE CITY ENGINEER.
4. CONCRETE SHALL BE MINIMUM FIVE SACK MIX. (3200 P.S.I. MIN. IN 28 DAYS).
5. DRIVEWAYS SHALL BE FORMED TO FULL DEPTH OF POUR, WOOD FORMS SHALL HAVE A NOMINAL THICKNESS OF 2". FORMS TO REMAIN FOR A MIN. OF 24 HOURS.
6. ALL SIDEWALKS SHALL HAVE A MAX. SLOPE OF 1.5%. COMPACTION SHALL BE 95% R.C. MIN.
7. SUBGRADE UNDER DRIVEWAY APPROACH SHALL BE COMPACTED TO 95% R.C. MIN.
8. THE DRIVEWAY SURFACE SHALL HAVE A HEAVY BROOM FINISH.
9. THE BOTTOM OF THE DRIVEWAY SHALL HAVE NO LIP.
10. WHERE CURB IS EXISTING, IT SHALL BE SAW CUT AT THE LIMITS OF THE DRIVEWAY.
11. REBAR SHALL BE CONTINUOUS THROUGH THE JOINTS (NO SPLICE).

CITY of CORCORAN • Department of Public Works

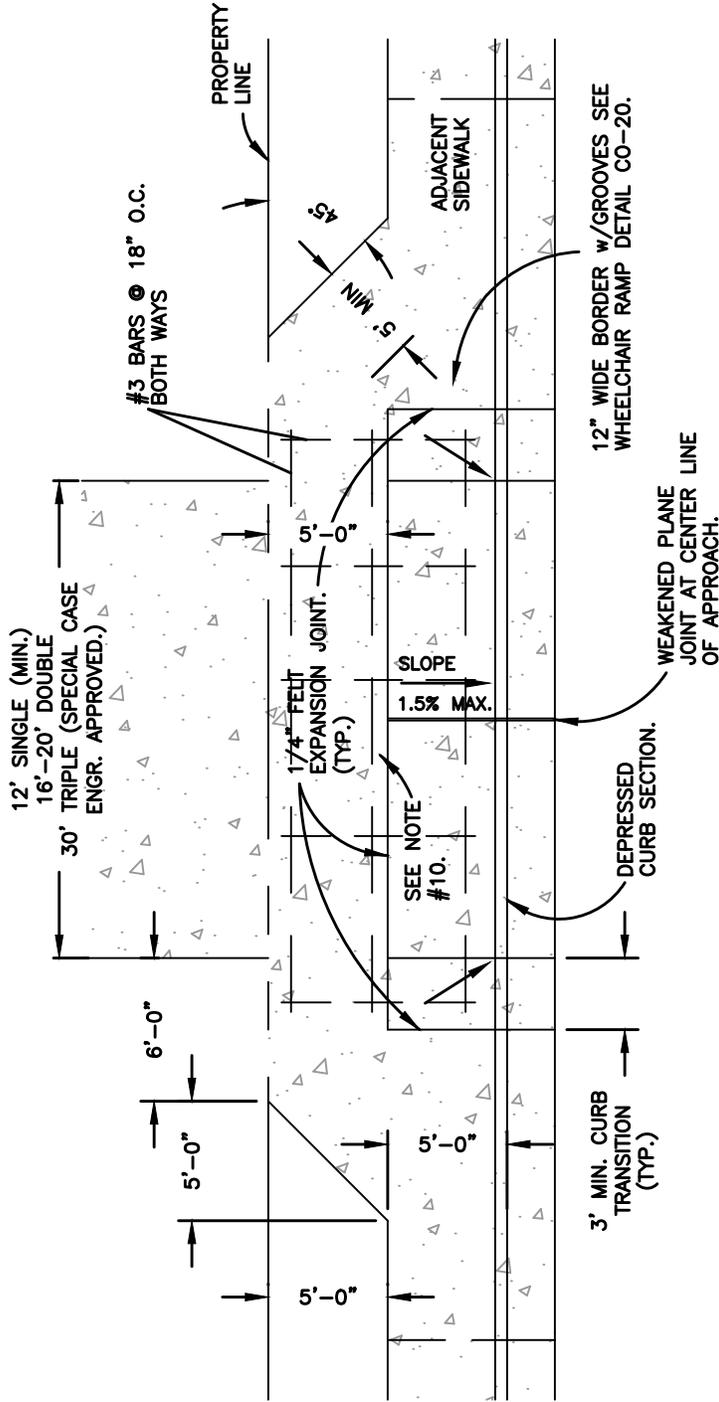
Standard Drawing for:

**RESIDENTIAL DRIVEWAY WITH PARKSTRIP SIDEWALK**

APPROVED BY:  10/30/17  
 City Engineer R.C.E 53350 Date

Revised:

**C-11**



NOTES:

1. ENTIRE WIDTH OF DRIVE APRON TO PROPERTY LINE SHALL HAVE A MIN. THICKNESS OF 6" OF CONCRETE.
2. DRIVE OPENINGS SHALL BE A MIN. OF 26' APART WHEN LOCATED ON THE SAME PROPERTY, NOT MORE THAN 40% OF THE BUILDING SITE FRONTAGE SHALL BE USED FOR VEHICULAR ACCESS.
3. THERE SHALL BE 25' MIN. SETBACK FROM STREET INTERSECTION OF PROPERTY LINE TO DRIVE APPROACH FOR LOCAL STREETS, 50' MIN. FOR ALL OTHER STREETS. THIS DISTANCE MAY BE INCREASED AT DISCRETION OF THE CITY ENGINEER.
4. CONCRETE SHALL BE MINIMUM FIVE SACK MIX. (3200 P.S.I. MIN. IN 28 DAYS).
5. DRIVEWAYS SHALL BE FORMED TO FULL DEPTH OF POUR, WOOD FORMS SHALL HAVE A NOMINAL THICKNESS OF 2". FORMS TO REMAIN FOR A MIN. OF 24 HOURS.
6. ALL SIDEWALKS SHALL HAVE A MAX. SLOPE OF 1.5%. COMPACTION SHALL BE 95% R.C. MIN.
7. SUBGRADE UNDER DRIVEWAY APPROACH SHALL BE COMPACTED TO 95% R.C. MIN.
8. THE DRIVEWAY SURFACE SHALL HAVE A HEAVY BROOM FINISH.
9. THE BOTTOM OF THE DRIVEWAY SHALL HAVE NO LIP.
10. WHERE CURB IS EXISTING, IT SHALL BE SAW CUT AT THE LIMITS OF THE DRIVEWAY.
11. REBAR SHALL BE CONTINUOUS THROUGH THE JOINTS (NO SPLICE).

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**RESIDENTIAL DRIVEWAY WITH  
ADJACENT SIDEWALK**

APPROVED BY:

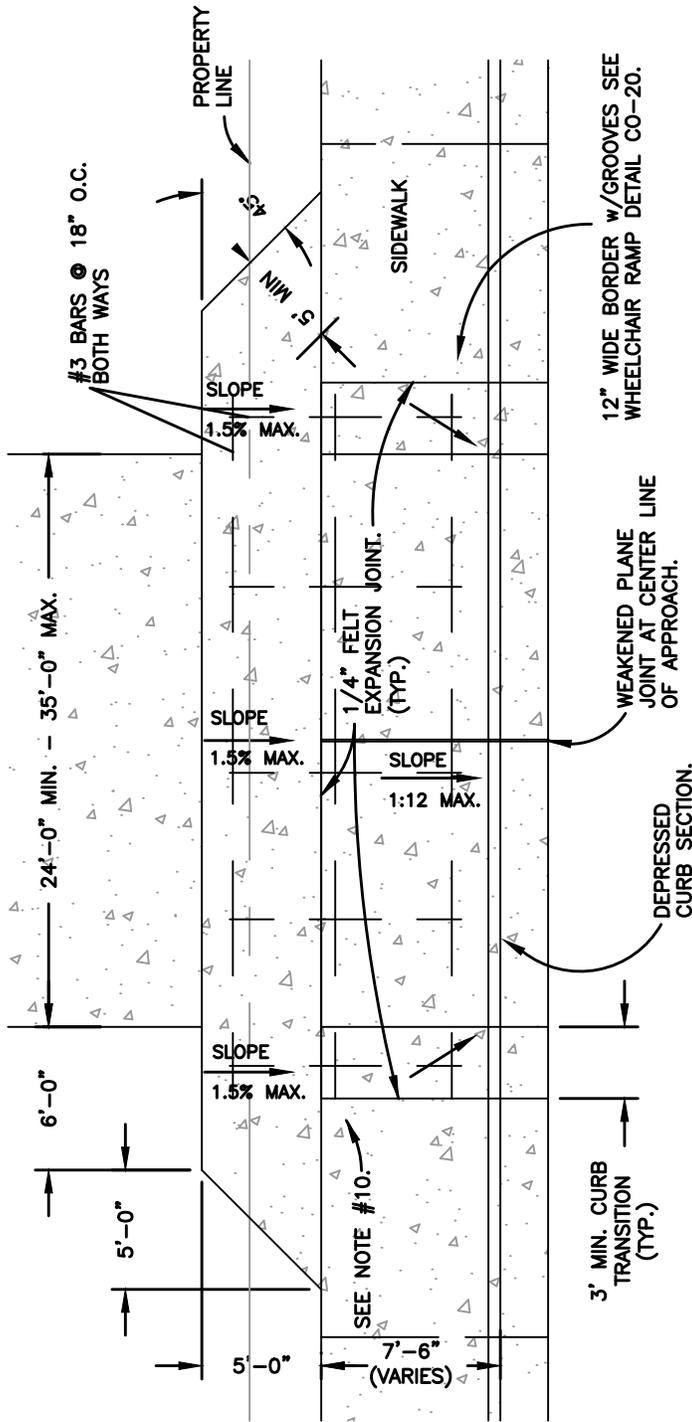
City Engineer R.C.E 53350

10/30/17

Date

Revised:

**C-11A**



**NOTES:**

1. ENTIRE WIDTH OF DRIVE APRON TO PROPERTY LINE SHALL HAVE A MIN. THICKNESS OF 6" OF CONCRETE.
2. DRIVE OPENINGS SHALL BE A MIN. OF 22' APART WHEN LOCATED ON THE SAME PROPERTY, NOT MORE THAN 40% OF THE BUILDING SITE FRONTAGE SHALL BE USED FOR VEHICULAR ACCESS.
3. THERE SHALL BE 25' MIN. SETBACK FROM STREET INTERSECTION OF PROPERTY LINE TO DRIVE APPROACH FOR LOCAL STREETS 50' MIN. FOR ALL OTHER STREETS PER CO-35. THIS DISTANCE MAY BE INCREASED AT DISCRETION OF THE CITY ENGINEER.
4. CONCRETE SHALL BE MINIMUM FIVE SACK MIX. (3200 P.S.I. MIN. IN 28 DAYS).
5. DRIVEWAYS SHALL BE FORMED TO FULL DEPTH OF POUR, WOOD FORMS SHALL HAVE A NOMINAL THICKNESS OF 2". FORMS TO REMAIN FOR A MIN. OF 24 HOURS.
6. ALL SIDEWALKS SHALL HAVE A MAX. SLOPE OF 1.5%. COMPACTION SHALL BE 95% R.C. MIN.
7. SUBGRADE UNDER DRIVEWAY APPROACH SHALL BE COMPACTED TO 95% R.C. MIN.
8. THE DRIVEWAY SURFACE SHALL HAVE A HEAVY BROOM FINISH.
9. THE BOTTOM OF THE DRIVEWAY SHALL HAVE NO LIP.
10. WHERE CURB IS EXISTING, IT SHALL BE SAW CUT AT THE LIMITS OF THE DRIVE APPROACH.
11. APPROACHES CONTINUALLY USED BY HEAVY VEHICLES WILL REQUIRE ADDITIONAL REINFORCING STEEL FOR ADDITIONAL STRENGTH.
12. REBAR SHALL BE CONTINUOUS THROUGH THE JOINTS (NO SPLICES).

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**COMMERCIAL AND INDUSTRIAL  
DRIVE APPROACH PLACEMENT**

APPROVED BY:

City Engineer R.C.E 53350

10/30/17

Date

Revised:

**C-12**

GENERAL CRITERIA

STORM DRAIN IMPROVEMENTS SUBMITTED FOR REVIEW AND APPROVAL SHALL BE IN CONFORMANCE WITH THE FOLLOWING STANDARDS:

1. ALL SUBMITTALS SHALL BE IN DUPLICATE.
2. TOPOGRAPHIC MAPS SHALL HAVE ELEVATIONS ADEQUATE TO DEFINE BOUNDARIES AND SLOPE OF DRAINAGE BASIN.
3. EACH DRAINAGE BASIN TO BE IDENTIFIED AND CORRELATED TO CALCULATIONS FOR THAT BASIN.
4. ALL DATA AND CALCULATIONS SHALL BE COMPLETE AND SHALL HAVE REASONABLE CLARITY.
5. ENGINEERING CALCULATIONS SHALL BE SUBMITTED VERIFYING THAT APPROPRIATE MEASURES HAVE BEEN ADDRESSED, INSURING THAT EXIT VELOCITIES ARE NON-ERODING.
6. THE ATTACHED STORM DRAIN DESIGN CRITERIA AND CHARTS SHALL BE USED WITH THE RATIONAL FORMULA FOR CALCULATING HYDROLOGIC AND PIPE AND/OR CHANNEL DESIGN CHARACTERISTICS, I.E., SIZE, TYPE, SLOPE, VELOCITIES AND ENTRANCE AND OUTLET STRUCTURES.
7. ALL EXISTING STREETS TO BE ASSUMED CONSTRUCTED TO ULTIMATE STANDARDS.
8. MINIMUM SIZE OF PROPOSED STORM DRAINAGE CULVERTS SHALL BE 12 INCHES IN DIAMETER.

HYDROLOGIC CRITERIA FOR PIPES AND CULVERTS

1. LEVEL OF DEVELOPMENT AS SHOWN IN CITY OF CORCORAN GENERAL PLAN.
2. RECURRENCE INTERVAL (STORM FREQUENCY)
  - A. FREQUENCY OF 2 YEARS FOR AREAS LESS THAT 160 ACRES.
  - B. 5 YEAR FREQUENCY FOR AREAS LARGER THAN 160 ACRES.

3. DESIGN CONSIDERATION

ON RESIDENTIAL STREETS, FACILITIES SHOULD BE DESIGNED SO THAT A 2-YEAR INTERVAL STORM DOES NOT POND LONGER THAN 1 HOUR. PROVISIONS SHALL ALSO BE MADE SO THAT WATER DEPTH DOES NOT EXCEED THE TOP OF CURB BY MORE THAN 1 (ONE) FOOT FOR A 100-YEAR RETURN INTERVAL STORM.

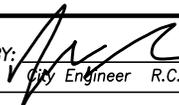
FOR COMMERCIAL ZONES, FACILITIES SHOULD BE DESIGNED SO THAT A 2-YEAR RETURN INTERVAL STORM DOES NOT POND LONGER THAN 1 HOUR AND THAT A 100-YEAR STORM DOES NOT CAUSE PROPERTY DAMAGE TO BUILDINGS.

4. MINIMUM PIPE VELOCITY  
V=2.5 FPS.

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**STORM DRAINAGE  
CRITERIA**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SD-1**

DESIGN CRITERIA

STREET SYSTEMS

THE FOLLOWING FORMULA IS USED TO DETERMINE FLOW CHARACTERISTICS IN TYPICAL STREET SECTIONS. FLOWS ARE BASED UPON THE MANNING FORMULA:

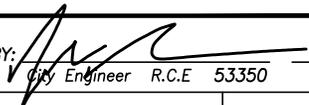
$$Q = \frac{1.49}{N} AR^{2/3} S^{1/2}$$

1. CURB AND GUTTER SECTIONS ARE FOR THE CITY OF CORCORAN STANDARD STREET SECTIONS MODIFIED SLIGHTLY FOR SIMPLICITY. STREET CROSS SLOPES = 2 PERCENT. GUTTER SECTION IS DEPRESSED 1/4", MAX., BELOW THE ADJOINING STREET SURFACE.
2. MANNING ROUGHNESS COEFFICIENT N=0.015
3. VALUES SHALL BE CALCULATED FOR ONE-HALF STREET ONLY. BE CAREFUL WHEN CALCULATING SO THAT THE CURB HEIGHT OF THE STREET IS NOT EXCEEDED.
4. DEPTH IS MEASURED AT FACE OF CURB.
5. WIDTH IS MEASURED FROM FACE OF CURB TO EDGE OF FLOODING.

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Standard Drawing for:

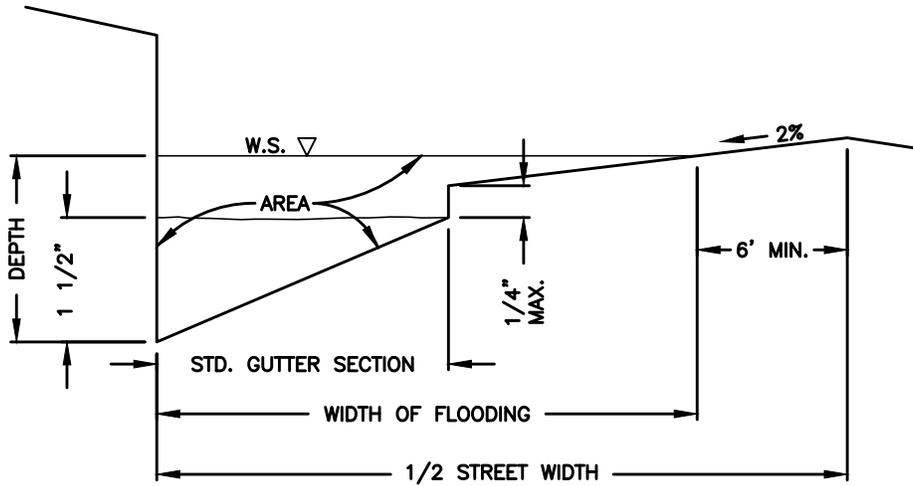
**STORM DRAINAGE  
CRITERIA**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SD-1A**

6. TYPICAL SECTION:



COLLECTION SYSTEMS:

USE RATIONAL EQUATION

$$Q = CIA$$

TO DETERMINE RUN-OFF FOR PIPELINES, DRAINAGE INLETS AND PUMP STATIONS.

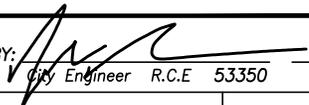
- Q = FLOW IN (C.F.S.)
- A = AREA OF AREA UNDER CONSIDERATION IN ACRES
- I = INTENSITY FACTOR (IN/HR) (SEE CURVE)
- C = RUN-OFF FACTOR (SEE TABLE)

TIME OF CONCENTRATION SHALL BE DETERMINED BY THE SUM OF THE CURB LENGTH AT 1 FPS PLUS THE STORM DRAIN LENGTH ASSUMING 2.5 FPS AND ADDING 30 MINUTES TO THAT TOTAL.

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**STORM DRAINAGE  
CRITERIA**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SD-1B**

DESIGN CRITERIA

RUN OFF COEFFICIENTS

<u>LAND USE/ZONING</u>	<u>C</u>
COMMERCIAL/CC	.85
COMMERCIAL/CN	.80
COMMERCIAL/OTHER	(1)
INDUSTRIAL/ML, MH, MP	(1)
PROFESSIONAL/P.O.	.75
MULTI FAMILY/RM-1.5, RM-2	.65
MULTI FAMILY/RM-2.5, RM-3	.55
SINGLE FAMILY/R-1-6, R-1-10	.35
SINGLE FAMILY/RA	.25
RESOURCE/OPEN SPACE	.15

(1) BASED UPON PROPOSED DEVELOPMENT

NOTE: OTHER RUNOFF COEFFICIENTS MAY BE APPROVED/REQUIRED BY CITY ENGINEER WHEN PROBABLE RUNOFFS ARE OBVIOUSLY INCONSISTENT WITH THE ABOVE CHART.

REQUIREMENTS FOR STORM DRAINAGE BASINS

DESIGN FOR 10 YR/10 DAY EVENT OF 4" OF RAIN. IF THE BASIN HAS AN OUTLET TO BE PUMPED AT OFF-PEAK, AND CAN BE DRAINED WITHIN 48 HOURS, BASIN SHALL BE DESIGNED FOR 2.4 INCHES OF RAIN.

USE:  $S = \frac{P}{12} CA$

S = STORAGE (A-F)

P = PRECIPITATION - 4" OR 2.4

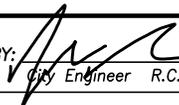
C = RUN-OFF COEFFICIENT

A = AREA (AC)

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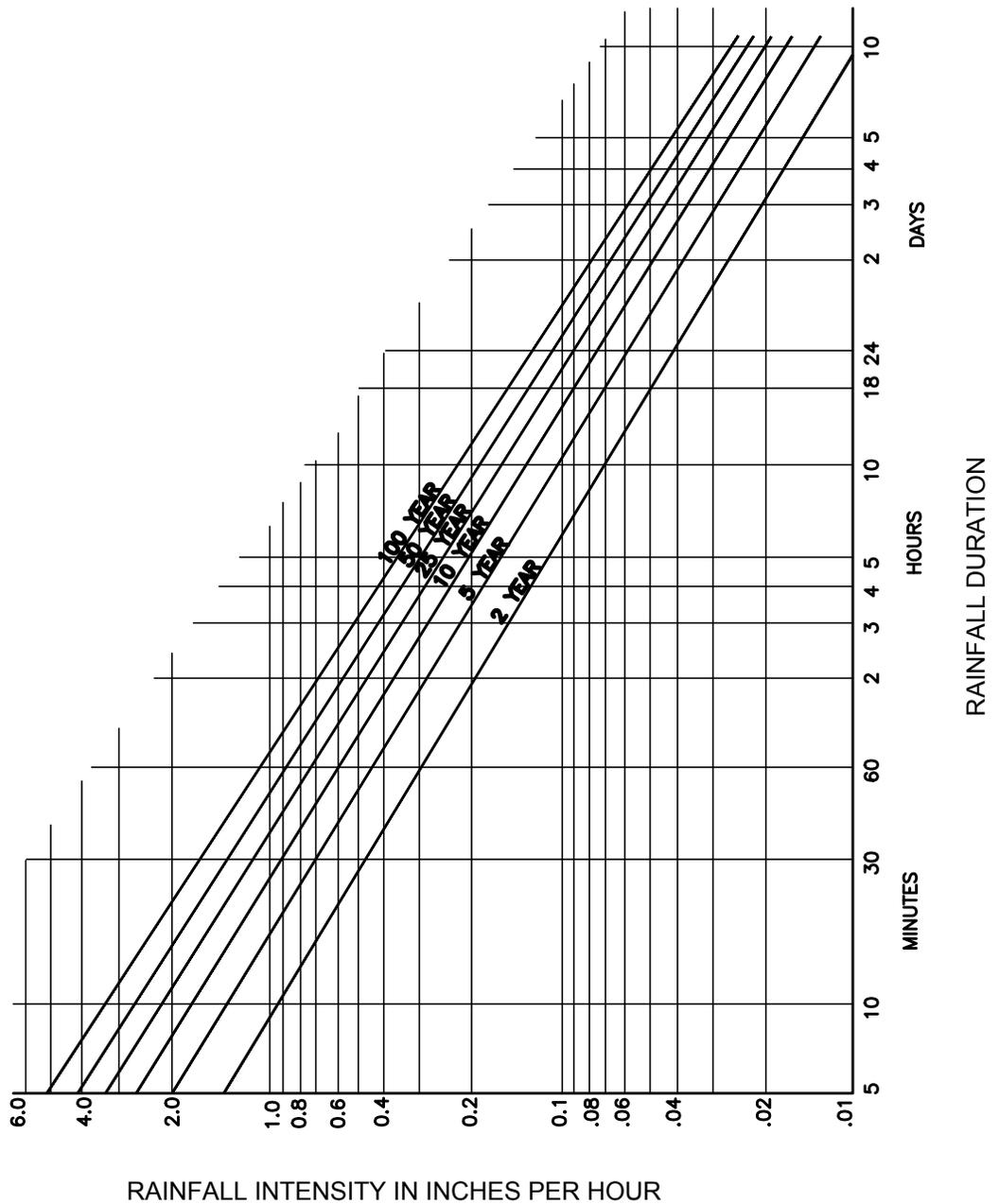
Standard Drawing for:

**STORM DRAINAGE  
CRITERIA**

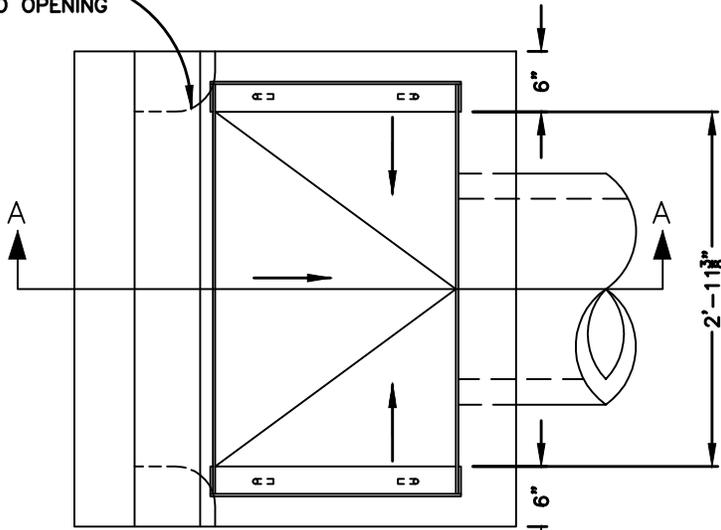
APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

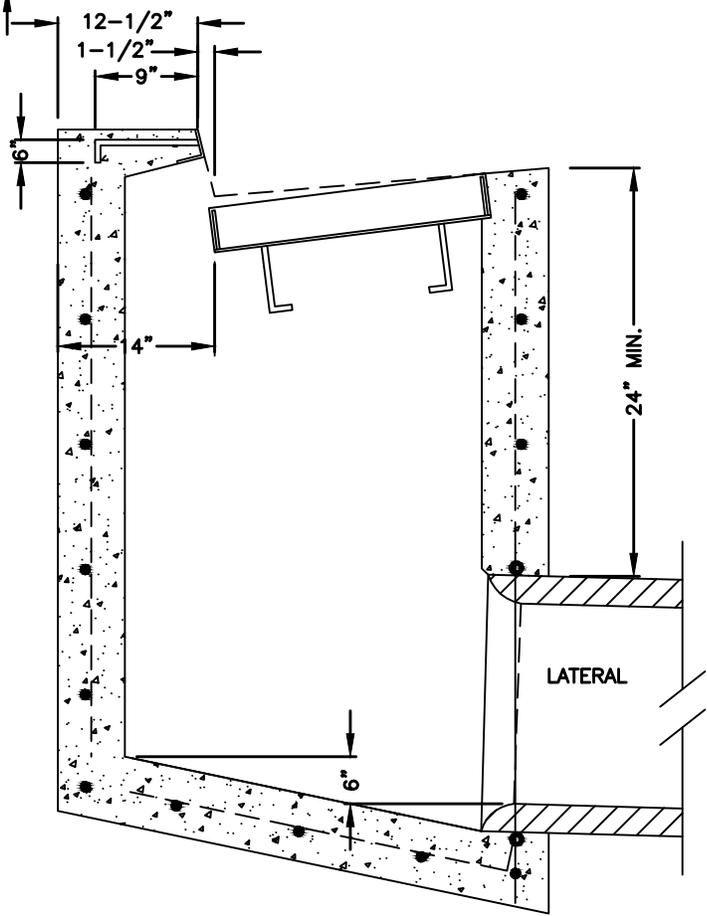
**SD-1C**



4" RADIUS TO OPENING



PLAN  
NOT TO SCALE



SECTION A-A  
NOT TO SCALE

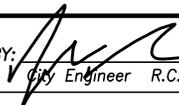
NOTES:

1. ALL CONCRETE SHALL BE FIVE SACK MIX (3200 P.S.I. MIN AT 28 DAYS)
2. MAJOR TYPE DRAINAGE INLETS SHALL BE INSTALLED ON ALL PUBLIC STREETS.
3. 12" MINIMUM STORM DRAIN LATERAL ON LOCAL STREETS.
4. 18" MINIMUM STORM DRAIN LATERAL ON LOCAL STREETS.
5. #4 REBAR AT 18" O.C. BOTH WAYS WHEN DRAIN INLET IS DEEPER THAN 6' BELOW TOP OF CURB.
6. MINIMUM CLEAR SPACING BETWEEN SURFACES OF CONCRETE AND REINFORCING STEEL SHALL BE 2".

CITY of CORCORAN • Department of Public Works

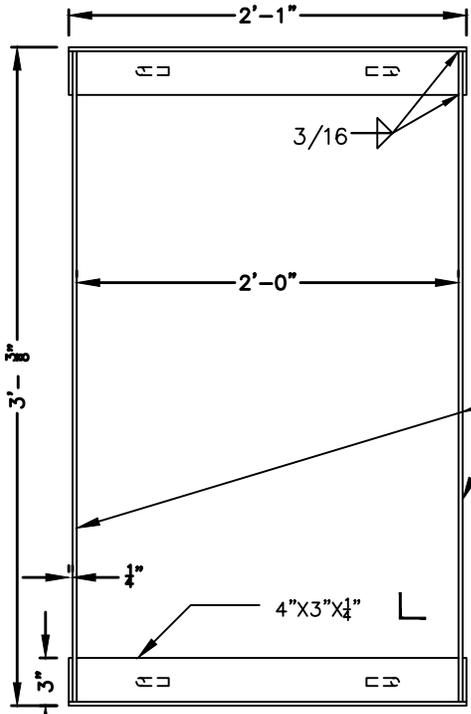
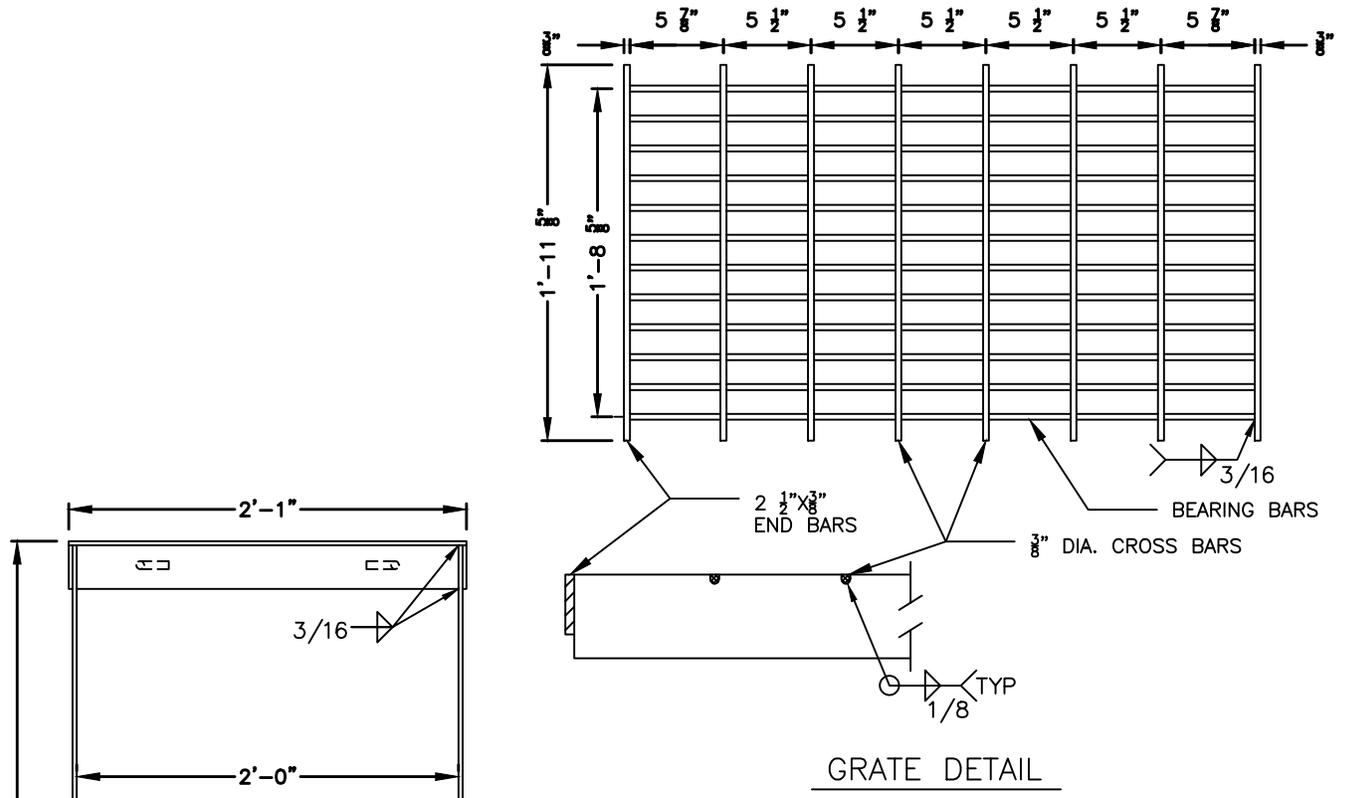
Standard Drawing for:

**STANDARD DRAINAGE  
INLET**

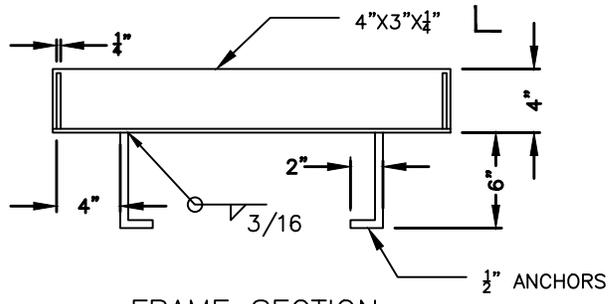
APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SD-3**



FRAME PLAN



FRAME SECTION

3 1/2" x 1/4" x 3'-4 7/8" BAR

NOTES:

1. BEARING BARS SHALL BE 3-1/2" x 3/8" BARS ON 1-7/8" CENTERS.
2. 3/8" DIAMETER CROSS BARS MAY BE FILLET WELDED, RESISTANCE WELDED OR ELECTROFORGED TO BEARING BARS.
3. FRAME AND GRATE SHALL BE HOT DIPPED GALVANIZED AFTER FABRICATION.

CITY of CORCORAN • Department of Public Works

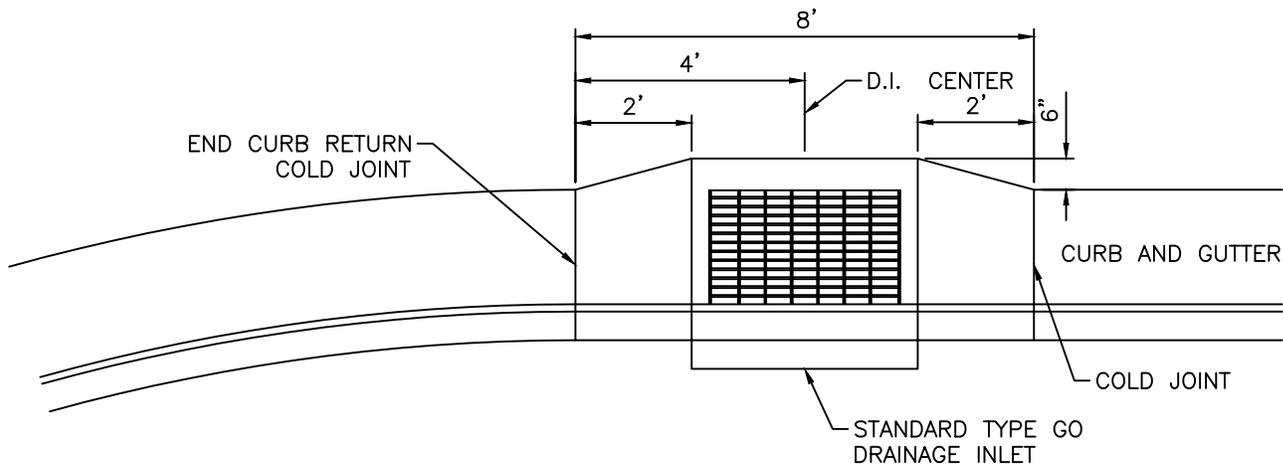
Standard Drawing for:

**STANDARD DRAINAGE  
INLET**

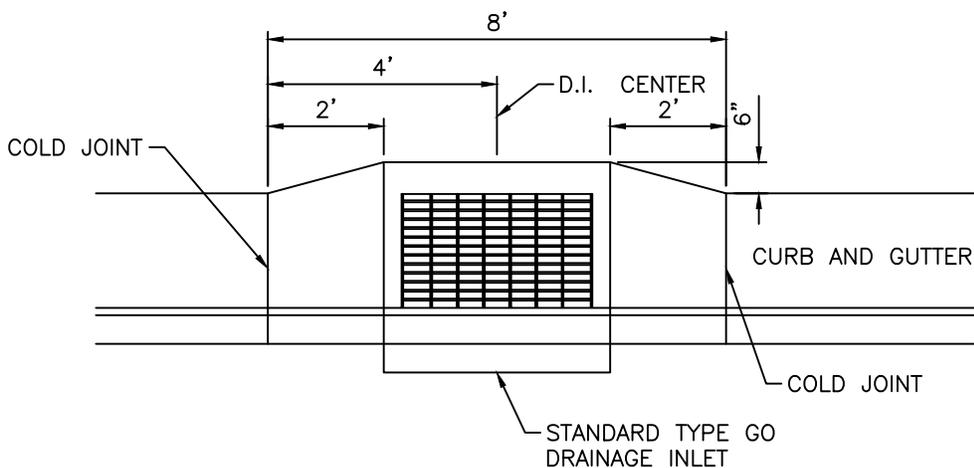
APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SD-3A**



TYPE GO DRAINAGE INLET (D.I.)  
INSTALLED CURB RETURN

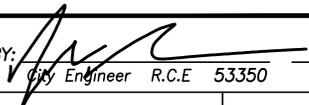


TYPE GO DRAINAGE INLET (D.I.)  
INSTALLED MID-BLOCK

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**MISCELLANEOUS  
DRAINAGE INLET DETAILS**

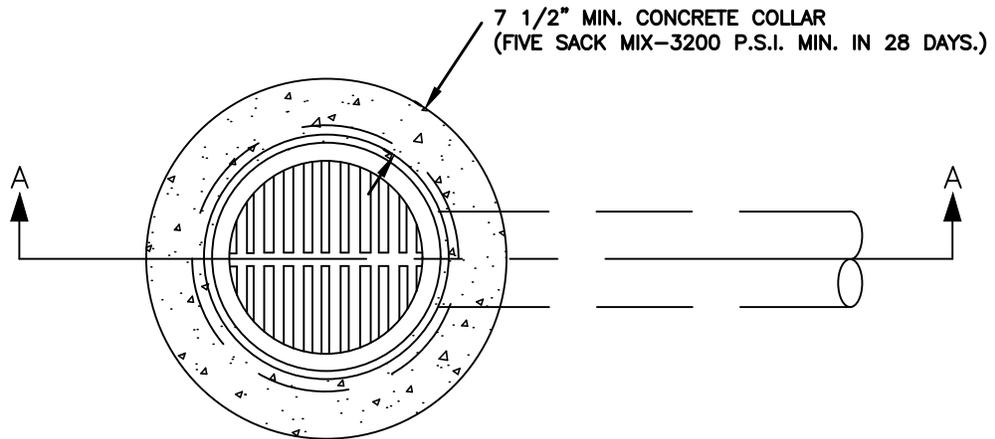
APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SD-3B**

NOTE:

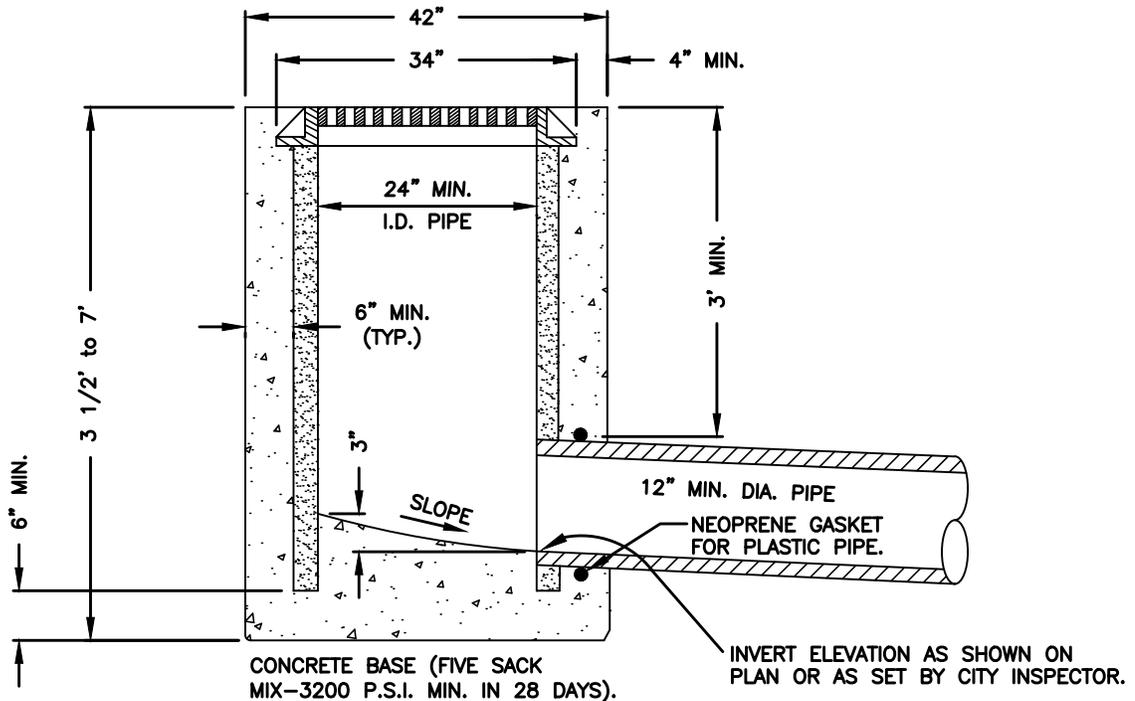
THIS CATCH BASIN MAY BE USED ONLY WITH SPECIFIC APPROVAL OF THE CITY ENGINEER.



C.I. FRAME & GRATE

PLAN  
NOT TO SCALE

CALIFORNIA CONCRETE PIPE, STOCKTON  
No. A-108 GRATE & A-515 FRAME OR  
APPROVED EQUAL.



CONCRETE BASE (FIVE SACK MIX-3200 P.S.I. MIN. IN 28 DAYS).

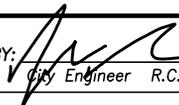
Q MAX. = 2.8 C.F.S.

SECTION A - A  
NOT TO SCALE

CITY of CORCORAN • Department of Public Works

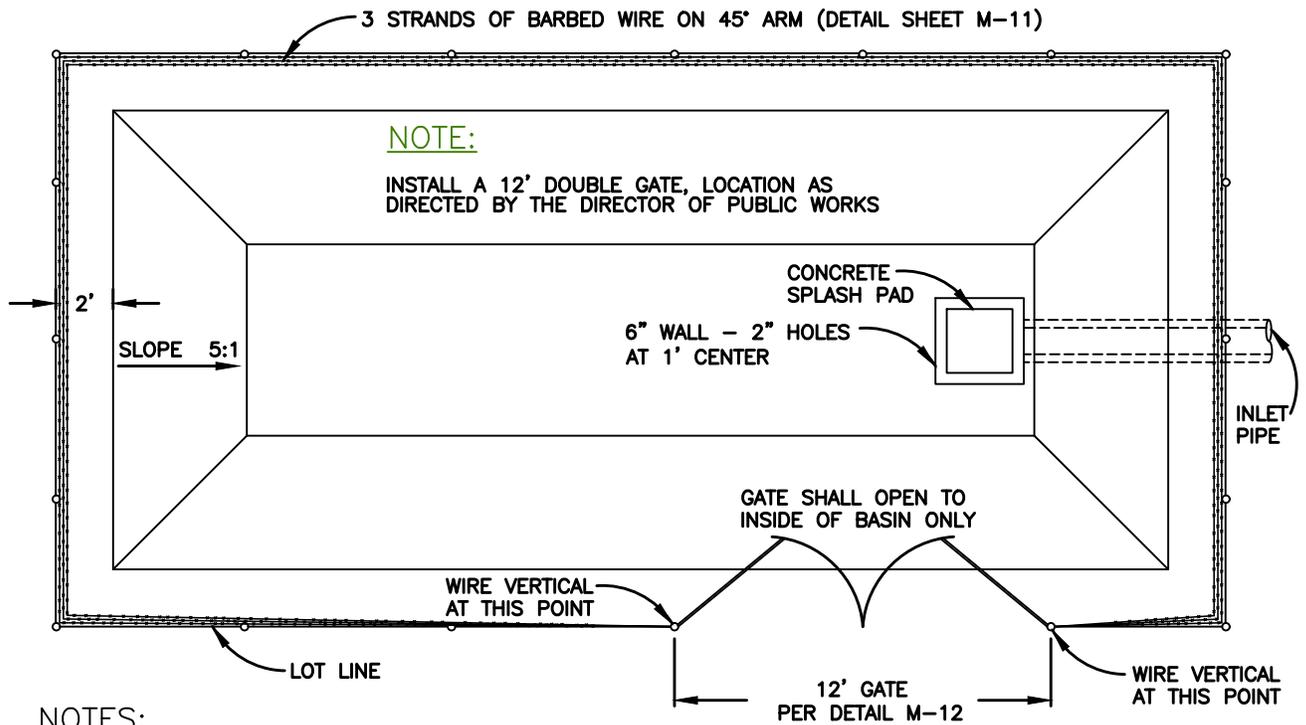
Standard Drawing for:

**STANDARD FLAT GRATE  
DRAINAGE BASIN**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

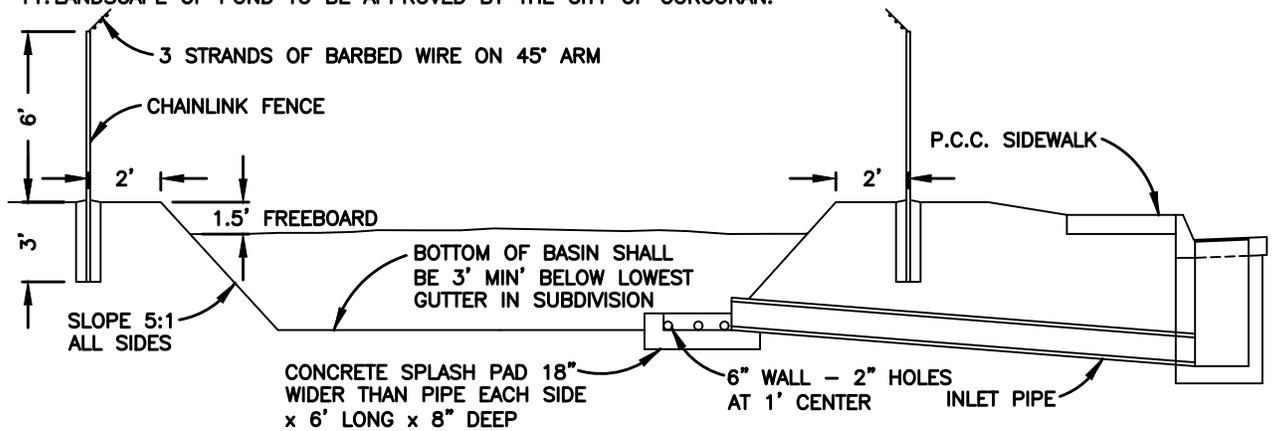
Revised:

**SD-4**



**NOTES:**

1. FENCE TO BE PLACED ON FOOT INSIDE OF LOT LINE.
2. MAXIMUM DEPTH OF WATER IN PONDING BASIN 3'-0".
3. FENCE POST TO BE PLACED IN FIVE SACK MIX (3200 P.S.I. MIN IN 28 DAYS).
4. ACCESS GATE 12'-0" MINIMUM (DOUBLE GATE).
5. ENTIRE AREA OF PONDING LOT TO BE TREATED WITH SOIL STERILANT TO EDGE OF INSIDE OF FENCE OR TO BACK OF CONCRETE CURB OR SIDEWALK.
6. THE SOIL STERILANT TO BE USED AND RATE OF APPLICATION MUST BE APPROVED BY PUBLIC WORKS DIRECTOR BEFORE BEING APPLIED.
7. WHERE PONDING BASIN IS ON CORNER LOT, FENCE SHALL FOLLOW CURVE OF LOT LINE.
8. AT THE CITY OF CORCORAN'S DISCRETION THE DEVELOPER MAY BE REQUIRED TO SUBMIT FOR APPROVAL DETAILS AND SPEC'S FOR EITHER GRAVITY DISCHARGE OR PUMPING OF STORM WATER FROM POND TO STORM SYSTEM. STANDING STORM WATER WILL NOT BE ALLOWED.
9. FENCE POST LOCATIONS AND POST SIZE PER DETAILS M-11 AND M-12.
10. LOCATION OF GATE AND SWING OF GATE TO BE DIRECTED BY THE CITY OF CORCORAN.
11. LANDSCAPE OF POND TO BE APPROVED BY THE CITY OF CORCORAN.



**CITY of CORCORAN • Department of Public Works**

Standard Drawing for:

**PONDING BASIN DETAIL**

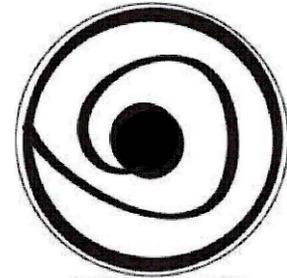
APPROVED BY: *[Signature]* 03/01/17  
City Engineer R.C.E 53350 Date

Revised:

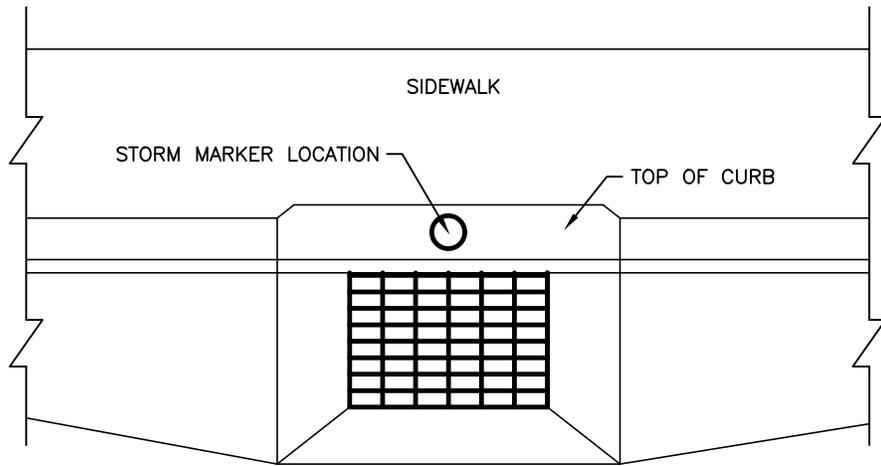
**SD-5**



FRONT  
STORM MARKER



BACK  
ADHESIVE PATTERN



PLAN

**NOTES:**

1. ALL STORM DRAIN INLETS SHALL BE MARKED "NO DUMPING – DRAINS TO WATERWAY".
2. STORM MARKERS AND ADHESIVE MUST BE PURCHASED FROM THE CITY OF CORCORAN.
3. ALTERNATE MARKERS MUST BE APPROVED BY THE DIRECTOR OF PUBLIC WORKS.

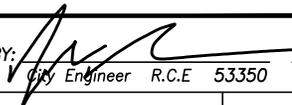
**INSTALLATION:**

1. READ THE ADHESIVE'S CAUTIONARY STATEMENT AND FIRST AID PROCEDURES BEFORE BEGINNING INSTALLATION, AS IT IS EXTREMELY FLAMMABLE
2. MARKERS SHALL BE INSTALLED ON THE CURB AND CENTERED ON THE GRATE AS SHOWN ABOVE.
3. APPLICATION SURFACE MUST BE FLAT.
4. CLEAN APPLICATION SURFACE WITH WIRE BRUSH. SURFACE MUST BE CLEAN, DRY, AND FREE OF ANY LOOSE DEBRIS.
5. APPLY ADHESIVE TO THE BACK OF THE MARKER. START 1/8" IN FROM THE OUTSIDE EDGE APPLYING AN EVEN BEAD AROUND THE ENTIRE EDGE, AND THEN SPIRAL INWARD TOWARD THE CENTER AS SHOWN ABOVE
6. PLACE MARKER ON APPLICATION SURFACE. PUSH IT DOWN HARD WITH A TWISTING MOTION, FORCING ADHESIVE OUT FROM THE EDGE. IT IS IMPORTANT THAT ADHESIVE SEALS THE ENTIRE OUTER EDGE OF THE MARKER.

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**STORM DRAIN INLET  
MARKER**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SD-6**

SANITARY SEWER CRITERIA

1. ACCEPTABLE MATERIALS:

TRUNKS, MAINS, COLLECTORS  
(8" AND LARGER) PVC (ASTM DESIGNATION D3034--SDR35)

SEWER SERVICE CONNECTIONS  
(4" AND 6") PVC (ASTM DESIGNATION D3034--SDR35)

\*ALL 4" SHALL BE FURNISHED IN 1/2 LENGTHS

2. ALL SEWER PIPE SHALL HAVE EITHER A COMPRESSION TYPE OR A SOLVENT WELDED JOINT.

3. A. SEWER CAPACITY DESIGN CRITERIA

1. 125 GAL. PER CAPITA PER DAY (RESIDENTIAL)
2. 5.0 DWELLINGS PER GROSS ACRE
3. 3.5 PERSONS PER DWELLING
4. DAILY CONTRIBUTION 2190 GAL. PER ACRE PER DAY (RESIDENTIAL)
5. AVERAGE FLOW – PIPE DESIGNED TO FLOW HALF FULL FOR PIPE SIZES LESS THAN 15" AND THREE-FOURTHS FULL FOR PIPE SIZES 15" AND LARGER TO ALLOW FOR PEAK FLOW.
6. COMMERICAL & LIGHT INDUSTRY  
3 X 2190 = 6570 GAL. PER ACRE PER DAY
7. HEAVY INDUSTRY  
6 X 2190 = 13,140 GAL. PER ACRE PER DAY

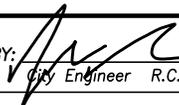
B. AVERAGE DESIGN FLOWS

1. RESIDENTIAL  
 $ACRES \times \frac{1290}{1440} = ACRES \times 1.52 \text{ GPM}$
2. COMMERICAL AND LIGHT INDUSTRY  
 $ACRES \times \frac{6570}{1440} = ACRES \times 4.57$
3. HEAVY INDUSTRY  
 $ACRES \times \frac{13,140}{1440} = ACRES \times 9.13 \text{ PGM}$

CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**SANITARY SEWER  
CRITERIA**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SS-1**

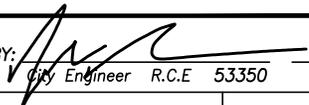
SANITARY SEWER CRITERIA

4. SEWER LINES SHALL BE DESIGNATED WITH A MINIMUM MANNING COEFFICIENT OF  $N=0.013$  FOR VCP AND  $N=0.012$  FOR PVC.
5. THE MINIMUM SLOPES SHALL BE CALCULATED ON THE BASIS OF A MINIMUM VELOCITY OF TWO FEET PER SECOND.
6. THE MINIMUM SIZE SEWER MAIN SHALL BE 8-INCHES.
7. THE MINIMUM RADIUS OF CURVATURE AND ANGLE OF DEFLECTION THAT WILL BE ALLOWED IN SEWER LINE IS 1.5 TIMES THE MANUFACTURER'S RECOMMENDED MINIMUM.
8. MINIMUM DEPTH OF COVER:
  - A. 3.5' OVER MAIN LINE IN STREET FOR 8"-12" PIPES. 4.0' OVER MAIN LINE IN STREET FOR PIPES GREATER THAN 12".
  - B. 3.0' OVER SERVICE CONNECTIONS AT PROPERTY LINE.
9. MANHOLE SPACING:
  - A. SEWERS 8" : 450' MAXIMUM
  - B. SEWERS 10" - 12" : 500 MAXIMUM
  - C. SEWERS 15" AND LARGER : 600 MAXIMUM
  - D. AT ALL ANGLE POINTS IN HORIZONTAL AND VERTICAL ALIGNMENT.
  - E. AT THE TERMINAL END OF ALL LINES.
10. DROP MANHOLES WILL NOT BE PERMITTED UNLESS APPROVED BY THE ENGINEER.
11. CLEAN-OUTS WILL ONLY BE ALLOWED ON THE TEMPORARY TERMINI OF A LINE.

CITY of CORCORAN • Department of Public Works

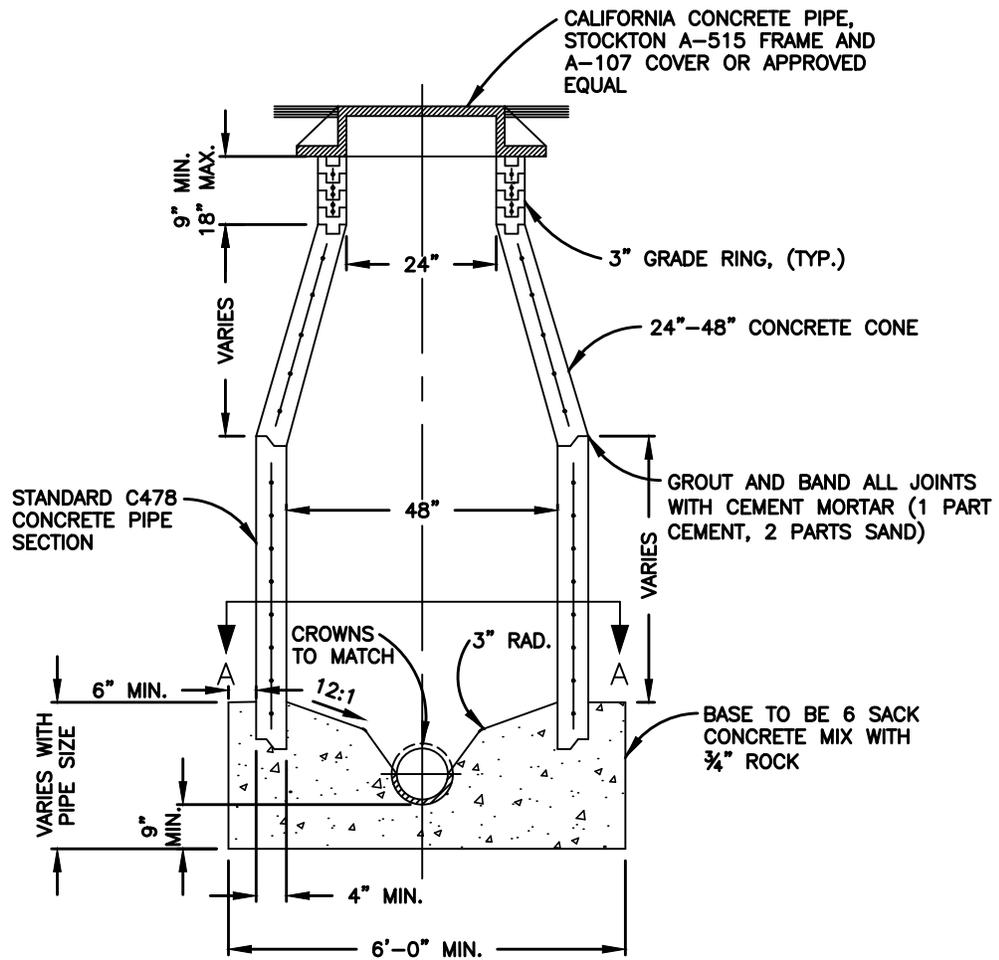
Standard Drawing for:

**SANITARY SEWER  
CRITERIA**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SS-1A**



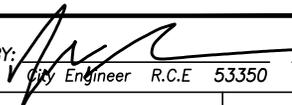
**NOTE:**

WHERE MANHOLES ARE NOT LOCATED IN STREETS OR TRAVELED WAY, PLACE TOP OF MANHOLE 12"-24" ABOVE EXISTING GROUND UNLESS OTHERWISE SHOWN ON THE PLANS.

**CITY of CORCORAN • Department of Public Works**

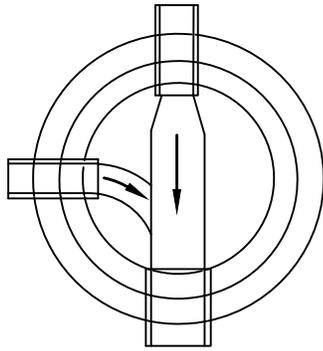
Standard Drawing for:

**48 IN. MANHOLE**

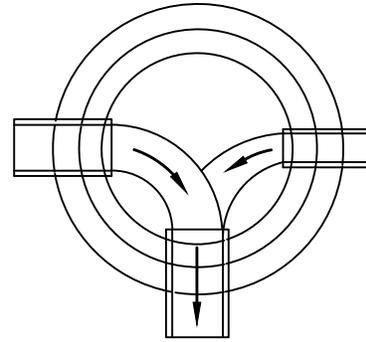
APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

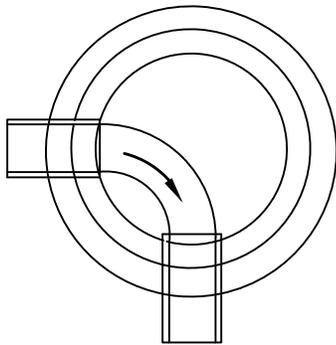
**SS-2**



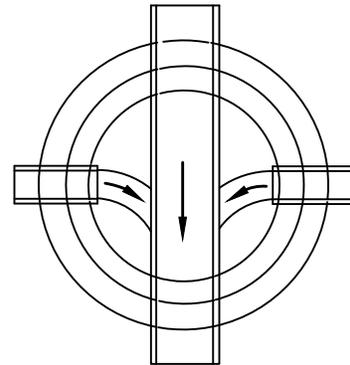
SIZE CHANGE IN THE MAIN  
NOT TO SCALE



MAIN TERMINATING IN A M.H.  
NOT TO SCALE



ALIGNMENT CHANGE IN THE MAIN  
NOT TO SCALE



PIPE CONTINUOUS THROUGH M.H.  
NOT TO SCALE

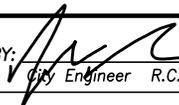
NOTES:

1. FLEXIBLE PIPE THROUGH A MANHOLE REQUIRES A WATER STOP.
2. TERMINATING PIPES TO EXTEND INTO M.H. INTERIOR A MAX. DISTANCE OF 4".
3. THE TOP HALF OF ALL PIPE WITHIN THE M.H. IS TO BE CUT AWAY. COVER THE CUT EDGES WITH MORTAR CONSTRUCTING THE BENCHING.
4. PREPARE A SMOOTH TROWLED CONC. CHANNEL HAVING UNIFORM GRADIENT BETWEEN PIPE INVERTS. END OF PIPE TO BE BEVELED AT 45°.
5. EXTEND THE CHANNEL WALLS UP TO A HEIGHT EVEN WITH TOP OF THE PIPE.
6. HAND TAMP 9" OF SELECT BACKFILL MATERIAL UNDER ALL PIPING COMING OUT OF THE MANHOLE UP TO THE FIRST JOINT.
7. THE BREADTH OF THE CHANNEL AT EACH JUNCTION MUST ALWAYS BE AS GREAT AS THE DIAMETER OF THE CONNECTED PIPE.
8. INSIDE RADIUS OF CHANNEL TO BE GREATER THAN THE DIAMETER OF THE LARGEST CONNECTED PIPE UP TO 2 PIPE DIAMETERS.
9. SLOPE THE BENCHING UP TOWARD THE M.H. WALL AS INDICATED IN THE M.H. SECTION. TROWEL THE SURFACE SMOOTH.
10. INCOMING NON-STRAIGHT LINES SHALL HAVE AN INVERT THAT IS AT LEAST .10' HIGHER THAN THE OUT GOING LINE.

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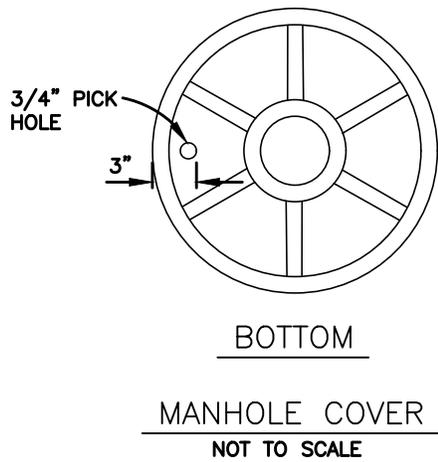
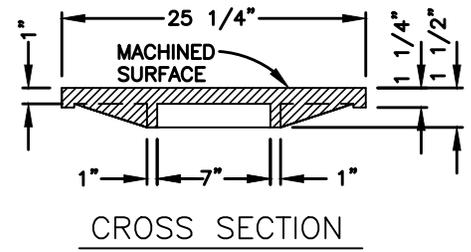
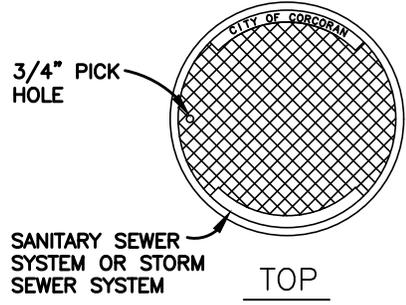
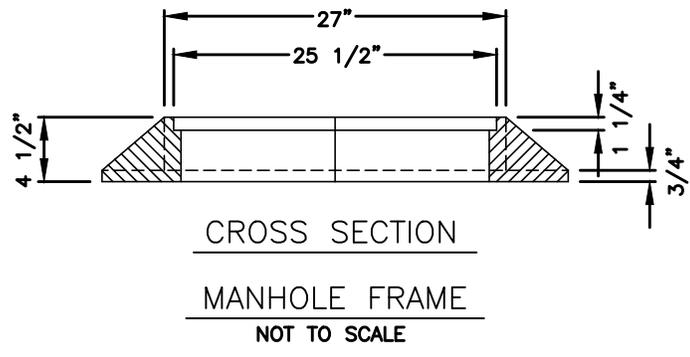
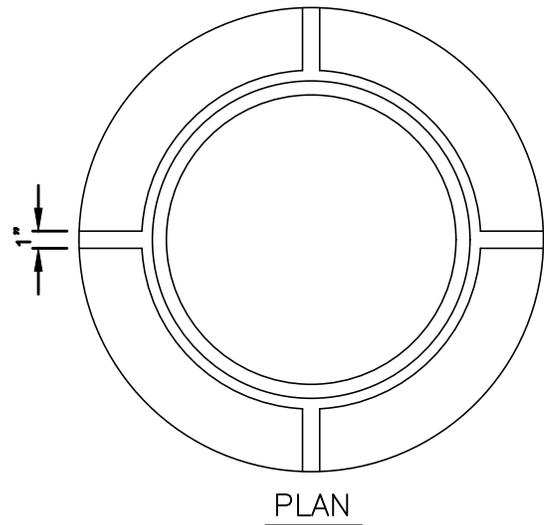
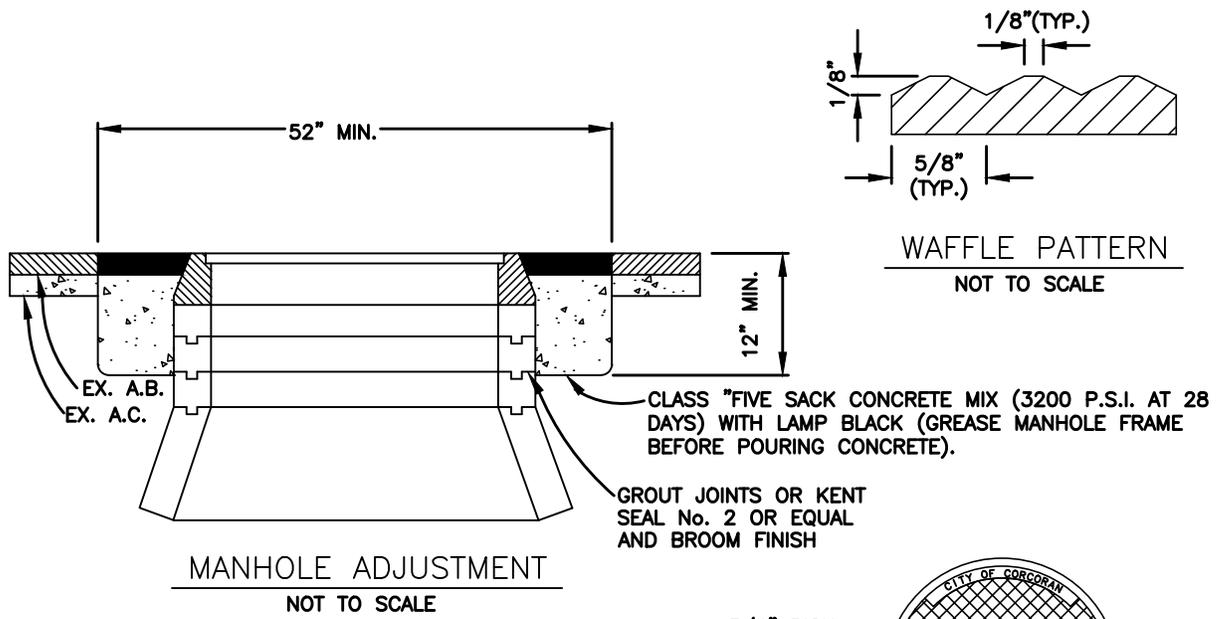
Standard Drawing for:

**INVERT PLANS OF  
STANDARD MANHOLES**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SS-3**



CITY of CORCORAN • Department of Public Works

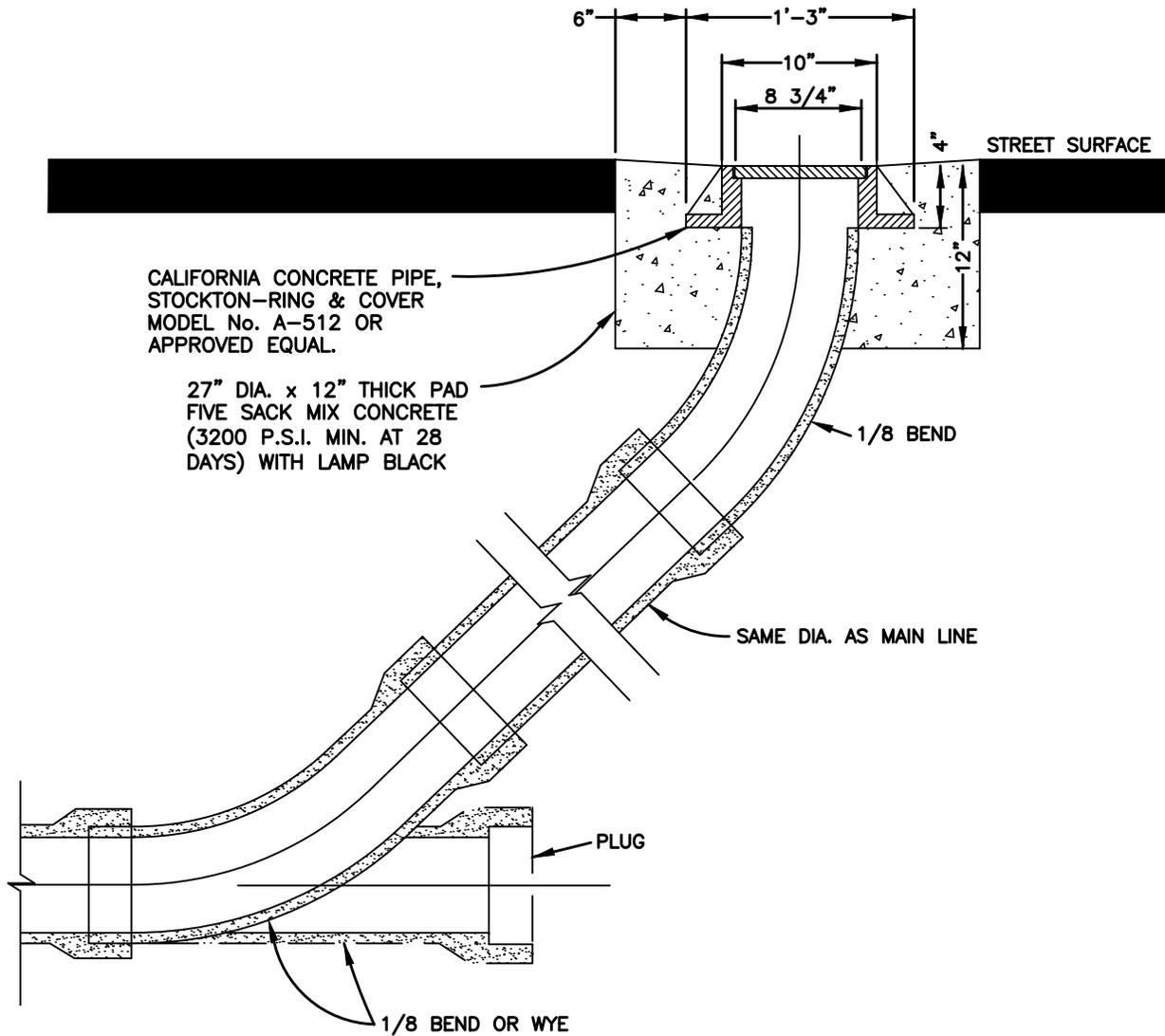
Standard Drawing for:

**MANHOLE FRAME AND COVER**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

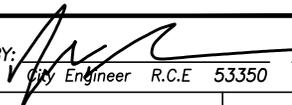
**SS-4**



CITY of CORCORAN • Department of Public Works

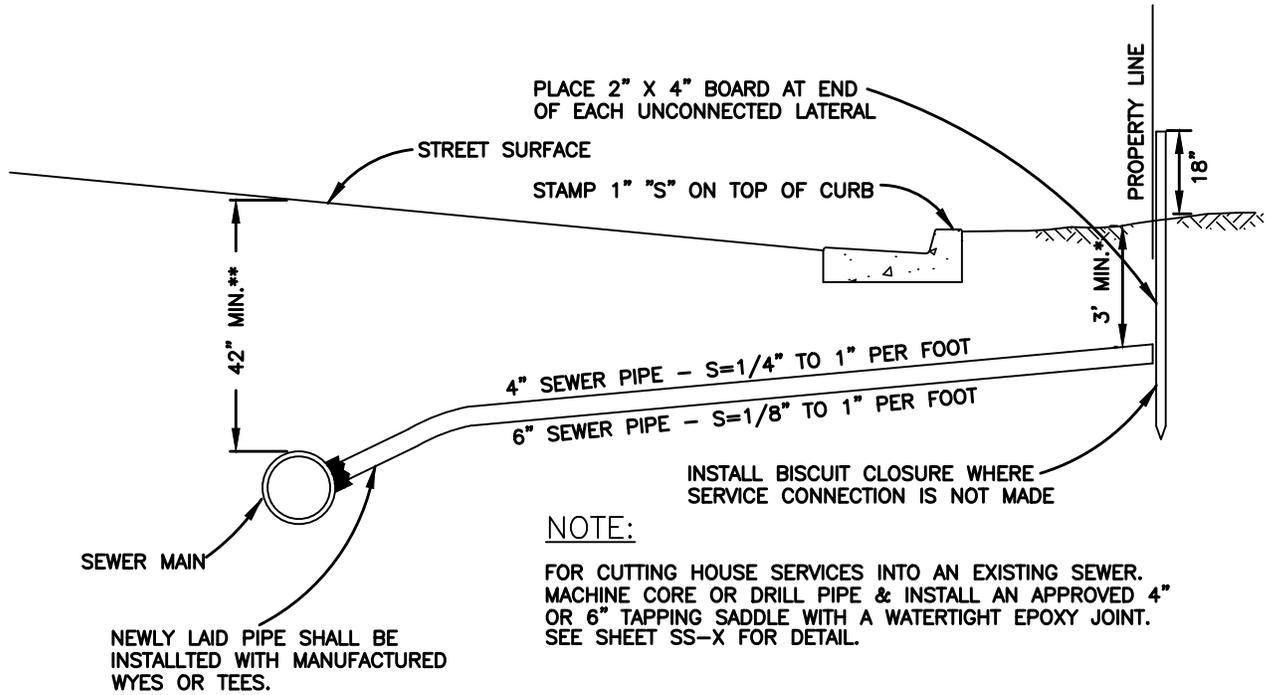
Standard Drawing for:

**CLEANOUT**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SS-5**



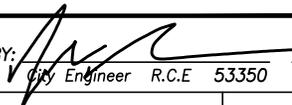
\* FOR DEPTHS OF LESS THAN 3' OF COVER USE DUCTILE IRON PIPE OR CEMENT SLURRY ENCASED PIPE TO THE APPROVAL OF THE CITY ENGINEER.

\*\* 8" - 12" PIPE, 42" MIN. COVER  
OVER 12" PIPE, 48" MIN. COVER

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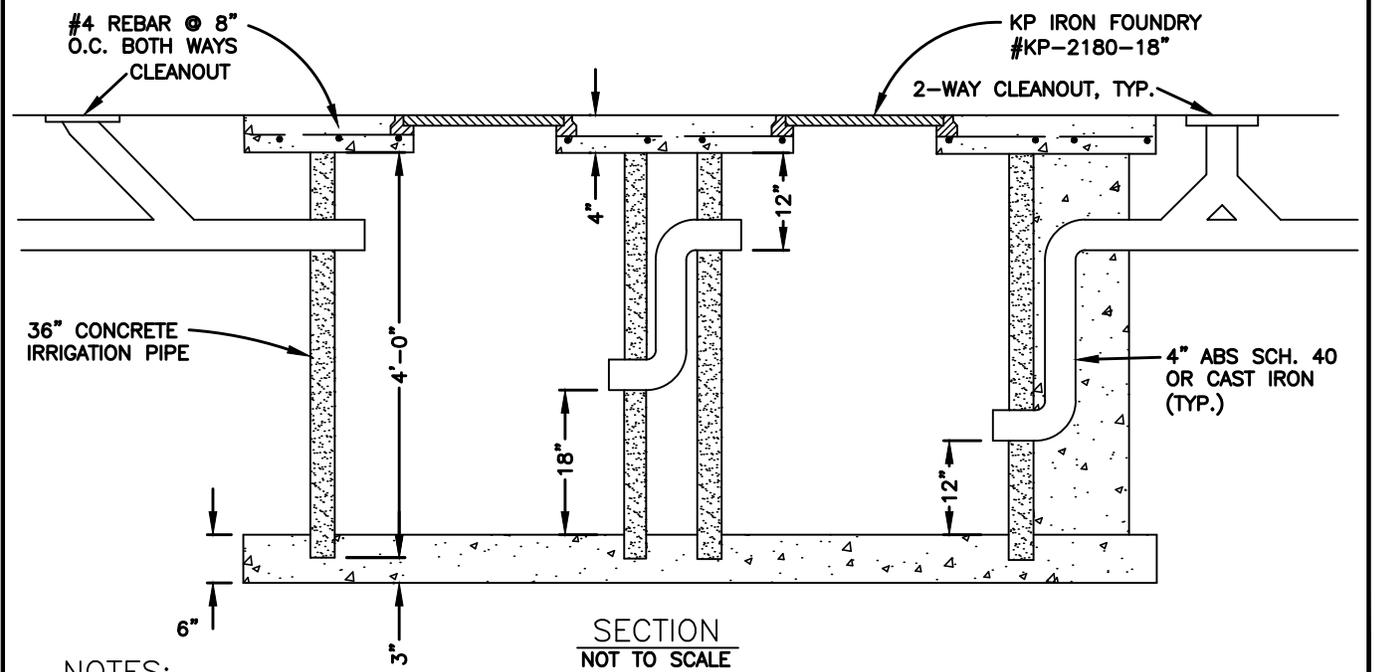
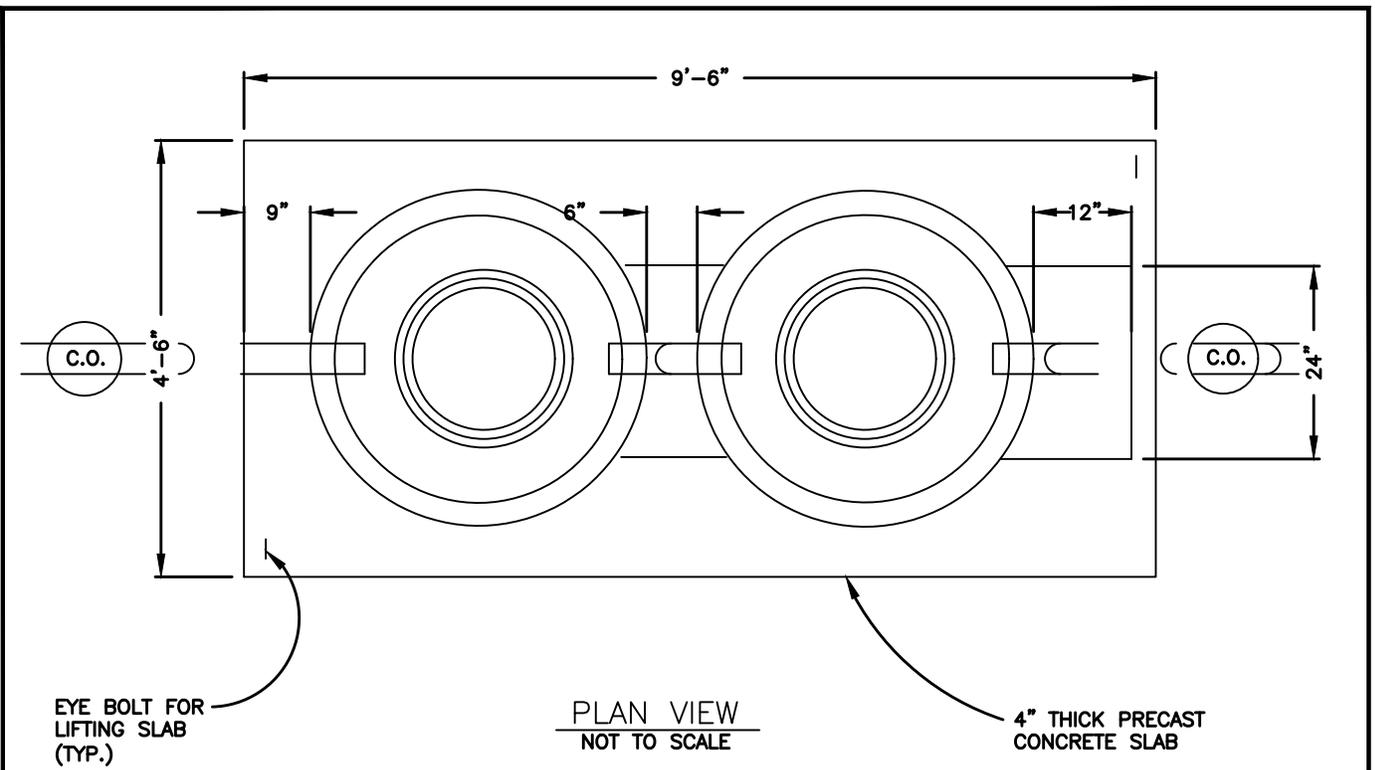
Standard Drawing for:

**SEWER LATERAL**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SS-6**



NOTES:

1. SLAB MAY BE POURED IN PLACE AT CONTRACTOR'S OPTION.
2. CONTRACTOR MAY SUBSTITUTE A MANUFACTURED GREASE TRAP WITH APPROVAL OF BUILDING OFFICIAL.
3. THE ABOVE DRAWING DEPICTS A MINIMUM SIZE GREASE TRAP ENCLOSURE. ACTUAL SIZE WILL VARY DEPENDING UPON QUANTITY OF FLOW. THE CITY BUILDING OFFICIAL MUST APPROVE ALL INSTALLATIONS.

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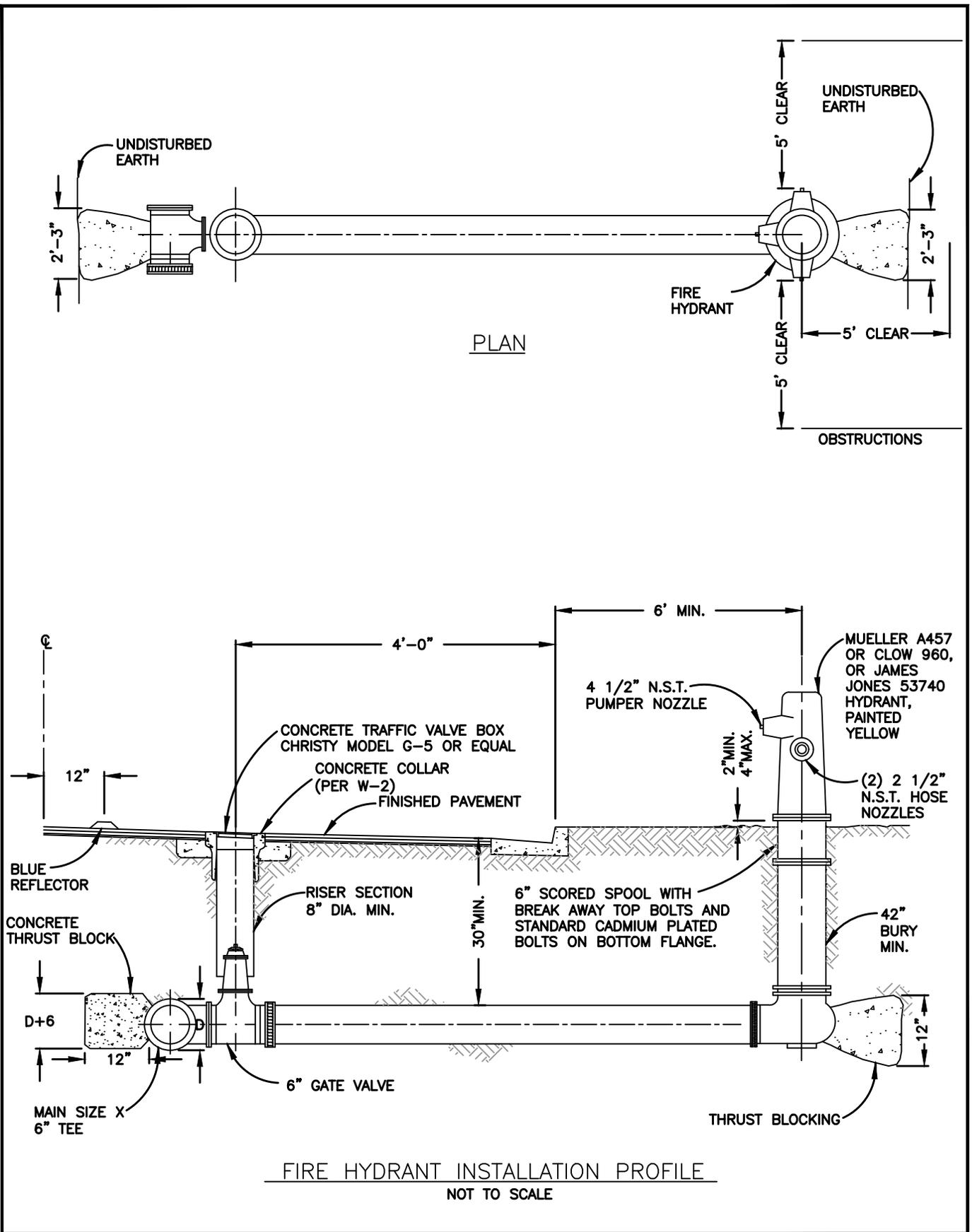
Standard Drawing for:

**SAND - GREASE TRAP**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**SS-7**



CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**FIRE HYDRANT ASSEMBLY**

APPROVED BY:

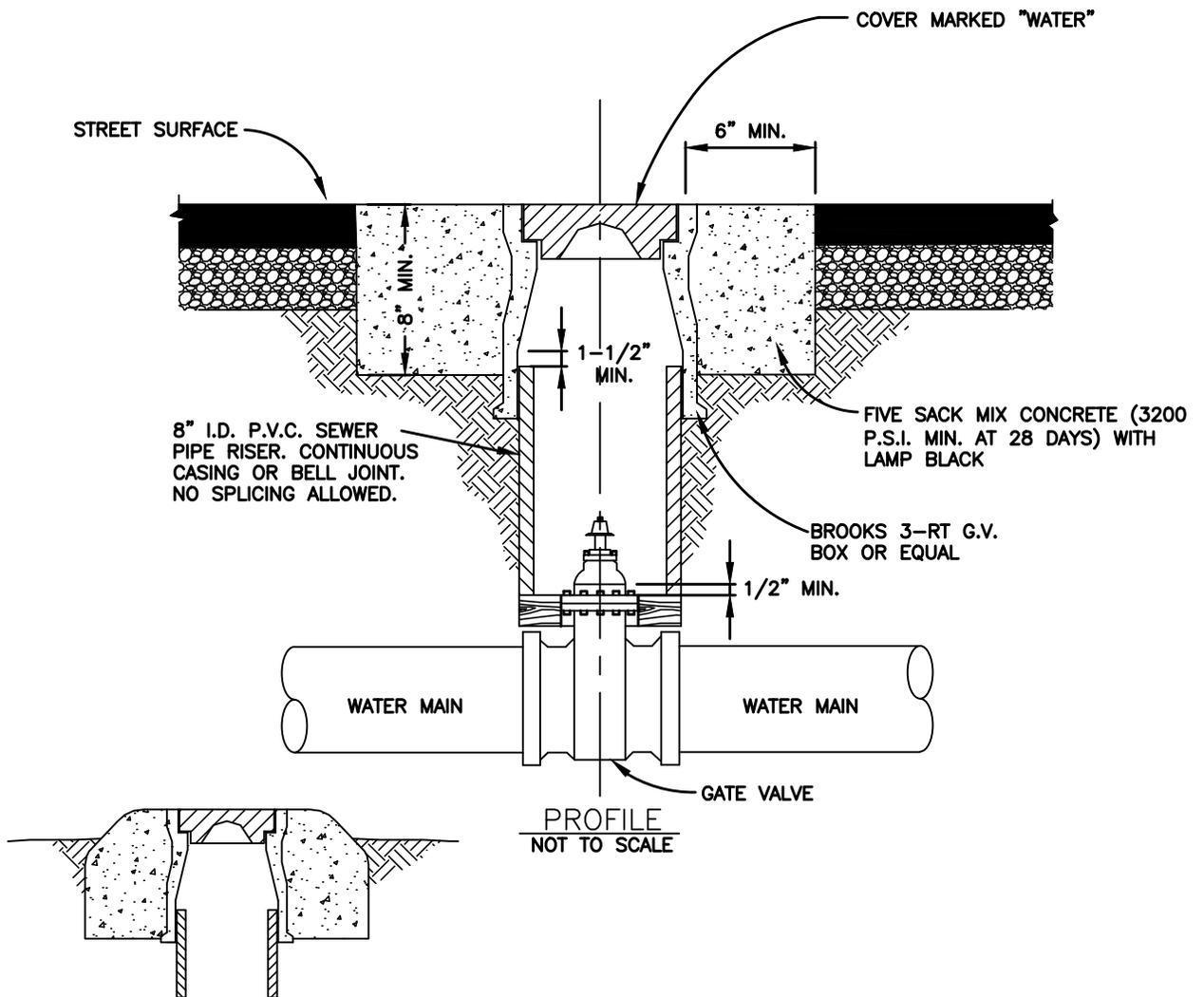
*[Signature]*  
City Engineer R.C.E 53350

03/01/17

Date

Revised:

**W-1**



DETAIL A

NOTE:

VALVE BOXES SET IN ROAD SHOULDERS, FIELDS, ETC. SHALL BE AS SHOWN IN DETAIL "A".

APPROVED GATE VALVES

6" - 10"		12" AND OVER	
CLOW F-5082	MUELLER A-2380-24	MUELLER A-2370-26	WATEROUS SERIES 500
KENNEDY 573X	MUELLER A-2370-26	MUELLER A-2370-26	WATEROUS SERIES 500
M & H 67 RT, O-RING	MUELLER A-2370-26	MUELLER A-2370-26	WATEROUS SERIES 500

ALL VALVES SHALL BE A.W.W.A. APPROVED RESILIENT WEDGE

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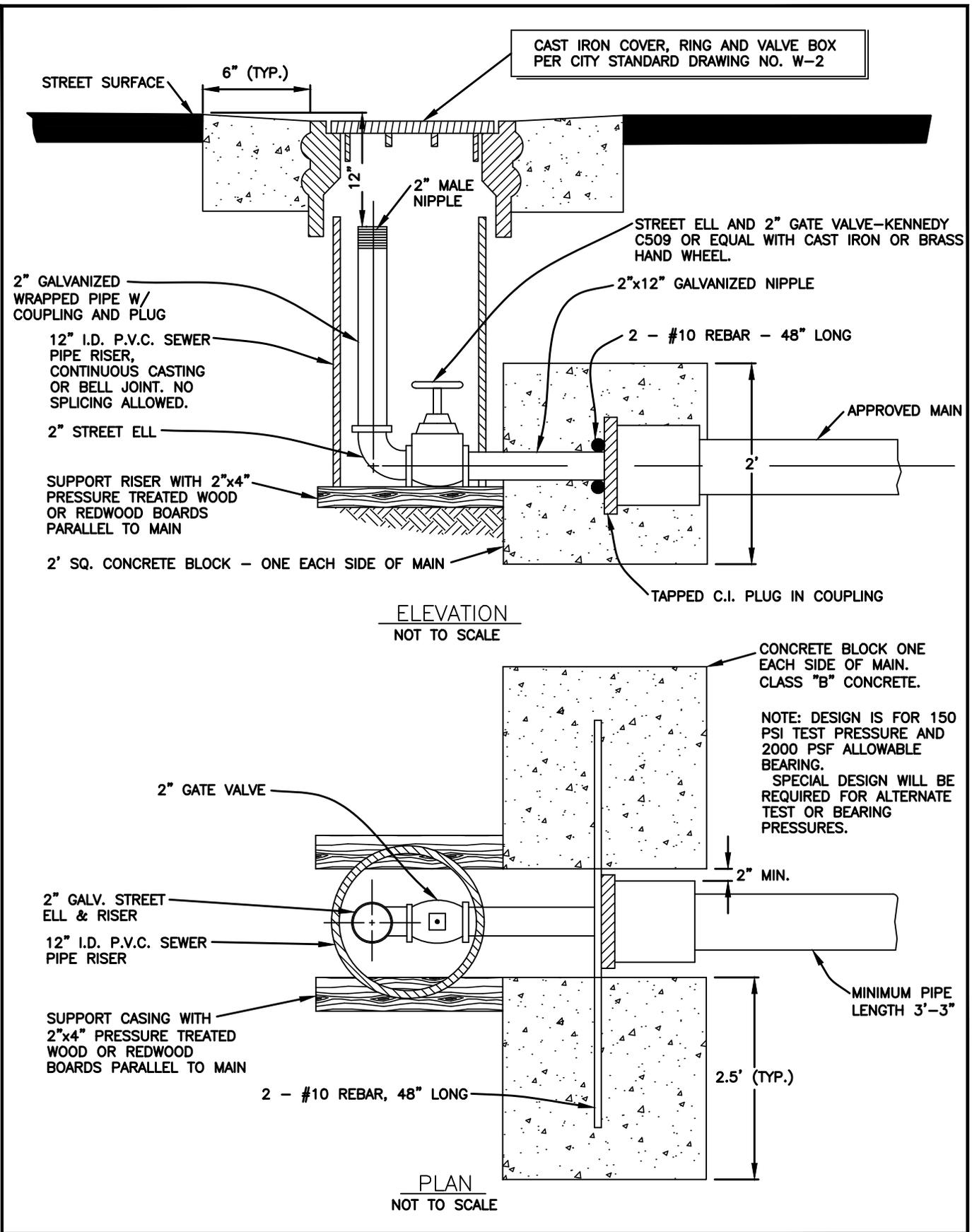
Standard Drawing for:

VALVE WELL & COVER

APPROVED BY: *[Signature]* 03/01/17  
City Engineer R.C.E 53350 Date

Revised:

W-2



CITY of CORCORAN • Department of Public Works

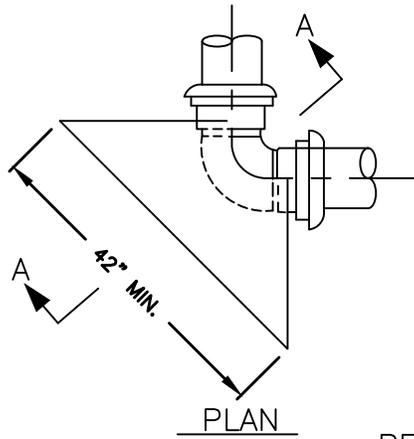
Standard Drawing for:

**BLOW-OFF ASSEMBLY**

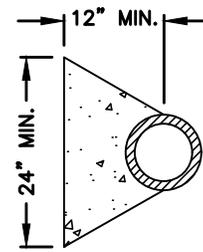
APPROVED BY: *[Signature]* 03/01/17  
City Engineer R.C.E 53350 Date

Revised:

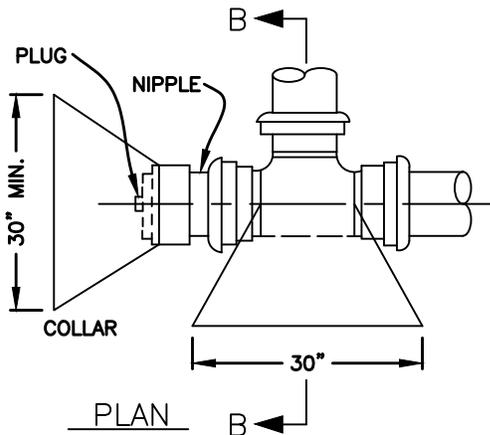
**W-3**



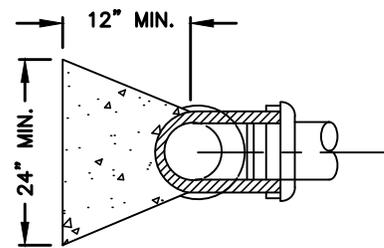
BENDS  
NOT TO SCALE



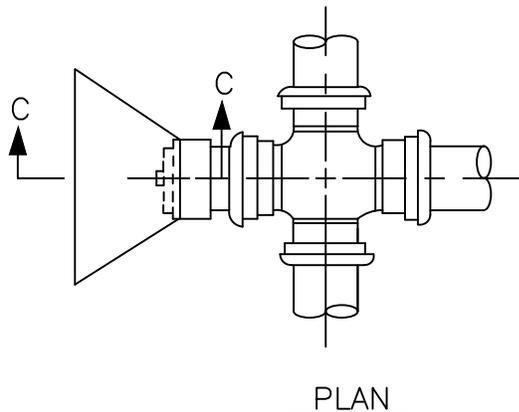
SECTION A - A



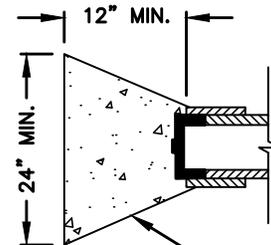
TEES & PLUGS  
NOT TO SCALE



SECTION B - B



PLUGGED CROSSES  
NOT TO SCALE



CONCRETE FIVE SACK  
MIX (3000 P.S.I. MIN.  
IN 28 DAYS).

SECTION C - C

NOTE:

THRUST BLOCKS ACCORDING TO PIPE MANUFACTURER  
INSTALLATION GUIDE AND APPROVAL OF CITY ENGINEER.

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Standard Drawing for:

**CONCRETE THRUST  
BLOCKS**

APPROVED BY:

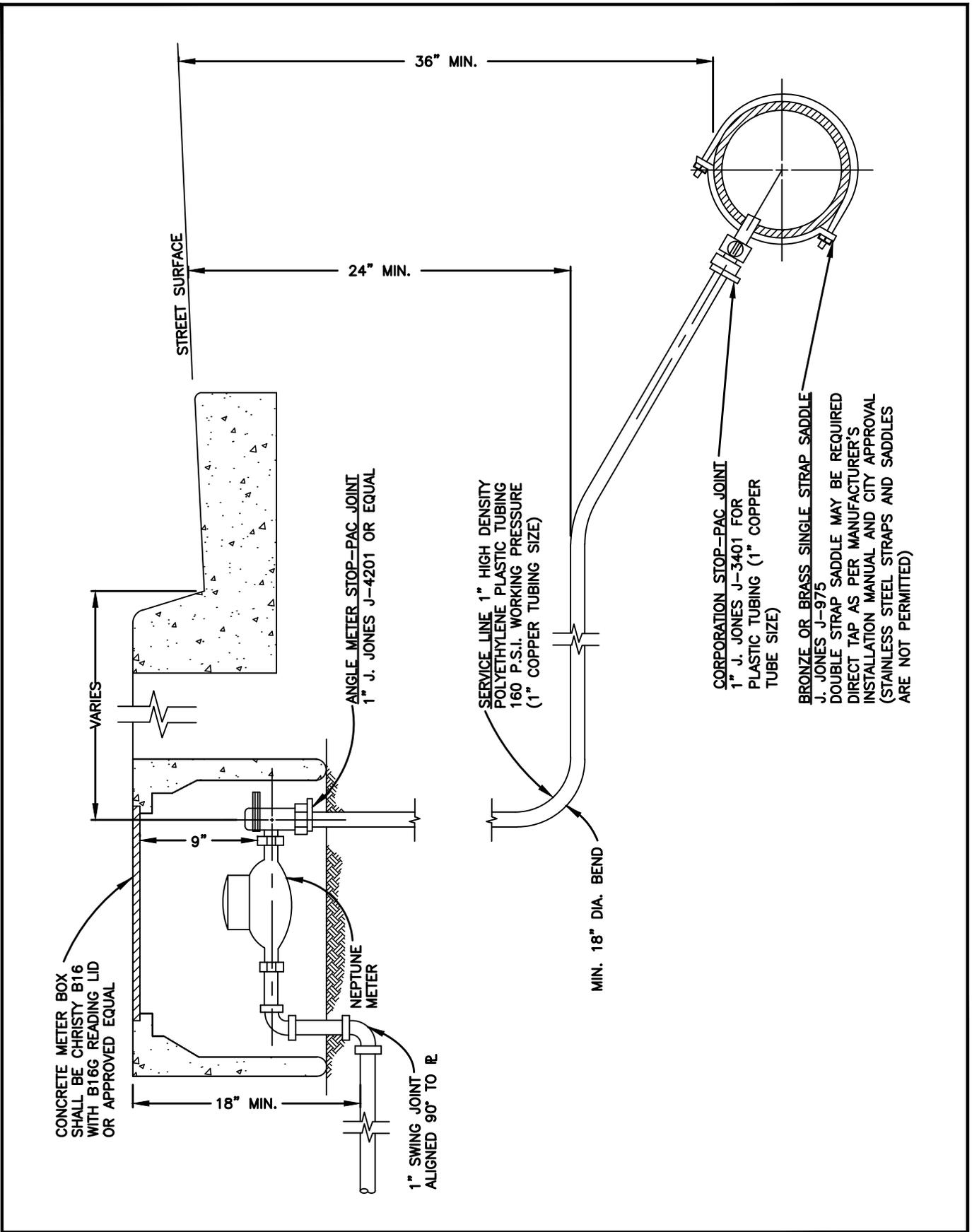
City Engineer R.C.E 53350

03/01/17

Date

Revised:

**W-4**



CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**1 IN. RESIDENTIAL  
WATER SERVICE**

APPROVED BY:

*[Signature]*

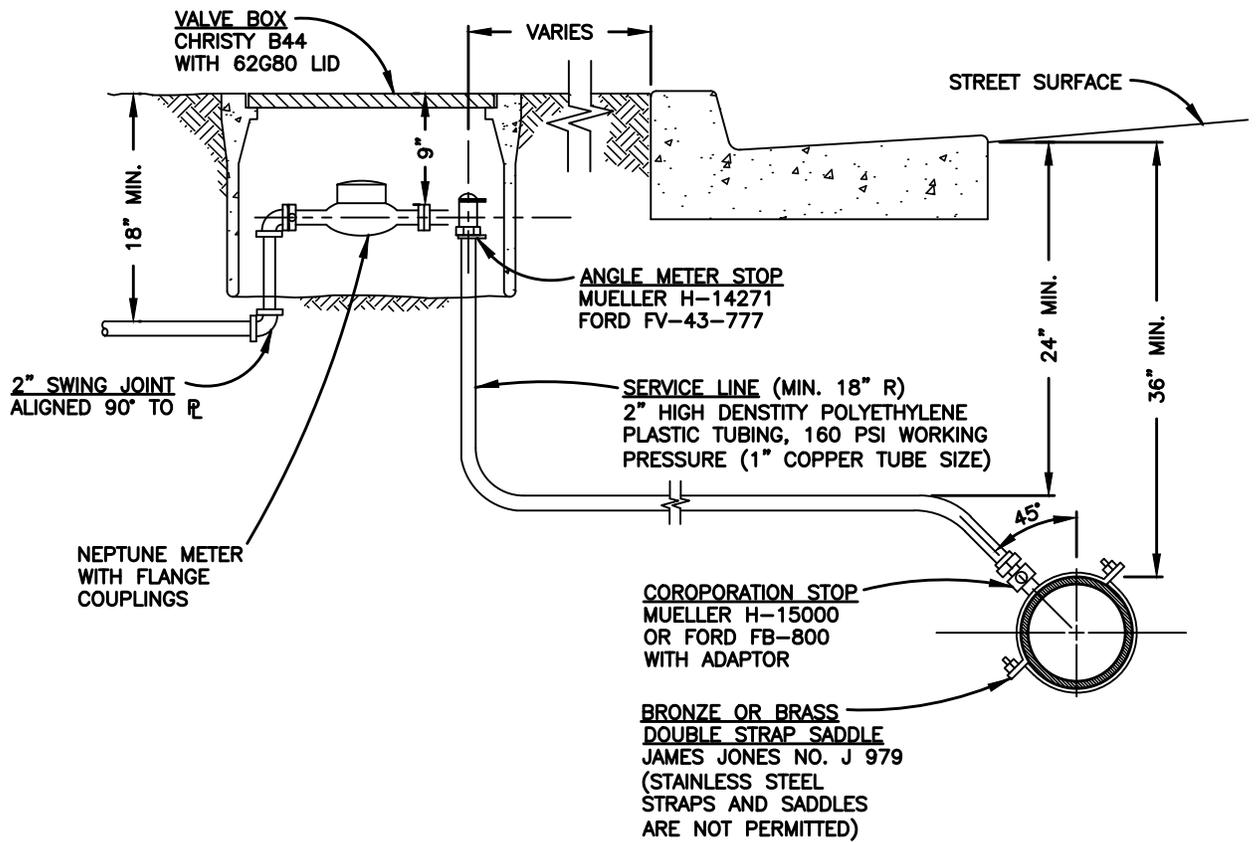
City Engineer R.C.E 53350

10/30/17

Date

Revised:

**W-5**



CITY of CORCORAN • Department of Public Works

Standard Drawing for:

**1.5 IN. AND 2 IN. WATER  
SERVICE**

APPROVED BY:

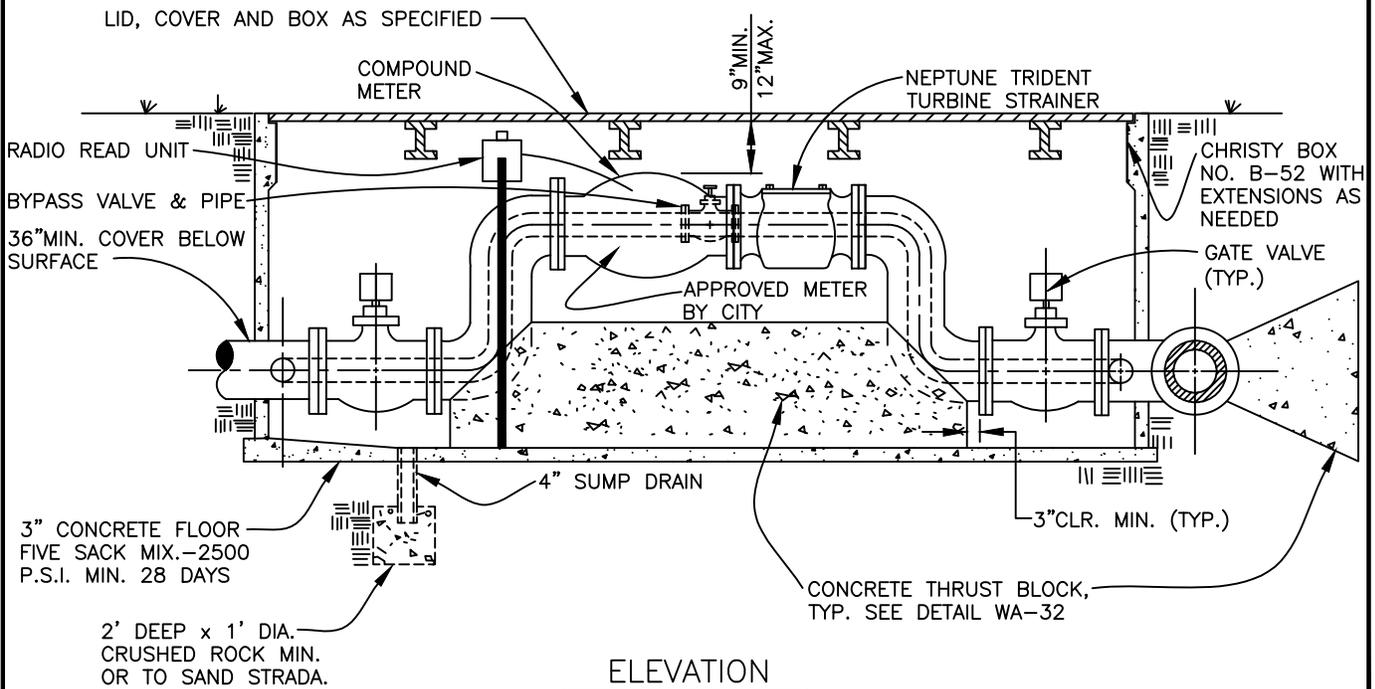
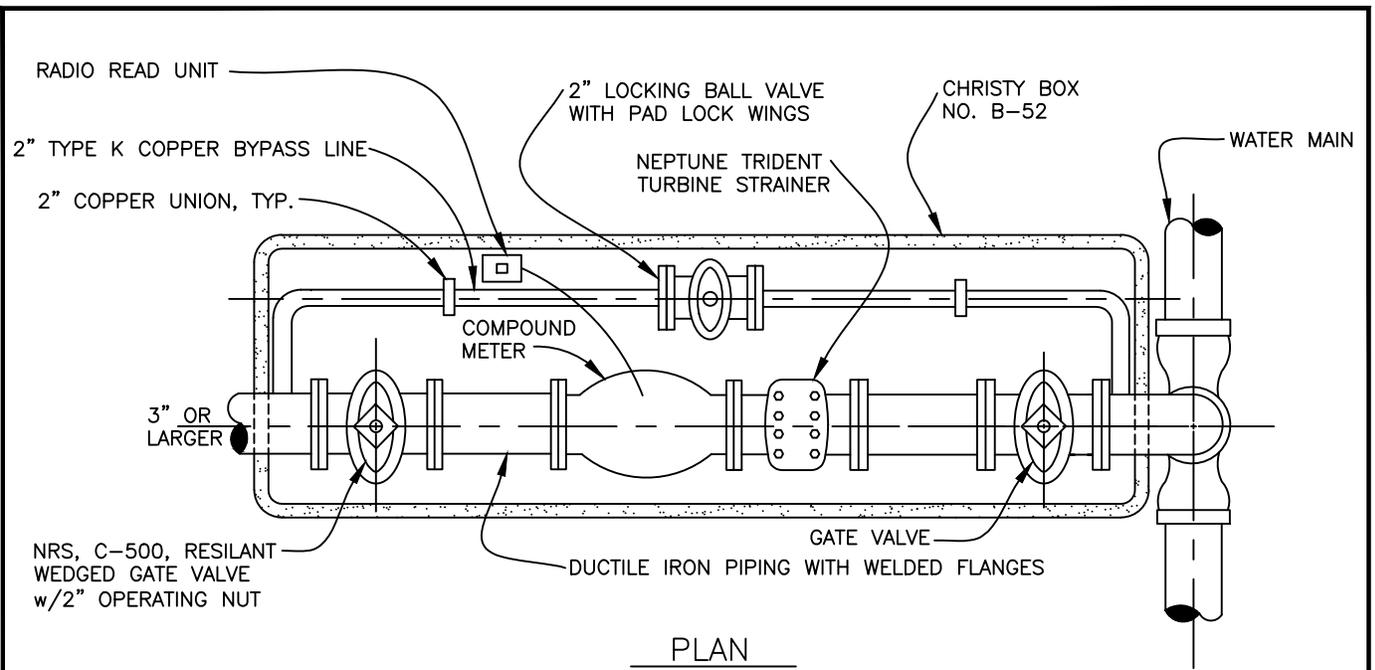
City Engineer R.C.E 53350

10/30/17

Date

Revised:

**W-6**



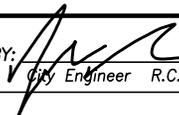
**NOTES:**

1. HOT TAP OF EXISTING WATER MAIN WILL REQUIRE A TAPPING VALVE & TAPPING SLEEVE.
2. THE FIREFLY (DATAMATIC) RADIO READ UNIT MUST BE CONNECTED TO THE REGISTER PRIOR TO INSTALLATION.

**CITY of CORCORAN • Department of Public Works**

Standard Drawing for:

**3 IN. OR LARGER  
WATER SERVICE**

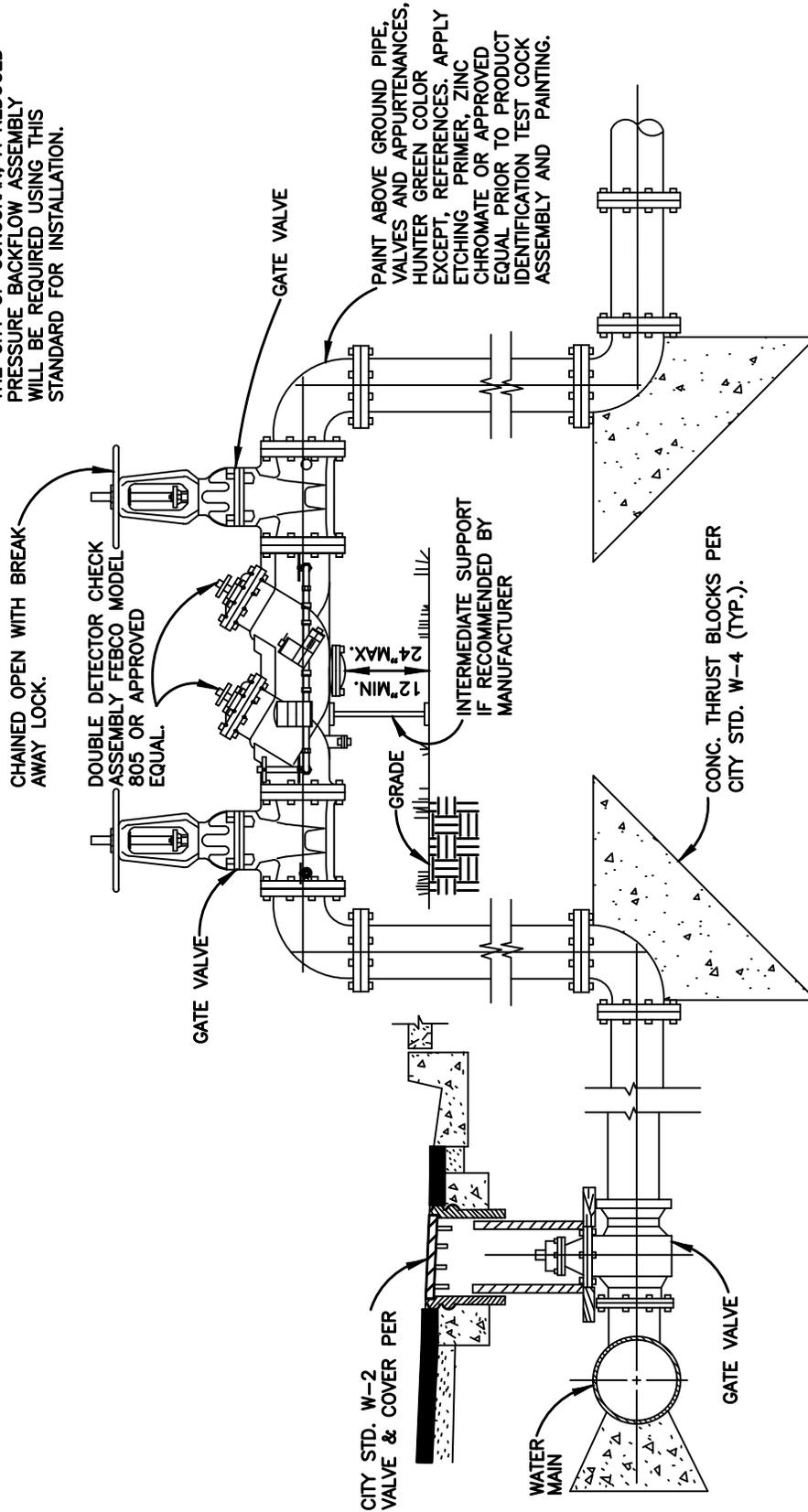
APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**W-6A**

**NOTE:**

WHERE IT HAS BEEN DETERMINED BY THE CITY OF CORCORAN, A REDUCED PRESSURE BACKFLOW ASSEMBLY WILL BE REQUIRED USING THIS STANDARD FOR INSTALLATION.



SECTION VIEW  
NO SCALE

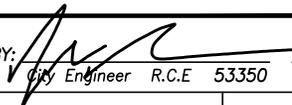
**NOTE:**

UNDERGROUND PIPING FOR FIRE LINES SHALL BE TESTED HYDRAULICALLY AT 200 PSI FOR 2 HOURS AS PER NFPA 13 UNDERGROUND PIPE SHALL NOT BE BURIED BUT MAY BE CENTER LOAD BURIED DURING 200PSI TEST. PIPE STAMP WORDING SHALL BE FACING UPWARD, FIRE DEPT. TO WITNESS TEST.

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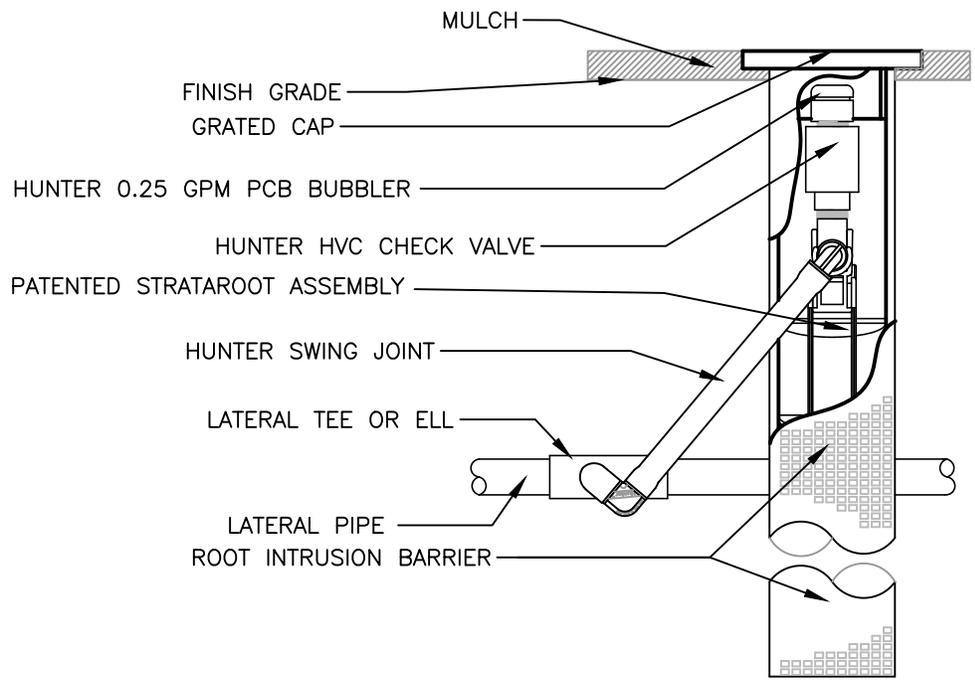
Standard Drawing for:

**FIRE SERVICE**

APPROVED BY:  03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**W-7**



NOTE: INSTALL HUNTER  
 FILTER FABRIC SLEEVE—  
 #RZWS—SLEEVE

CITY of CORCORAN • Department of Public Works

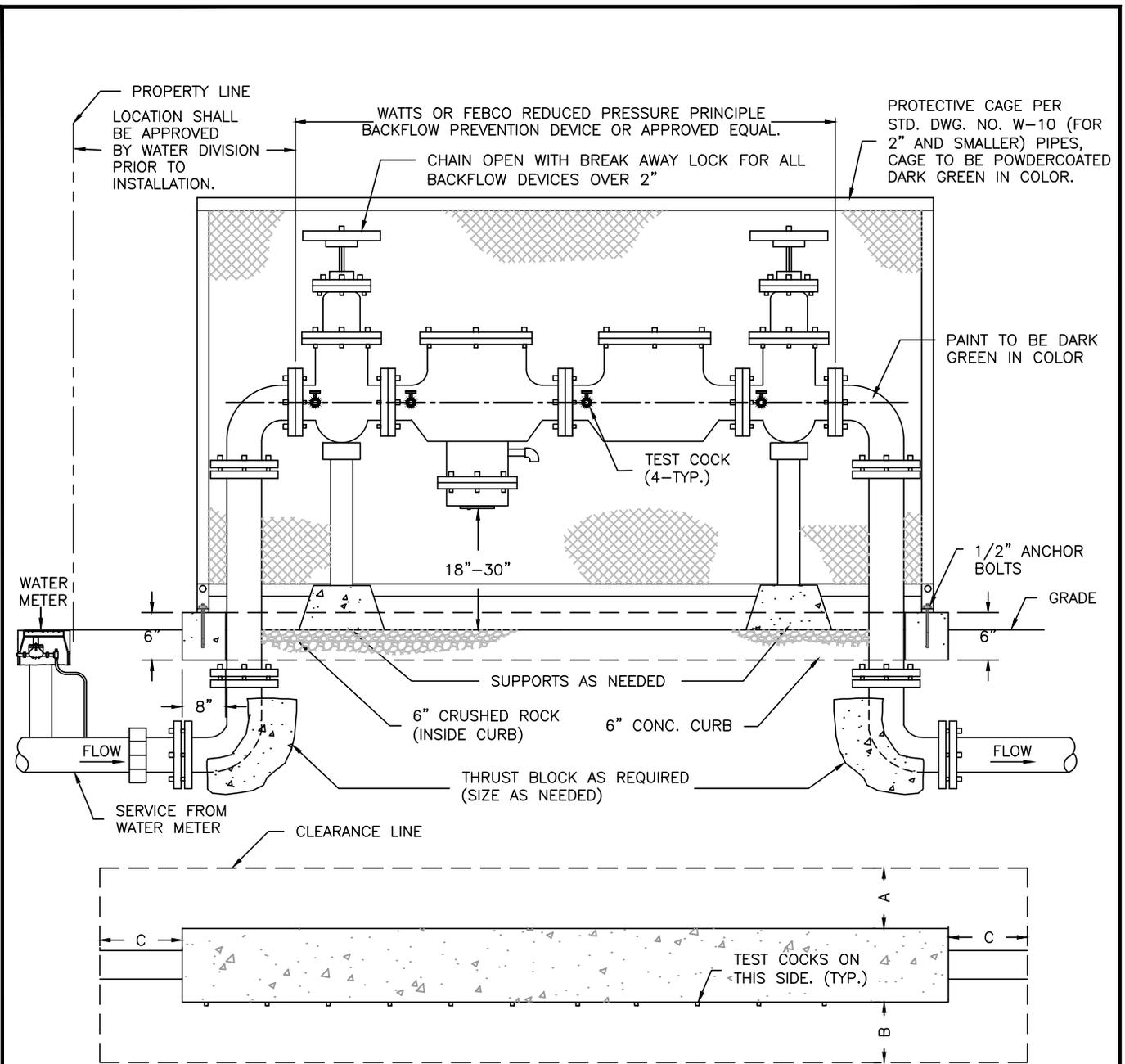
Standard Drawing for:

**DEEP ROOT WATERING  
 SYSTEM**

APPROVED BY:  03/01/17  
 City Engineer R.C.E 53350 Date

Revised:

**W-9**



**NOTES:** **PLAN VIEW**

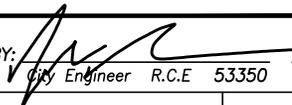
1. GATE VALVES AND TEST COCKS ARE REQUIRED.
2. NO CONNECTIONS OR TEES WILL BE ALLOWED BETWEEN METER AND DEVICE.
3. PROVIDE INSULATION BLANKET.
4. DEVICE MUST BE ACCESSIBLE FOR TESTING AND MAINTENANCE.
5. PROVIDE PROTECTIVE CAGE PER STD. DWG. NO. W-10.
6. THE DEVICES SHALL BE INSTALLED ADJACENT TO AND ON THE PROPERTY SIDE OF THE SIDEWALK WHERE APPLICABLE. THE DEVICE SHALL BE INSTALLED AS CLOSE TO THE WATER METER AS POSSIBLE.
7. THE DEVICE SHALL BE THE SAME SIZE OR LARGER THAN THE SERVICE METER.
8. LANDSCAPING OR OTHER SCREENING AROUND THE DEVICE SHALL BE AS SHOWN ON THE APPROVED LANDSCAPE PLANS.
9. PROVIDE PROTECTIVE CAGE PER STD. DWG. NO W-10.
10. PIPE MUST BE BRONZE OR DUCTILE IRON.
11. BACKFLOW MUST BE CERTIFIED PRIOR TO ACCEPTANCE BY CITY OF LEMOORE.
12. SUBMIT BACKFLOW DEVICE FOR ACCEPTANCE BY CITY OF LEMOORE.
13. BACKFLOW DEVICE SHALL BE COVERED WITH AN INSULATION BLANKET TO PREVENT FREEZING

MINIMUM CLEARANCE			
SIZE	A	B	C
1'-3"	12"	18"	12"
4" & UP	24"	24"	12"
Maximum	36"	36"	36"

**CITY of CORCORAN • Department of Public Works**

Standard Drawing for:

**REDUCED PRESSURE  
BACKFLOW DEVICE**

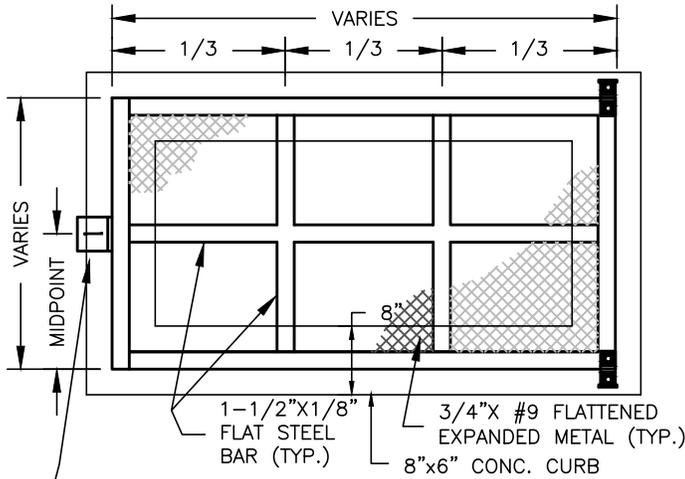
APPROVED BY:  **03/01/17**  
City Engineer R.C.E 53350 Date

Revised:

**W-10**

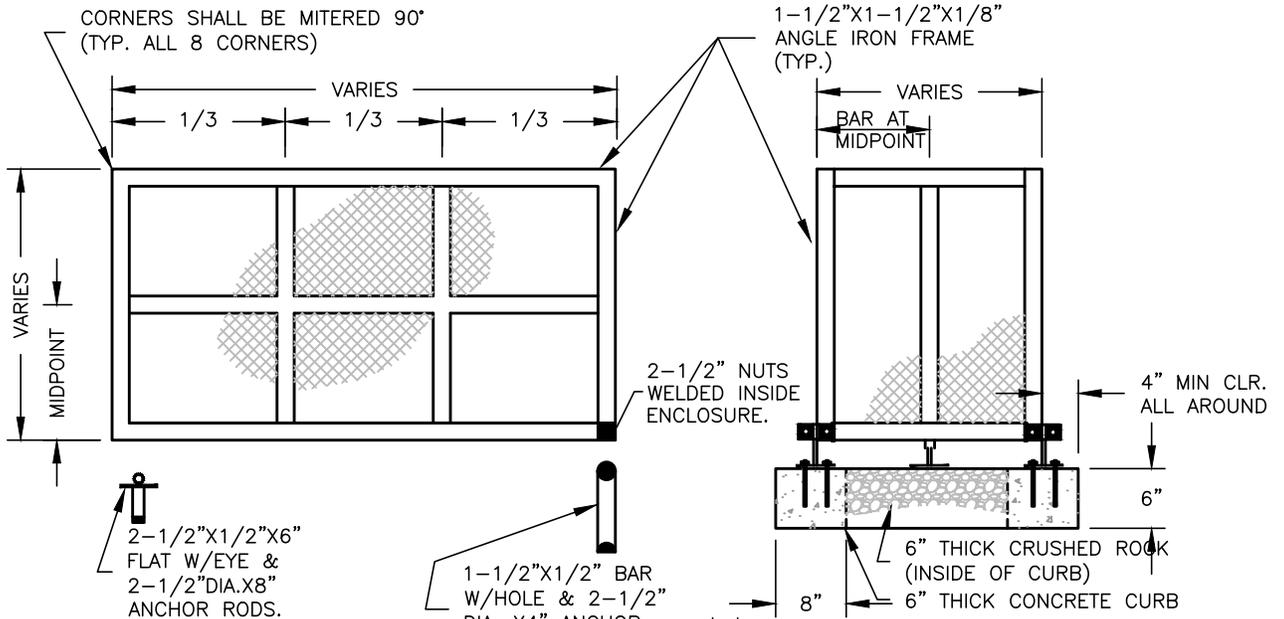
**NOTES:**

1. EXPANDED METAL GRATING TO BE WELDED INSIDE STRUCTURAL SUPPORTS EVERY 5" MIN.
2. ALL DIAGONAL & CIRCULAR CUT EXPOSED EDGES SHALL BE Banded WITH A BAR EQUAL TO THE OVER ALL GRATING THICKNESS AND WELDED AT ALL CONTACT POINTS.
3. HINGE SHALL BE 1/2" GALVANIZED BOLT IN TO THE NUT WELDED INSIDE ENCLOSURE. END OF BOLT SHALL BE ALTERED TO PREVENT REMOVAL FROM OUTSIDE ENCLOSURE.
4. WELD ALL 1/2" DIA. ROD INTERSECTIONS WITH 3/8" FILLET WELDS.
5. CURB SHALL BE 6" THICK CLASS "A" CONCRETE.
6. ALL PIPING EXTENDING THROUGH CONCRETE CURB SHALL BE WRAPPED WITH 20 MIL. PLUMBERS TAPE.
7. ALL EXPOSED PARTS SHALL BE PRIMERED AND PAINTED WITH RUST PREVENTIVE PAINT.
8. ENCLOSURE DIMENSIONS WILL VARY DEPENDING ON SIZE AND TYPE OF EQUIPMENT. SEE MINIMUM CLEARANCE CHART ON CITY STD, NO. W-10.



2 1/2"x1/2"x6" FLAT W/SLOT FOR LOCK EYE, WELDED TO 2 1/2"x1/2"x3" FLAT, WHICH SHALL BE WELDED TO THE ANGLE IRON FRAME.

**TOP VIEW**  
N.T.S.



2-1/2"x1/2"x6" FLAT W/EYE & 2-1/2"DIA.X8" ANCHOR RODS.

1-1/2"x1/2" BAR W/HOLE & 2-1/2" DIA. X4" ANCHOR RODS.

**FRONT VIEW**  
N.T.S.

**END VIEW**  
N.T.S.

1/2" ANCHOR BOLTS

**HINGE CONNECTION**  
N.T.S.

**CITY of CORCORAN • Department of Public Works**

Standard Drawing for:

**PROTECTIVE CAGE**

APPROVED BY: *[Signature]* 03/01/17  
City Engineer R.C.E 53350 Date

Revised:

**W-11**